

June 23, 2010

TO: Members of the MAG Regional Council

FROM: Councilwoman Peggy Neely, Phoenix, Chair

SUBJECT: MEETING NOTIFICATION AND TRANSMITTAL OF TENTATIVE AGENDA

Meeting - 5:00 p.m.

Wednesday, June 30, 2010

Sheraton Phoenix Downtown Hotel
Second Floor, Valley of the Sun Ballroom, Room D
340 North 3rd Street, Phoenix, Arizona 85004

Reception - 5:30 p.m.

Desert Peaks Awards - 6:15 p.m.

Sheraton Phoenix Downtown Hotel
Second Floor, Valley of the Sun Ballroom, Room A

**THE NEXT REGIONAL COUNCIL MEETING WILL BE HELD AT THE SHERATON PHOENIX
DOWNTOWN HOTEL AT THE TIME AND PLACE NOTED ABOVE.
(see enclosed driving instructions)**

The Regional Council meeting will be held in conjunction with the 2010 Desert Peaks Awards. MAG will host the Desert Peaks Awards reception at 5:30 p.m. The Awards presentation is scheduled to begin at 6:15 p.m. Parking will be provided by MAG in the hotel parking garage. Transit tickets will be available for those using transit.

Pursuant to Title II of the Americans with Disabilities Act (ADA), MAG does not discriminate on the basis of disability in admissions to or participation in its public meetings. Persons with a disability may request a reasonable accommodation, such as a sign language interpreter, by contacting the MAG office. Requests should be made as early as possible to allow time to arrange the accommodation.

If you have any questions, please call the MAG office.

c: MAG Management Committee

A Voluntary Association of Local Governments in Maricopa County

**MAG REGIONAL COUNCIL
TENTATIVE AGENDA
June 30, 2010**

COMMITTEE ACTION REQUESTED

<p>1. <u>Call to Order</u></p>	
<p>2. <u>Pledge of Allegiance</u></p>	
<p>3. <u>Call to the Audience</u></p> <p>An opportunity will be provided to members of the public to address the Regional Council on items not scheduled on the agenda that fall under the jurisdiction of MAG, or on items on the agenda for discussion but not for action. Citizens will be requested not to exceed a three minute time period for their comments. A total of 15 minutes will be provided for the Call to the Audience agenda item, unless the Regional Council requests an exception to this limit. Please note that those wishing to comment on agenda items posted for action will be provided the opportunity at the time the item is heard.</p>	<p>3. Information.</p>
<p>4. <u>Executive Director's Report</u></p> <p>The MAG Executive Director will provide a report to the Regional Council on activities of general interest.</p>	<p>4. Information and discussion.</p>
<p>5. <u>Approval of Consent Agenda</u></p> <p>Council members may request that an item be removed from the consent agenda. Prior to action on the consent agenda, members of the audience will be provided an opportunity to comment on consent items. Consent items are marked with an asterisk (*).</p>	<p>5. Approval of the Consent Agenda.</p>

ITEMS PROPOSED FOR CONSENT*

MINUTES

<p>*5A. <u>Approval of the May 26, 2010, Meeting Minutes</u></p>	<p>5A. Review and approval of the May 26, 2010, meeting minutes.</p>
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TRANSPORTATION ITEMS

*5B. American Recovery and Reinvestment Act (ARRA) Status Report

A Status Report on the American Recovery and Reinvestment Act (ARRA) funds dedicated to transportation projects in the MAG region details the status of project development. The report covers highway, local, transit, and enhancement projects programmed with ARRA funds and the status of project development milestones per project. At the May 26, 2010, meeting of the Regional Council, staff provided a presentation on the local projects bid savings, and reported that an accounting on the expenditure of the funds and a possible backup strategy for using bid savings might be available by the June 30, 2010, Regional Council meeting. Please refer to the enclosed material.

*5C. Update to Federal Functional Classification System

The City of Chandler has requested to classify Airport Boulevard as a Major Collector in the federal functional classification system. MAG concurrence is required in order for the Arizona Department of Transportation to proceed with classification of the facility. On May 27, 2010, the Transportation Review Committee recommended approval and on June 9, 2010, the MAG Management Committee recommended approval. Please refer to the enclosed material.

*5D. Project Changes/Amendments and Administrative Modifications to the Fiscal Year 2008-2012 MAG Transportation Improvement Program

The fiscal year (FY) 2008-2012 Transportation Improvement Program (TIP) and Regional Transportation Plan 2007 Update were approved by the MAG Regional Council on July 25, 2007. Since that time, there have been requests from member agencies to modify projects in the programs. ADOT is requesting the addition of three new projects, and there are requests to modify project costs for 12 transit projects. These were heard and recommended for approval at the May 27, 2010, Transportation

5B. Information, discussion, and possible action.

5C. Approval of the City of Chandler request to classify Airport Boulevard as a Major Collector in the federal functional classification system.

5D. Approval of amendments and administrative modifications to the FY 2008-2012 Transportation Improvement Program, and as appropriate, to the Regional Transportation Plan 2007 Update.

Review Committee. Twelve ARRA related project change requests were provided for the first time at the Management Committee meeting on June 9, 2010. The Management Committee recommended approval of the requested changes. Since the Management Committee met, there are 21 new project change requests related to ARRA projects and 12 requests for project changes related to transit projects funded with Federal Transit Administration (FTA) funds in FY 2010. All ARRA projects are being modified to reconcile the project costs from cost savings. Please refer to the enclosed material.

*5E. Interim Closeout of the Federal Fiscal Year 2010 MAG Federally Funded Program

There were 28 projects submitted to MAG for closeout funds, requesting \$7.9 million to be advanced and \$15.6 million of new funds. After calculating the estimated amount of federal surface transportation program (STP) and federal congestion mitigation air quality (CMAQ) funds available for the MAG region for federal fiscal year (FFY) 2010, and deducting the funding commitments for projects this fiscal year and future funding commitments for projects and regional programs identified in the Draft FY 2011-2015 MAG TIP, it is determined that the money available for FFY 2010 Closeout is \$2.204 million from projects that are requesting to be deleted from the TIP. Please see the attached memorandum, Table A that lists the projects requesting deferrals and deletions, and Table B, which lists the projects submitted for Closeout. Projects highlighted in Table B are the projects associated with the motion. On June 9, 2010, the MAG Management Committee recommended approval. Please refer to the enclosed material.

5E. Approval of additional projects to be deferred from FFY 2010 to FFY 2011 or later and additional projects requesting removal of federal funds; advancing the three projects submitted for priority I and IA projects to FFY 2010; allocating the \$2.204 million from deleted projects in FFY 2010 by the cities of Glendale and Mesa to fund Glendale's GLN09-609 with \$63,000 and fund Mesa's, MES08-604 and MES10-810 with \$2,141,307; and amending and modifying the FY 2008-2012 Transportation Improvement Program, and as appropriate, the Regional Transportation Plan 2007 Update.

AIR QUALITY ITEMS

*5F. Conformity Consultation

The Maricopa Association of Governments is conducting consultation on a conformity assessment for an amendment and administrative modification to the FY 2008-2012 MAG Transportation Improvement Program (TIP). The

5F. Consultation.

proposed amendment and administrative modification involve several projects, including a new Arizona Department of Transportation pavement preservation project on State Route 85, a new region wide Intelligent Transportation Systems project, a new Transportation Enhancement project located at Interstate-17 at the Central Arizona Project, and transit projects. The amendment includes projects that may be categorized as exempt from conformity determinations. The administrative modification includes minor project revisions that do not require a conformity determination. Please refer to the enclosed material.

GENERAL ITEMS

*5G. Approval of Electric Vehicle Charging Infrastructure Deployment Guidelines

On April 16, 2009, MAG entered into a Memorandum of Understanding with ECOtality and Nissan North America to support the adoption of electric vehicles in an effort to address environmental concerns in the region. On August 5, 2009, ECOtality North America was selected by the U.S. Department of Energy for a grant of approximately \$99.8 million to implement the largest deployment of electric vehicles and charging infrastructure in history. The ECOtality initiative, in partnership with Nissan North America, proposes to deploy charging infrastructure in major population areas, including Phoenix/Tucson. On March 17, 2010, ECOtality presented the Electric Vehicle Charging Infrastructure Deployment Guidelines version 2.0 document to the MAG Building Codes Committee (BCC). The MAG BCC reviewed the document and provided feedback to ECOtality. On May 19, 2010, ECOtality presented version 3.0 of the Electric Vehicle Charging Infrastructure Deployment Guidelines to the MAG BCC. The Deployment Guidelines document is intended to create a common knowledge base of electric vehicle (EV) requirements for stakeholders involved in the implementation of EV charging infrastructure. ECOtality's Deployment Guidelines provide the

5G. Approval of the Electric Vehicle Charging Infrastructure Deployment Guidelines document version 3.0 as guidelines to the implementation of infrastructure that will support and encourage the adoption of electric vehicles in the MAG region.

necessary background information for understanding EV requirements and the related codes, laws and standards for this effort. At the May 19, 2010, meeting of the MAG Building Codes Committee, the committee voted to recommend approval of the EV Charging Infrastructure Deployment Guidelines document version 3.0 as guidelines to the implementation of infrastructure that will support and encourage the adoption of electric vehicles in the MAG region. On June 9, 2010, the Management Committee recommended approval of the guidelines. Please refer to the enclosed material.

*5H. FY 2011 MAG Regional Plan to End Domestic Violence

The first MAG Regional Plan to End Domestic Violence was approved by the MAG Regional Council in 1999. The MAG Regional Domestic Violence Council, with more than 150 stakeholders, has developed a new Regional Plan responding to the changing dynamics created by the recession. Fifteen strategies in the areas of funding, training and education, coordination and collaboration, and services were developed to maximize impact with limited resources. The MAG Regional Domestic Violence Council recommended approval of the Regional Plan on May 6, 2010, and the MAG Management Committee recommended approval on June 9, 2010. Please refer to the enclosed material.

5H. Approval of the FY 2011 MAG Regional Plan to End Domestic Violence.

ITEMS PROPOSED TO BE HEARD

TRANSPORTATION ITEMS

6. Appointment of Members and Officers for the Transportation Policy Committee

On April 24, 2002, the Regional Council approved the composition of the Transportation Policy Committee (TPC). The composition of the TPC provided that the Central City and the seven largest cities have a seat on the TPC, and five seats be selected from the remaining cities and towns. Three of the five would be from areas that need to be represented to achieve

6. Appointment of the members and officers of the Transportation Policy Committee.

geographic balance, with the members selected from and by the under-represented geographic area and ratified by the Regional Council. Interstate 17 is used as a boundary in determining geographic balance. Two At-Large (geographically balanced) would be selected by the Regional Council. The three members to achieve geographic balance, the two At-Large members, and the Native American member serve for two years and are eligible for reappointment. The officers (Chair and Vice Chair) serve one-year terms with succession of positions occurring through the ascending order of officers. The Regional Council is requested to appoint the members of the TPC and the officer positions. Please refer to the enclosed material.

AIR QUALITY ITEMS

7. Update on Exceptional Events and MAG Five Percent Plan for PM-10

At the May 26, 2010, Regional Council meeting, staff provided an update on four exceptional events at the West 43rd Avenue monitor in 2008 and EPA's decision to disapprove the MAG Five Percent Plan for PM-10 for reducing dust pollution in the Valley. The MAG Regional Council directed staff to bring recommendations to the Executive Committee on obtaining legal advice and suggested that staff work with the Governor and the Arizona Congressional Delegation to stay the action of EPA until EPA corrects its flawed policy. On June 21, 2010, the MAG Executive Committee directed staff to retain legal counsel and other consultants to take administrative action needed regarding the EPA nonconcurrency on the four exceptional events at the West 43rd Avenue monitor in 2008 and the EPA's intent to disapprove the MAG Five Percent Plan for PM-10 for reducing dust pollution in the Valley. On June 23, 2010, EPA indicated that the consent decree has been lodged with the court, but still has to go out to public notice. EPA has to propose action on the MAG Five Percent Plan for PM-10 by September 3, 2010, and finalize the action by January 28, 2011. An update will be

7. Information and discussion.

provided to the Regional Council. Please refer to the enclosed material.

GENERAL ITEMS

8. Election of Regional Council Officers and Executive Committee Members

On May 26, 2010, the MAG Nominating Committee recommended a slate of officers for 2010-2011. The MAG officer positions are Chair, Vice Chair and Treasurer. The officers serve one-year terms with succession of positions occurring through the ascending order of officers. In accordance with the MAG Nomination Process, three At-Large members were also nominated to serve on the Executive Committee. According to the MAG Nomination Process, the Past Chair of the Regional Council also serves on the Executive Committee. Please refer to the enclosed material.

9. Request for Future Agenda Items

Topics or issues of interest that the Regional Council would like to have considered for discussion at a future meeting will be requested.

10. Comments from the Council

An opportunity will be provided for Regional Council members to present a brief summary of current events. The Regional Council is not allowed to propose, discuss, deliberate or take action at the meeting on any matter in the summary, unless the specific matter is properly noticed for legal action.

Adjournment

8. Election of the Regional Council officers: Chair, Vice Chair and Treasurer, and the three At-Large Members as members of the Executive Committee. According to the MAG Nomination Process, the Past Chair of the Regional Council also serves on the Executive Committee.

9. Information and discussion.

10. Information.

General Directions to the Hotel

From East

- Take the Loop 202 Freeway to the 7th Street exit.
- Turn left onto 7th Street and travel south for 0.5 miles to Fillmore.
- Turn right onto Fillmore to 3rd Street.
- Turn left onto 3rd Street, and the hotel will be on the right.

From North

- Take Interstate 17 south to Interstate 10 and travel eastbound on I-10 until you reach the 7th Street exit.
- Turn right onto 7th Street.
- Continue south for 0.5 miles to Fillmore.
- Proceed right on Fillmore to 3rd Street.
- Turn left onto 3rd Street, and the hotel will be on the right.

From West

- Take Interstate 10 east to the 7th Avenue exit.
- Turn right onto 7th Avenue and immediately get into the left lane.
- Turn left onto Roosevelt Street.
- Turn right onto 3rd Street, and the hotel will be on the right.

From South

- Take Interstate 10 west to the 7th Street exit.
- Turn left onto 7th Street and travel south for 0.5 miles to Fillmore.
- Turn right onto Fillmore to 3rd Street.
- Turn left onto 3rd Street, and the hotel will be on the right.

**Parking will be provided by MAG in the hotel parking garage.
Transit tickets will be available for those using transit.**

**MINUTES OF THE
MARICOPA ASSOCIATION OF GOVERNMENTS
REGIONAL COUNCIL MEETING**

May 26, 2010
MAG Office, Saguaro Room
Phoenix, Arizona

MEMBERS ATTENDING

Councilwoman Peggy Neely, Phoenix, Chair	Mayor James M. Cavanaugh, Goodyear
Mayor Thomas Schoaf, Litchfield Park, Vice Chair	Mayor Yolanda Solarez, Guadalupe
# Councilwoman Robin Barker, Apache Junction	Supervisor Mary Rose Wilcox, Maricopa Co.
# Mayor Marie Lopez Rogers, Avondale	Mayor Scott Smith, Mesa
* Mayor Jackie Meck, Buckeye	* Mayor Vernon Parker, Paradise Valley
Mayor David Schwan, Carefree	Mayor Bob Barrett, Peoria
Councilman Dick Esser, Cave Creek	# Mayor Arthur Sanders, Queen Creek
# Mayor Boyd Dunn, Chandler	* President Diane Enos, Salt River Pima-Maricopa Indian Community
Mayor Michele Kern, El Mirage	# Mayor Jim Lane, Scottsdale
* President Clinton Pattea, Fort McDowell Yavapai Nation	Councilwoman Sharon Wolcott, Surprise
# Mayor Jay Schlum, Fountain Hills	# Mayor Hugh Hallman, Tempe
* Mayor Ron Henry, Gila Bend	* Mayor Adolfo Gamez, Tolleson
* Governor William Rhodes, Gila River Indian Community	# Mayor Kelly Blunt, Wickenburg
Councilman Les Presmyk for Mayor John Lewis, Gilbert	Mayor Michael LeVault, Youngtown
# Mayor Elaine Scruggs, Glendale	# Felipe Zubia, State Transportation Board
	* Victor Flores, State Transportation Board
	# Roc Arnett, Citizens Transportation Oversight Committee

* Those members neither present nor represented by proxy.

Attended by telephone conference call.

+ Attended by videoconference call.

1. Call to Order

The meeting of the MAG Regional Council was called to order by Chair Peggy Neely at 5:00 p.m.

2. Pledge of Allegiance

Vice Chair Schoaf led the Pledge of Allegiance.

Chair Neely noted those participating by telephone: Mr. Roc Arnett, Councilwoman Robin Barker, Mayor Kelly Blunt, Mayor Boyd Dunn, Mayor Hugh Hallman, Mayor Michele Kern, Mayor Jim Lane, Mayor Marie Lopez Rogers, Mayor Art Sanders, Mayor Jay Schlum, Mayor Elaine Scruggs, and Felipe Zubia.

Chair Neely introduced Councilman Les Presmyk as proxy for Mayor John Lewis.

Chair Neely bid farewell to Mayor Art Sanders, who was participating in his last Regional Council meeting. She noted that Mayor Sanders has been a member of the MAG Regional Council since 2006. Chair Neely thanked him for his service to the MAG region and said that he had made a difference in the MAG community. The Regional Council applauded Mayor Sanders.

Mayor Sanders thanked the Regional Council and said that the Town of Queen Creek will be well represented by Mayor-elect Gail Barney.

Chair Neely noted the following items were at each place: For agenda item #9, replacement pages for A-19 through the end of the draft Work Program, and the addendum to the agenda (item #14) and supporting material.

Chair Neely requested that members of the public who would like to comment fill out a blue public comment card for the Call to the Audience agenda item or a yellow public comment card for Consent Agenda items, or items on the agenda for action. Parking garage validation and transit tickets for those who used transit to attend the meeting were available.

3. Call to the Audience

Chair Neely noted that public comment cards were available to members of the audience who wish to speak on items not scheduled on the agenda that fall under the jurisdiction of MAG, or on items on the agenda for discussion but not for action. Citizens are requested to not exceed a three minute time period for their comments. A total of 15 minutes is provided for the Call to the Audience agenda item, unless the Regional Council requests an exception to this limit. Those wishing to comment on agenda items posted for action will be provided the opportunity at the time the item is heard.

Chair Neely recognized public comment from Dianne Barker, who commented that Chair Neely would be concluding her term as Chair in June 2110. Ms. Barker complimented Chair Neely on a job well done, and she noted accomplishments during her term included the Sun Corridor, the Five Percent Plan for PM-10, the framework studies, and the stimulus projects. Ms. Barker noted that Chair Neely had also done a good job with the citizens and gave her time to them. She wished Chair Neely the best and said that she was well qualified to become mayor. Ms. Barker noted that she had come to the Regional Council meeting via bus and light rail. She commented that MAG might hear from citizens whose transit service got cut and she added that a lot of citizens have good ideas that might be implemented. Ms. Barker commented on the deficiencies of the regional freeway system even though \$5 billion has been invested since 1985. She encouraged multimodalism to help with solving transportation congestion. Ms. Barker spoke about a project that provided computers to remote villages in India. She said that the computers were installed at children's height and the children were able to learn and then pass along this knowledge to the other children in the village. Ms. Barker stated that this was an example that learning could happen in ways other than top down from a book. Chair Neely thanked Ms. Barker for her comments.

5. Approval of Consent Agenda

This agenda item was taken out of order.

Chair Neely noted that agenda items #5A, #5B, #5C, #5D, #5E, #5F, #5G, and #5H were on the Consent Agenda. She noted that no public comment cards had been received.

Chair Neely asked members if they had questions or requests to hear an item individually. No requests were noted.

Councilman Presmyk moved to approve the Consent Agenda. Mayor Barrett seconded, and the motion passed unanimously.

5A. Approval of the April 28, 2010, Meeting Minutes

The MAG Regional Council, by consent, approved the April 28, 2010, meeting minutes.

5B. American Recovery and Reinvestment Act (ARRA) Status Report

A Status Report on the American Recovery and Reinvestment Act (ARRA) funds dedicated to transportation projects in the MAG region details the status of project development. The report covers highway, local, transit, and enhancement projects programmed with ARRA funds and the status of project development milestones per project.

5C. Arterial Life Cycle Program Fiscal Year 2010 Regional Area Road Fund Closeout

The MAG Regional Council, by consent, approved advancing \$23.995 million in Arterial Life Cycle Program (ALCP) project reimbursements to 2010 for the fiscal year (FY) 2010 ALCP RARF Closeout, and amending the FY 2010 Arterial Life Cycle Program, the 2008-2012 Transportation Improvement Program, and Regional Transportation Plan 2007 Update, as necessary. The Regional Area Road Fund (RARF) Closeout Process was established in Section 260 of the Arterial Life Cycle Program (ALCP) Policies and Procedures approved by the MAG Regional Council. A financial analysis of ALCP revenues and expenditures as well as the ALCP bonding program was conducted. After reviewing the output of the analysis, MAG staff recommended that five eligible projects be reimbursed in the fiscal year 2010 ALCP RARF Closeout Process. The MAG Transportation Review Committee and the MAG Management Committee recommended approval.

5D. Fiscal Year 2010 MAG Highway Safety Improvement Program Projects

The MAG Regional Council, by consent, approved the listing of selected projects for fiscal year (FY) 2010 highway safety improvement program funds. A total of \$1 million in FY 2010 Highway Safety Improvement Program funds has been suballocated by the Arizona Department of Transportation (ADOT) to MAG for road safety improvements in the region. On March 1, 2010, ADOT informed MAG that the list of recommended safety projects was due by June 1, 2010, to enable timely obligation. Due to the short time available to obligate the funds, the MAG Transportation Safety Committee adopted a process that would result in three categories of road safety improvement projects that could

be obligated in the available time frame. On March 24, 2010, MAG staff announced a call for projects with a submittal deadline of April 9, 2010. Seventeen applications were received by MAG. The Transportation Safety Committee reviewed the applications and recommended a list of projects for funding. The Transportation Review Committee and the MAG Management Committee concurred with the recommendation of the Transportation Safety Committee. The FY 2008-2012 MAG Transportation Improvement Program amendment to include these projects is addressed in agenda item #5E.

5E. Project Changes – Amendments and Administrative Modifications to the FY 2008-2012 MAG Transportation Improvement Program

The MAG Regional Council, by consent, approved amendments and administrative modifications to the fiscal year (FY) 2008-2012 Transportation Improvement Program, and as appropriate, to the Regional Transportation Plan 2007 Update. The FY 2008-2012 Transportation Improvement Program (TIP) and Regional Transportation Plan (RTP) 2007 Update were approved by the MAG Regional Council on July 25, 2007. Since that time, there have been requests from member agencies to modify projects in the programs. The proposed project changes include amendments and administrative modifications to the FY 2008-2012 TIP for highway projects that include adding an ADOT pavement project on I-17, combining two Glendale pedestrian projects into a single project, changes to a Mesa project on Dobson Road, adding transportation enhancement funding projects in Phoenix and Wickenburg, and adding a series of safety projects in various MAG cities and towns contingent on approval of agenda item #5D. The project adjustments and new projects being added to the TIP are fiscally constrained and funding is available. The projects to be added or amended have been categorized as exempt from conformity determinations and the administrative modification includes minor revisions that do not require a conformity determination. The MAG Transportation Review Committee and the MAG Management Committee recommended approval of the amendments and administrative modifications.

5F. Update and Review of Project Deferral Requests for Federal Fiscal Year 2010 MAG Closeout

The MAG Regional Council, by consent, approved a list of projects to be deferred from federal fiscal year (FFY) 2010 to FFY 2011 or later, approval of a list of projects requesting to remove federal funds from the project, and make the necessary amendments and modifications to the 2008-2012 MAG Transportation Improvement Program, and as necessary to the Regional Transportation Plan 2007 Update. By April 29, 2010, member agencies submitted requests to defer or delete federal funds from projects for approximately \$14.5 million. The Federal Fiscal Year (FFY) 2010 Closeout process is following the Draft FY 2009 MAG Programming Principles. The attached memorandum explains the process and the requirements for requesting a project deferral. The Transportation Review Committee and the MAG Management Committee recommended approval of this item.

5G. Status of Remaining MAG Approved PM-10 Certified Street Sweeper Projects That Have Not Requested Reimbursement

A status report is being provided on the remaining PM-10 certified street sweeper projects that have received approval, but have not requested reimbursement. To assist MAG in reducing the amount of obligated federal funds carried forward in the MAG Unified Planning Work Program and Annual

Budget, MAG is requesting that street sweepers be purchased and reimbursement be requested by the agency within one year plus ten calendar days from the date of the MAG authorization letter.

5H. Conformity Consultation

The Maricopa Association of Governments is conducting consultation on a conformity assessment for an amendment and administrative modification to the FY 2008-2012 MAG Transportation Improvement Program (TIP). The amendment and administrative modification involve several projects, including a new ADOT pavement project on Interstate-17, two Glendale pedestrian projects combined into a single project, and a series of safety projects in various MAG cities and towns. The amendment includes projects that may be categorized as exempt from conformity determinations. The administrative modification includes minor project revisions that do not require a conformity determination.

4. Executive Director's Report

Dennis Smith, MAG Executive Director, reported on items of interest in the MAG region. He stated that the Arizona Department of Transportation submitted an application for a grant to study the Union Pacific Railroad Wellton Branch line from Phoenix to Yuma for potential future Amtrak service. He reported that on May 17, 2010, the MAG Regional Council Executive Committee approved providing \$60,000 to ADOT as matching funds for the grant. Mr. Smith informed the Council that since the Executive Committee meeting, ADOT has informed MAG that they will be increasing the total grant amount by using in kind contributions, which will be in excess of \$300,000. He stated that it is anticipated that a resolution will be brought forward soon to support renewing Amtrak service.

Mr. Smith stated that the Desert Peaks Awards will be held following the MAG Regional Council Annual Meeting on June 30, 2010, at the Sheraton Hotel in downtown Phoenix. He said that RSVPs are requested to be returned to MAG by June 18, 2010.

Mr. Smith stated that in April 2009, the Regional Council received a briefing regarding a concept paper that would provide incentives to regions that pass funding initiatives to address their transportation needs. He said that this concept paper was developed by MAG staff titled, "United States Department of Transportation and Metropolitan Planning Organizations: A New Partnership." Mr. Smith reported that Mark Muro and Rob Puentes from the Brookings Institution are releasing an article entitled, "Helping Those Who Help Themselves," which acknowledges the MAG concept paper. He added that Mr. Puentes will be at MAG next week to discuss reauthorization issues with the MAG Intergovernmental Representatives and will also be presenting at the National Multi-Modal Transportation Steering Committee meeting, sponsored by the City of Maricopa at the Sheraton Wild Horse Pass Resort.

Mr. Smith reported that the Sky Harbor International Airport Skytrain project received no funding for the federal transportation TIGER grants, but the City of Tucson received funding for its streetcar project. Mr. Smith stated that Kansas City received \$50 million from an application request of \$87.7 million that was submitted through that region's Council of Governments on behalf of a partnership of 13 agencies. He reported that the TIGER grants were awarded to projects based on their ability to provide economic benefits, improve safety and the condition of the existing transportation system, increase quality of life,

reduce greenhouse gas emissions, and demonstrate strong collaboration among a broad range of participants. Mr. Smith noted that the Kansas City region represents 1.7 million in population and the MAG region represents four million. He commented that working collectively appears to be more successful than working individually.

6. American Recovery and Reinvestment Act Update and Guidance

Eric Anderson, MAG Transportation Director, briefed the Regional Council on the American Recovery and Reinvestment Act (ARRA) funds for local projects and guidelines. He said that approximately \$105 million in ARRA funds were allocated to the MAG region for 59 local projects. Mr. Anderson then provided the status of these projects. He stated that the bids for the last of the projects are to be opened by the end of May 2010. Mr. Anderson remarked that this region has done a tremendous job to get the projects out the door and get people to work. He added that this could not have been accomplished without the assistance of the Arizona Department of Transportation (ADOT) and the Federal Highway Administration (FHWA).

Mr. Anderson then explained the bid and deobligation process by saying that if bids are higher than the estimate, jurisdictions will need to identify the funding required to complete the project, or reduce the scope of the project. He noted that if bids are below estimate, a jurisdiction may request a change order to add to a project to utilize the funds, however, the requested change must be consistent with the approved environmental clearance, must not require any new right of way or utility clearances, and must be consistent with the current scope of the project, i.e., no new work elements. Mr. Anderson advised that the project savings also may be applied to an existing ARRA project, or to another eligible project, provided that a project meets the eligibility requirements.

Mr. Anderson stated that in order to apply savings to an existing project, it must meet the eligibility requirements, a jurisdiction must have \$200,000 or more in savings, it must be able to obligate in time, and it is through or in the final stages of federal clearances through ADOT. Mr. Anderson displayed these criteria on a slide and noted that the \$200,000 or more in savings requirement had been stricken through due to Management Committee action, but not removed pending Regional Council action. He also added that if a project has not yet started this process, it is too late to begin.

Mr. Anderson reviewed the schedule for ARRA funds: March 2, 2010, was the deadline for all MAG region projects to be obligated; May 30, is the deadline for all MAG project bids to be opened; June 18, 2010, is to allow ADOT and Federal Highway Administration time to conduct formal process to deobligate the bid savings; July 30, 2010, is the ADOT deadline for all required clearances and full bid packages.

Mr. Anderson stated that MAG and ADOT staff met that day (May 26) and discussed the ARRA funded projects. He said that as of May 26, or at least by the end of this week, a project must be in approved TIP/STIP, it must have an ADOT TRACS number, and the environmental document must be in the ADOT review process. He advised that an environmental review typically takes six weeks and includes agency consultation. Mr. Anderson stated that by July 15, 2010, right of way and utility clearances must be signed off by ADOT. Mr. Anderson stated that by July 30, 2010, the executed Joint Project Agreement (JPA) between ADOT and the jurisdiction must be signed, local funds must be on deposit

with ADOT, and plans, specifications and details must be complete. He remarked that this is a significant amount of work to do on a project, and MAG had a commitment with ADOT to do the projects quickly. Mr. Anderson stated that ADOT might accommodate a couple of new, simple projects, but not a large number. He stated that staff will work with member agencies on this.

Mr. Anderson displayed a modified recommendation for the allocation of bid savings, and he noted that the changes from the policy adopted by the Regional Council on January 27, 2010 were indicated in ~~strikeout~~ and double underline. “The local agency with the ARRA project savings will have local discretion to move the project savings to another existing ARRA project in that jurisdiction; and/or swap the ARRA funds with ADOT-STP funds and move the project savings to an eligible project ~~that is above \$200,000 and~~ if all of the required documents and clearances for the project can be completed by July 30, 2010 ~~can obligate before September 30, 2010~~, including new projects. Any jurisdiction that cannot meet the ~~\$200,000 threshold and obligation~~ deadline of ~~September 30~~ July 30, 2010 will return the project savings to the regional pool for reallocation.”

Mr. Anderson noted that he made the changes indicated in double underline due to the requirement that projects be obligated on time. He said that this is a very important piece. Mr. Anderson stated that the deadline was originally September 30, but after discussions with ADOT, it was determined that all jurisdictional work must be completed by July 30 to allow time for ADOT to finish up their books.

Mr. Anderson pointed out that the Management Committee recommended elimination of the \$200,000 threshold included in the adopted guidelines because smaller member agencies that received only \$500,000 to \$700,000 in ARRA funds, but have \$150,000 in savings feel that this is represented a significant portion of their allocation and they should be able to spend the savings on an eligible project. He noted that Nate Banks from the Federal Highway Administration also was in attendance at the meeting to assist with answering questions.

Chair Neely thanked Mr. Anderson for his report and asked members if they had questions.

Mayor Smith asked what a community would need to do between now and July 30 if it had project savings of say, \$170,000. Mr. Anderson replied that the change in policy would allow a jurisdiction to use the \$170,000 savings on a project that is ready to go, is eligible and meets all of the requirements.

Mayor Smith asked the disposition of the bid savings if a project had not met all of the requirements by July 30. Mr. Anderson replied that the plan is to move the leftover ARRA funds to highway projects for the benefit of the region.

Mayor Smith remarked that he hoped all jurisdictions would be able to use their ARRA funds. He stated that the concern from the beginning was to ensure that the funds were expended, not how much each jurisdiction received. Mayor Smith asked if the Regional Council would be provided an accounting of the final distribution of the ARRA Local funds. He said that he would be interested in seeing how much of the funds ended up being applied to the highway program. Mr. Anderson replied that staff anticipates that by the June 30 Regional Council meeting, the tabulation of bid savings could be completed, and they might know which projects will be able to meet the July 30 deadline, and the amount that might go to a regional highway project.

Vice Chair Schoaf stated that the City of Litchfield Park was allocated approximately \$650,000, of which \$145,000 remained. He said that he supported the policy when it passed because they had a \$450,000 project that had completed all of its environmental work, plans, and requirements to which they could apply the \$145,000 and pay the difference. However, when the City went to ADOT to get a TRACS number, ADOT told city staff that according to MAG's policy, the city did not have the minimum \$200,000 and would have to return the \$145,000 to the regional pool of funds. Vice Chair Schoaf stated that the City took this through the Management Committee and asked the policy be revised because it was not fair to smaller communities. He added that under the approved policy, the City would lose 25 percent of the little amount it was given to begin with.

Mayor Schwan stated that the Town of Carefree is in a similar situation. He said that their original project was scaled back and if the change to the guidelines is approved, the Town would be able to do the project they planned all along. Mayor Schwan expressed his support for changing the guidelines.

Mayor Smith stated that it would be awesome for every jurisdiction to use all of its allocation. He commented that meeting the requirements for a process such as this is easier for a large city such as Mesa that has staff that do this every day, but it could be challenging for smaller communities. Mayor Smith stated that the City of Mesa will assist smaller communities with paperwork, etc. He added that he just did not want to leave any money on the table that would be returned to Washington, D.C.

Councilmember Esser stated that the Town of Cave Creek is in a similar situation as a result of a favorable bid. He expressed that he supported what Vice Chair Schoaf suggested and Mr. Anderson explained.

Chair Neely stated that Phoenix shared concerns at the Management Committee meeting. She indicated that she would be supporting the elimination of the \$200,000 threshold. Chair Neely stated that she believed that MAG needs to listen to the smaller communities and help them with their unique needs. She stated that it is also important to note that MAG does not control the destiny of the ARRA funds, and to obligate them will require the cooperation of ADOT and FHWA. Chair Neely stated that ADOT and FHWA have an extreme workload and should be commended for all they have accomplished thus far. She said that the goal, however, is to not return any of these funds to the federal government. Chair Neely stated that if a project is not going to be able to obligate in time, the funds will need to be placed on a freeway project that can utilize them. She commented that this could mean that some member agencies could expend funds getting projects ready without completing the federally prescribed process.

With no further discussion, Chair Neely called for a motion. Mayor Schwan moved approval that the local agency with the ARRA project savings will have local discretion to move the project savings to another existing ARRA project in that jurisdiction; and/or swap the ARRA funds with ADOT-STP funds and move the project savings to an eligible project if all of the required documents and clearances for the project can be completed by July 30, 2010, including new projects. Any jurisdiction that cannot meet the deadline of July 30, 2010, will return the project savings to the regional pool for reallocation. Councilmember Esser seconded.

Supervisor Wilcox expressed her support for showing flexibility to the smaller communities, and asked for clarification that those ARRA Local funds not utilized will then go to the region. She noted that the

County has old US-60 bridge projects that were ready to go. Mr. Anderson replied that he would talk with County staff about the status of the projects. He stated that the plan is to push any unobligated funds to regional freeway projects that are federally eligible and have been cleared. He added that there will be no other option for any project not approved and not ready to go by the end of July. Mr. Anderson mentioned that staff anticipates the report on the expenditure of funds and a backup strategy may be available by the June 30 Regional Council meeting, which will be the last opportunity for the Regional Council to weigh in on how they want to use the money, because there is no time to apply the funds anywhere else.

With no further discussion, Chair Neely called the question, which passed unanimously.

14. Update on Exceptional Events and MAG Five Percent Plan for PM-10

This agenda item was taken out of order.

Lindy Bauer, MAG Environmental Director, reported that on May 25, 2010, the Environmental Protection Agency (EPA) Region IX Administrator conducted a meeting to announce that EPA would not concur with the Arizona Department of Environmental Quality (ADEQ) documentation regarding four exceptional events at the West 43rd Avenue monitor. She noted that EPA had signed a letter stating this on May 21, 2010, without MAG's knowledge.

Ms. Bauer stated that the four exceedances will count as a violation at the West 43rd Avenue monitor and there will be no first year of clean data at the monitors and the region will not be in attainment of the PM-10 standard by 2010. Ms. Bauer explained that attainment of the PM-10 standard requires three years of clean data (in 2008, 2009, and 2010). She added that if the EPA had approved even one exceedance as an exceptional event, the MAG region would have had its first year of clean data.

Ms. Bauer advised that at the meeting, the EPA Region IX Administrator acknowledged that the EPA Exceptional Events Rule has problems and said it is flawed, but EPA was forced to use it. She reported that the Western States Air Resources Council (Westar), which is composed of 15 states, has sent a letter to EPA that identified several issues with the implementation of the rule. She noted that states are confused about what to submit for documentation. Ms. Bauer commented that she thought EPA was concerned over lawsuits since the Exceptional Events Rule has been legally challenged.

Ms. Bauer advised that the EPA intends to disapprove the MAG 2007 Five Percent Plan for PM-10, which includes 53 aggressive measures. She noted that the timing of the disapproval action is unknown at this time. Ms. Bauer added that the EPA is negotiating with the Arizona Center in the Public Interest on the deadline for disapproval action and could not disclose to MAG when they would have to take the disapproval action. She commented that the meeting with the EPA Administrator was disturbing. She said that following the meeting, the EPA sent a 40+ page document that MAG staff is currently reviewing.

Ms. Bauer indicated that EPA had previously expressed concern over four high wind exceptional events in 2008 (March 14, April 16, April 30, and June 4), when the West 43rd Avenue monitor exceeded the PM-10 standard. She reported that additional information was prepared by the MAG consultant, Sierra

Research, which is a nationally renowned firm, to supplement ADEQ's exceptional event documentation. Ms. Bauer stated that MAG's products submitted to support ADEQ's documentation to EPA included surface roughness and friction velocity maps, a table with maximum anthropogenic PM-10 contributions based on land use, a paper explaining why PM-10 concentrations are higher at the West 43rd Avenue than downwind monitors, and graphs showing winds were exceptionally high during those four days.

Ms. Bauer then explained the consequences of plan disapproval action. She said that a conformity freeze would occur 30 to 90 days after the final disapproval is published in the Federal Register, after which, only projects in the first four years of the conforming Transportation Improvement Program (TIP) and Regional Transportation Plan (RTP) can proceed. Ms. Bauer advised that means no new TIPs, RTPs or projects until a new Five Percent Plan is submitted that fulfills the Clean Air Act requirements and EPA finds the conformity budget adequate. She remarked that this is new to MAG and she added that MAG has not been threatened with a freeze before.

Ms. Bauer stated that the first Clean Air Act sanction would be imposed if the problem is not corrected within 18 months from the disapproval action, which would be tighter controls on major industries (2:1 offsets in emissions). She said that 24 months from the disapproval action would be the loss of federal highway funds (\$1.7 billion in the Draft FY 2011-2015 MAG Transportation Improvement Program), and a federal implementation plan would be imposed. Ms. Bauer stated that the imposition of highway sanctions may trigger a conformity lapse and major projects in the \$7.3 billion Draft TIP could not proceed. She added that some projects, such as transit and safety projects, would be exempt from the highway sanctions.

Ms. Bauer stated that as a result of the disapproval of the Five Percent Plan for PM-10, a new emissions inventory would be needed. She noted that this was last conducted in 2005, and since then the major downturn in the economy has changed the mix of sources. Ms. Bauer stated that the new 2008 inventory is anticipated to be completed by Maricopa County by June 1, 2010. She said that more measures may be needed to reduce emissions by five percent per year until attainment, as measured at the monitors, and she advised that it is becoming increasingly difficult to find additional measures. Ms. Bauer stated that the modeling in the Plan will need to be revised and three years of clean data at all PM-10 monitors will be needed for attainment. She expressed concern for the seven exceedances in 2009, which ADEQ had recently indicated were exceptional events.

Ms. Bauer then addressed the data collection effort underway in the vicinity of the West 43rd Avenue monitor, which includes air quality and meteorological monitoring and analysis to identify the sources contributing to the elevated PM-10 levels under windy conditions, including nearby sources, unique soil conditions, and transport from outside the area. Ms. Bauer displayed a map of temporary monitors. She said that it is important to complete this data collection, address Plan issues as quickly as possible, prevent violations at the monitors, obtain a conformity finding on the new Draft 2011-2015 TIP and RTP 2010 Update (which is out for public review), seek the assistance from the Governor and Congressional Delegation to suspend the EPA action on the Five Percent Plan for PM-10 until the flawed Exceptional Events Rule is fixed, and explore a possible legal challenge.

Chair Neely thanked Ms. Bauer for her report and asked members if they had questions.

Mayor LeVault asked for clarification of an exceedance day. He asked if the violation has to occur over the entire 24-hour period or it is the result of spikes throughout the day. Ms. Bauer replied that the PM-10 standard is a 24-hour average.

Mayor Smith asked if that average was a weighted average. Ms. Bauer replied it was a regular average. Mayor Smith asked if a reading was zero for 23 hours and eight for one hour, the reading would be the average of those numbers. Ms. Bauer replied that the PM-10 standard is a 24-hour average.

Mayor Barrett asked if any other entities in the nation had this happen due to air quality issues. Ms. Bauer replied that the EPA rarely has to impose the loss of federal highway funds. She stated that the state of Pennsylvania came close over its vehicle emissions testing program. The EPA put its federal highway funds in escrow, but the problem was resolved. Ms. Bauer reported that the state of California has been threatened with sanctions, but the problems were remedied before that happened.

Mr. Smith stated that at the meeting with the EPA Administrator, he referred to MAG's CMAQ funds as funds to be used for air quality projects. Mr. Smith remarked that there is a belief that there is a project at the site of the monitor that is eligible for CMAQ or STP funds and could solve this problem that MAG has neglected to implement. Mr. Smith stated that MAG has \$7 billion riding on this, and if people think MAG has a project that it has not done, they are mistaken. Mr. Smith stated that MAG hired the best environmental consultant it could find to determine the cause of the problem at the West 43rd Avenue monitor. He recalled the presentation at a previous Regional Council meeting that mentioned the fine silt picked up by the high winds and the surface roughness. Mr. Smith stated that MAG has its technical experts and the EPA has theirs, and what is potentially at risk is \$7 billion in the MAG TIP. Mr. Smith stated that the region is an area with 64,000 pending or foreclosed homes. He expressed that it baffled him how EPA could find no justification with the compelling information MAG gave them to acknowledge that one of those readings could be an exceptional event. Mr. Smith stated that when he and Ms. Bauer went to the EPA Administrator's meeting they thought they were going to hear good news regarding the technical analysis; instead, they saw on-screen: "Final decision by the EPA," and by the time they had returned to the MAG office, the EPA had already issued its press release. Mr. Smith indicated that EPA had already made a preliminary agreement with the Arizona Center for Law in the Public Interest. Mr. Smith said that he mentioned to the EPA representative that in his mind, a collaborative effort would have been to give to MAG EPA's technical report and the MAG consultant would have reviewed it to see if there were reasons for improvement. Then a decision would be a decision, but that is not how this worked.

Mayor Hallman stated the importance of ensuring that the public understands the concept of the 24-hour average and how it could be impacted by something like a dust devil carrying sufficient silt from the riverbed passing over the monitor. He commented that the EPA's approach to this is absurd given this monitor's position and the environment surrounding it.

Mayor Hallman asked what led MAG staff to believe that there would be good news at the meeting with EPA. Mr. Smith replied that there were a series of technical meetings attended by EPA and MAG staff at which MAG's consultant, Bob Dulla from Sierra Research, presented research on high winds and surface roughness that the monitor just past the West 43rd Avenue monitor was not exceeding the standard. He said that EPA did not indicate displeasure with MAG's information.

Mayor Hallman asked the last time the EPA staff indicated they understood there were justifications for the West 43rd Avenue monitor readings. Ms. Bauer replied in early in April and in May. Mayor Hallman remarked that this occurred as late as early May, and he wondered if anyone has thought through who may have acted to influence the EPA decision or any activities, because it starts to sound like a purely politically influenced decision. Mayor Hallman stated that MAG needs to make it public if someone wanted to punish Arizona or Maricopa County. He said that he knew how hard MAG staff had been working and he also knew the reality of what goes on with violations. Mayor Hallman stated that he had long been an advocate of what needs to be done with the Loop 101/US 60 crossover and why it should not be super elevated ramps. Mayor Hallman remarked that the absurdity of this decision, after all of the detailed data that have been presented over the past year, is quite disturbing.

Chair Neely stated that she thought there were several elements to this, and she thought it happened before the April meetings. She advised that MAG does not know if the reason is due to visits to Region IX, the lawsuits, or the pressure on the Administration in Washington, D.C., to get tough on environmental issues. Chair Neely said that MAG needs to take an aggressive, tough stand. She remarked that it is important to get legal advice and strong people who know how to work through the EPA system at Region IX. Chair Neely commented that cooperation is great, but this is one-sided and the only one cooperating is MAG. She stated that jobs are at stake here and this could limit the types of businesses in the community and MAG could lose its transportation program. Chair Neely stated that the question is why the EPA should be allowed to make decisions based on a program that EPA has stated publicly is flawed. She said that the EPA does not seem to want to accept the data from the MAG consultant, and she questioned whether it was because it has only had to deal with issues in the East and MAG is in a different climate in the West. Chair Neely urged aggressively pursuing this and said that MAG cannot afford to wait. She encouraged finding all the resources possible. Chair Neely expressed that she thought it was a good idea to get with the Governor and the Congressional delegation and tell our story, but at the same time, work through consultants and attorneys because their expertise is needed to get this done.

Mayor Cavanaugh asked the status of exceedances in 2010. Ms. Bauer replied that there have been no exceedances in 2010. She said that the region had a lot of rain that helped stabilize the soil and increased the vegetation in areas that might not have had vegetation growth, and put water in the rivers. Mayor Cavanaugh commented that a good 2010 is good for MAG's argument.

Mayor Lane expressed that he agreed with Mayor Hallman's comments and he said that it is disturbing to follow a data-based argument and suddenly it makes a turn. He remarked that even though the region has a good 2010 at the monitors, it will still be subjected to the denigration of its programs and plans, and the loss of funds. Mayor Lane said that he agreed with continuing to follow the course, but he is more a proponent of taking an offensive look and taking time to study and stating concerns publicly. He expressed his concern for the exposure for what appears to be a potentially political move.

Supervisor Wilcox expressed her agreement with the previous comments. She asked if there was a formal appeal process and also if MAG would be joining Westar in questioning the exceptional events. Ms. Bauer replied that after EPA publishes its technical document as to why it does not consider these exceptional events, MAG will be able to comment. She said as far as an appeal, the EPA Administrator said this decision was final and indicated that he could not talk about the plan disapproval until the

agreement with the Center is lodged in court. Ms. Bauer stated that staff will research what types of appeals are available.

Mayor Smith asked if this situation came down to one monitoring site in the entire county. Ms. Bauer replied that was correct. Mayor Smith asked if complete non-compliance is as simple as four days of exceedance on the 24-hour average at a monitor. Ms. Bauer replied that was correct, it was a violation. Mayor Smith agreed with Mayor Hallman that this was absurd when a sixth-grader could look at the map, see the monitor downwind from the open desert and a dry riverbed, and realize the cause. He said that he wondered what the technical staff at EPA were doing all day figuring out how this could be a non-compliance, which could result in a \$7 billion hit to this region. Mayor Smith stated that he did not know how this is anything other than political. He commented that it is surprising there have not been more than four violations in an area that is surrounded by open desert and a dry riverbed. Mayor Smith remarked that when he looks at the graphs and data, he sees the absurdity of San Francisco, where there is the bay and water in the air. Mayor Smith stated that he thought MAG needed to take all of the steps available, politically and legally, to counteract something that is off the charts.

Councilmember Esser asked if a joint resolution of the Regional Council would be appropriate. Chair Neely responded that a resolution could be one of many solutions.

Vice Chair Schoaf stated he wanted to join Mayor Hallman and Mayor Smith in expressing the absurdity of the decision. He said that the West Valley has been dealing with the EPA for more than 20 years on what once was a very small Superfund site of less than one square mile but now covers 15 to 20 square miles and is affecting the water supplies in several West Valley cities. Vice Chair Schoaf stated that EPA's attitude when not in the political arena, is to allow an admitted polluter to do nothing to clean up a problem. He added that the way EPA has acted with MAG's situation is disappointing.

Vice Chair Schoaf stated that in light of the serious consequences to the region of plan disapproval by the EPA, he would move that the Regional Council needs to evaluate all of its options to postpone or prevent disapproval of the MAG Five Percent Plan for PM-10 or the triggering of Clean Air Act sanctions, specifically, MAG is directed to present a detailed listing of available actions that the Regional Council can take toward those ends, such as seeking the assistance of the Governor and its Congressional Delegation to stop the EPA action until there is an Exceptional Events Plan that is not flawed; to look into appeals or other legal challenges; to look into whether the joint actions of the Regional Council and taking this argument to the public will be helpful; to look into whether it would be helpful to withdraw portions of the Plan, or to prepare and submit a revised Plan. The listing should also include the applicable timeframes for implementation of all of these actions. To develop that list, MAG staff is authorized by this motion to retain and consult as needed with experts regarding the Clean Air Act and its implementing regulations, experts with experience negotiating with EPA Region IX and/or EPA Headquarters in Washington, D.C., and experts regarding strategies to identify and address sources of PM-10 emissions to the extent MAG does not already have those experts on its staff or consulting with us. Councilmember Esser seconded.

Chair Neely called for discussion of the motion.

Mayor Hallman proposed adding to the motion that staff report to the Executive Committee at its next meeting to further discuss this issue and have a presentation on a positive plan of action.

Vice Chair Schoaf, as maker of the motion, agreed with the additional language. Councilmember Esser, as second, also agreed.

Mayor Cavanaugh said that he would support the motion, but he wanted to express some opposition to the “take no prisoners” approach. He said that he thought moving out aggressively initially could be harmful. Mayor Cavanaugh stated that MAG has its own expert in Ms. Bauer, who makes a good impression on people, and a diverse delegation representing it in Washington, including two of the most influential senators. Mayor Cavanaugh said that he thought it made sense to do this offline and have those people with expertise and influence in Washington make a presentation to the head administrators of the EPA before anything else is done.

Mayor Hallman stated that he did not disagree with Mayor Cavanaugh, and what he suggested could be an option discussed by the Executive Committee and accomplished without great expense, but with the likely timeframe with the administrative action and appeal, there needs to be a quick examination of options at the next Executive Committee meeting: the timeframe, consultants and lawyers and their costs. Mayor Hallman expressed that he agreed that the first step should be the immediate engagement of our delegation and connections in Washington to put pressure on the opposite side of the argument.

Vice Chair Schoaf stated that he agreed with Mayor Cavanaugh’s comments that there is a high probability that this could be solved in a political manner and offline. He stated that parts of the motion do that by seeking the assistance of the Governor and the Congressional delegation, but the alternative steps are complicated and potentially expensive and MAG needs to be ready to implement them if the other action is not successful.

Mayor Cavanaugh agreed with Vice Chair Schoaf.

Mayor Smith stated that Ms. Bauer’s response about the effects in 2010 of water in the river, rain creating a crust, and vegetation, was a simple, non-technical answer, but very powerful and defines the message that needs to be conveyed.

Councilmember Wolcott stated that she supported Mayor Cavanaugh’s caution, and she said that this is a lot to digest on the fly without becoming emotional. Councilmember Wolcott stated that she thought MAG needed to take an aggressive approach, but she was concerned with the political nature this appears to have taken. She expressed that her major concern was the surprise – being told that this was a final decision with no notice, and she found that particularly troubling. Councilmember Wolcott stated that she felt that MAG needed to proceed with caution, but aggressively.

Supervisor Wilcox expressed her agreement and added that this region has one of the best Congressmen, Ed Pastor. She agreed that the first step is to put together an action plan, but also to call Congressman Pastor and request that he set up a meeting with the EPA Administrator to see if anything can be done to calm this down. Supervisor Wilcox stated that she would be glad to facilitate with Congressman Pastor and she also offered the assistance of the County’s lobbyist in Washington. Supervisor Wilcox

expressed that she was upset because the County and the City of Phoenix spent a lot of money paving the dirt roads and shoulders in that area to control the dust problem. She added that she thought the meeting with Congressman Pastor should be a part of the strategy before this hits the Federal Register.

Mayor Smith stated that MAG has a public relations issue because the EPA news release condemns the County for non-compliance. He commented that the details show a much different story than what the EPA laid out. Mayor Smith cautioned against trying to justify what EPA put out there that MAG is not doing its job with the CMAQ funding. He said that the EPA has not reflected the real conditions in the Valley, but the public does not know that. They hear that we are out of compliance and that is all they hear.

Chair Neely called the question, and the motion passed unanimously.

Councilman Presmyk called for a quorum check and a quorum was present.

7. Acceptance of Commuter Rail Planning Studies and Amtrak Update

Marc Pearsall, MAG Transit Planner, presented three MAG commuter rail studies, the Commuter Rail System Study, the Grand Avenue Commuter Rail Corridor Development Plan, and the Yuma West Commuter Rail Corridor Development Plan, for acceptance. Mr. Pearsall reported that since December 2008, these studies had been vetted by MAG member agencies and the general public at more than 60 meetings and stakeholder presentations. He added that the item also was the agenda to revise the corridor ranking that was included in the Commuter Rail System Study upon the completion of update regional socioeconomic forecasts or relevant passenger rail studies.

Mr. Pearsall stated that the study purpose was to evaluate the feasibility, costs, constraints and operating scenarios of implementing commuter rail. He further elaborated on the Grand Avenue and Yuma West corridors and their operations phases: Phase A opening day service, Phase B mid-level service and Phase C mature system service. He added that the recommended overall most productive system map is a four-line, X-shaped system with nearly 18,000 daily boardings.

Mr. Pearsall explained that the study findings recommend a prioritization for the implementation of startup service in the two most productive corridors. He said that the first recommended corridor to receive service was Segment #1, the East Valley-Union Pacific Phoenix Subdivision at 6,450 daily boardings, and the second corridor recommended to receive service was Segment #2, the BNSF Railway interlined with East Valley-Union Pacific Phoenix Subdivision for a combined total of nearly 10,000 daily boardings. Mr. Pearsall stated that beyond the two initial corridors, there was no one outstanding performer in other three corridors: Tempe, Chandler, Yuma-West. He said that the study's corridor prioritization does recommend a full system build-out, but there was no definitive priority on how the remainder of the corridors should be phased for service. Mr. Pearsall added that considerations for future phasing and system build-out would include development patterns, changes in travel demand, community support, potential integration with intercity rail and owner railroad support.

Mr. Pearsall noted that potential future corridor extensions, including using existing railroad lines, historic railroad corridors and new rights of way parallel to proposed MAG region freeways were

possible beyond the 2035 timeframe. He noted that corridors such as the Hassayampa Valley-Buckeye, Hidden Waters-Gila Bend, Hidden Valley-Mobile, Tempe-Maricopa extension, Chandler-Sacaton-Coolidge extension, Queen Creek Florence extension, and Superstition Vistas-Apache Junction were all listed as potential candidates for future commuter rail service.

Mr. Pearsall discussed the study's eight recommended implementation steps. 1) Continued coordination with ADOT and railroads, 2) Determine liability and indemnification statutes, 3) Regional Sustainable Transportation and Land Use Integration Study (FY 2011), 4) Identify local funding, 5) Develop and implement governance plan, 6) Railroad agreements, 7) Design and construction and, 8) Operation. He noted that these implementation steps give a road map toward commuter rail service, should the MAG region decide that commuter rail is a viable investment. Mr. Pearsall pointed out that once the local funding source is identified, it is about three to five years before a train begins operating.

Mr. Pearsall stated that this item was on the agenda for action to 1) Accept the findings of the Grand Avenue Commuter Rail Corridor Development Plan, Yuma West Commuter Rail Corridor Development Plan, and Commuter Rail System Study, and 2) Revise the corridor ranking included in the Commuter Rail System Study upon the completion of update regional socioeconomic forecasts or relevant passenger rail studies.

Mr. Pearsall concluded his presentation with a slide depicting MAG's possible future with commuter rail, specifically a photo-simulation displaying a mockup of the "Sun Runner" commuter train sitting in downtown Phoenix.

Mr. Pearsall then introduced Mr. Lonnie Blaydes from the Commuter Rail Project team. Mr. Pearsall explained that Mr. Blaydes had more than 35 years of experience in the freight and passenger rail industry and has successfully brokered negotiations for new commuter rail system in areas such as Denver, Dallas, Seattle and Albuquerque.

Mr. Blaydes explained the next steps for implementation of commuter rail. He said that each region is different, but he would compare the factors needed for implementation to a three-legged stool: a railroad agreement, funding, and a local champion. Mr. Blaydes stated that usually the champion comes first to assist in obtaining funding, which is needed in order to be taken seriously by the railroads. He stated that when approached by a jurisdiction wanting to use the rail for commuter rail, the railroads always want to know if the request is "real," which means, is there funding and a political will.

Mr. Blaydes stated that a champion could be an individual, organization or event to galvanize local, regional and state support for implementation. Mr. Blaydes described that in New Mexico, it was Governor Richardson who promoted the Rail Runner through adding language to the GRIP legislation to include funding. Mr. Blaydes noted that the funds flowed through the New Mexico Department of Transportation, but Governor Richardson had more confidence in the Mid-Region Council of Governments, who then implemented the service. He said that Governor Richardson always pushed hard for commuter rail and would step in when delays were encountered. Mr. Blaydes stated that Seattle had funding for the Seattle Sounder, but Senator Patty Murray championed getting a railroad agreement. Mr. Blaydes stated that he was not involved Minnesota Northstar, but Councilmember Sharon Wolcott knows a lot about its implementation because she served in the Minnesota Legislature at the time. He

noted that the community kept it alive until funding became available, at which time they moved to implement it quickly. Mr. Blaydes stated that with the California Metrolink in Los Angeles an event, the 1994 earthquake, was the catalyst because the highways were devastated and that helped emphasize the value of commuter rail to the community. Mr. Blaydes stated that the TriRail in Florida was begun as a reliever during the reconstruction of I-95, and 15 years later, it is still operating. Mr. Blaydes advised that funding is critical; without it, no one will take you seriously.

Chair Neely stated that Mr. Blaydes stressed the importance of funding. She asked what amount would demonstrate to the railroads that a region was serious. Mr. Blaydes responded that people usually focus on the capital costs but should look at the ongoing operating costs. He said that if federal funding is being sought, the formula used puts a premium on operating costs. Mr. Blaydes added that there are ways to get capital funding partners, but not as easy to get funding for operational costs.

Councilmember Wolcott stated that she agreed with a lot of what had been presented, but in her experience with Northstar, it takes a state or region to say they want commuter rail and to educate the public so that they know what is at stake. She said that an integrated system with more than one mode is needed to get people excited about it and then a funding source for operations is located. Councilmember Wolcott stated that Mr. Blaydes was correct – the easy part is the capital costs, the difficult part is the operating costs. She said that in Minnesota, the Legislature approved the funding source, but in Arizona, because the voters would have to approve it, the education piece is important from the beginning.

Mayor Hallman stated that he was grateful commuter rail had been moved this far along in this process. He said that Tempe has been pushing for commuter rail for six years. Mayor Hallman stated that the southeast line will never show great traffic until the ridership is completed to the destinations where it makes sense, with the Tempe to Chandler corridor, the ultimate destination of the corridor is to continue to Maricopa on the abandoned rail lines. He said he hoped that those types of lines will be studied in their entirety. Mayor Hallman stated that previous studies for BRT, modern streetcar, or commuter rail for mass transit in Tempe stopped at the Tempe border. He indicated that this will never make sense for BRT until the destinations in Chandler and Scottsdale are included. Mayor Hallman stated that the same is true for commuter rail: it will not make sense without the data from the ridership with the Maricopa connection, in particular as the alternatives analysis for I-10 as a line for use in conjunction with the expanded freeway and as a reliever for truck congestion during construction moves forward. Mayor Hallman urged discontinuing examinations of these lines as small pieces, but examine them in full build-out.

Supervisor Wilcox stated that she thought the studies were very good. She said that she sat in on some of the public input opportunities and the West Valley is very excited about commuter rail. Supervisor Wilcox stated that she realized funding is very hard to come by, but with people now seeing the benefits of light rail, commuter rail becomes closer to reality.

Supervisor Wilcox moved to 1) Accept the findings of the Grand Avenue Commuter Rail Corridor Development Plan, Yuma West Commuter Rail Corridor Development Plan, and Commuter Rail System Study; and 2) Revise the corridor ranking included in the Commuter Rail System Study upon

the completion of updated regional socioeconomic forecasts or relevant passenger rail studies, and a continual look at the figures as Mayor Hallman mentioned. Councilmember Wolcott seconded.

Chair Neely asked if there was discussion on the motion.

Mayor Rogers expressed her agreement with Mayor Hallman and Supervisor Wilcox about the possibilities. She expressed appreciation for the consultant's admission that the ridership modeling was based on outdated land use plans and population statistics from 2000. Mayor Rogers stated that she was opposed to this if it is not on the map, because, as everyone knows, if it is on the map, it becomes reality. Mayor Rogers stated that a lot has changed in Avondale since 2000 and she was not confident that the agreement to update the model will impact the priorities once they are set. She expressed that she could not support a study based on misguided data to form priorities that do not equally benefit all of the Valley. Mayor Rogers stated that the report ignores the desperate state of transportation in the West Valley, where there are limited public transportation options, no light rail, limited grid service, and not even a reliever highway for another 20 years. She stated that the region oversupplies some areas with transportation and supplies none in other areas. Mayor Rogers stated that she will be voting no on this item for those reasons.

Chair Neely stated that what she was hearing is that a more comprehensive study is needed, and she hoped that this motion would be moving that forward. She indicated that she would be horrified if plans from 2000 were being used.

Mr. Pearsall replied that they used April 2007 data, which were approved by the Regional Council. He added this was the last model run. Mr. Pearsall also noted that item #2 in the motion is to use the newer data in 2011 after the 2010 Census results are received.

Supervisor Wilcox stated that was her understanding and intention when she made the motion that new data would be used.

Councilmember Presmyk referenced Mayor Hallman's comment that commuter rail had been studied for six years. He asked the timeline that commuter rail could be implemented. Mr. Pearsall replied that it depends on the work with ADOT and the Legislature to identify the indemnity and liability issues, as requested by the railroad. He noted that the railroad cannot move forward as a partner until a local funding source is identified to move commuter rail beyond the study phase. Mr. Pearsall stated that the railroad has told MAG that it will not enter into an agreement or memorandum of understanding until there is a commitment of funds at the local or regional level to address their concerns. He noted that the average operating cost per corridor is \$10 million to \$20 million per year, and the cost for the 105 mile system would be \$1.5 billion. Mr. Pearsall stated that until the funding source is identified and legal issues are resolved, this study will be a workbook on which to base work. He added that item #2 in the motion allows the study to be updated perpetually as new data become available.

Mayor Hallman clarified that the jurisdiction studying commuter rail for six years was the City of Tempe. He said that the City of Phoenix has also been involved in a process with BNSF four years ago. Mayor Hallman noted that the studies began in earnest with the Commuter Rail Stakeholders Group, who looked at options and interests and how to move forward. He explained that the City of Tempe had

funds designated for commuter rail, but that has not been sustained due to the recession. Mayor Hallman expressed that they hope to be able to recommit the funding after the recession. He stated that from the City's perspective, it only makes sense to study commuter rail if the study extends to Maricopa. Mayor Hallman referenced the legal issues with the railroads. He said that the City of Tempe has been in discussions with the railroads for years on options that could be beneficial to freight rail as a partnership with commuter rail.

Councilmember Wolcott stated that she had met with Chairman Oberstar the day before yesterday and he thinks MAG should not wait, but move forward with an environmental assessment, ultimately to keep the momentum going and develop an opportunity to educate the public about commuter rail and needs, and the possibilities of expanding to a full regional system with full connectivity. She said that would create a dual-track process where the studies are continued and the rankings are revised with updated data. Councilmember Wolcott stated that if MAG waits, it will keep waiting. She remarked that this has to be a multitrack process or it will not be moving forward.

Chair Neely asked about funding for commuter rail in Proposition 400. Mr. Smith replied that Proposition 400 included study funds. Chair Neely stated that she was not discouraging efforts, but everyone has financial shortfalls right now. She indicated that she thought that commuter rail should move forward if there is an ability to do so, but she would say if there was a request at the City of Phoenix today, it would be difficult to come up with funds.

Chair Neely called the question. The motion passed, with Mayor Rogers voting no.

9. Approval of the Draft FY 2011 MAG Unified Planning Work Program and Annual Budget and the Member Dues and Assessments

This agenda item was taken out of order.

Becky Kimbrough, MAG Fiscal Services Manager, reported on the request for approval of the Draft MAG FY 2011 Unified Planning Work Program and Annual Budget. She said that the MAG dues and assessments were presented in January 2010 with a proposed overall decrease of 50 percent due to economic conditions.

Ms. Kimbrough noted that the proposed new projects for FY 2011 were first presented at the February committee meetings. She stated that these new project proposals come from the various MAG technical committees, policy committees and other discussions with members and stakeholders regarding joint efforts within the region.

Ms. Kimbrough noted that the Draft Work Program was provided to members during March for review. Ms. Kimbrough stated that in April, staff positions and salary information and improvements to the MAG office space were presented. Ms. Kimbrough stated that the draft Work Program and Annual Budget was reviewed at the Intermodal Planning Group meeting on April 29, 2010. She noted that representatives from the Federal Highway Administration, Federal Transit Administration, Arizona Department of Administration, the Environmental Protection Agency, Valley Metro and Valley Metro

Rail, and Phoenix Transit participated in the meeting. She reported that the review did not result in any new recommendations for the FY 2011 Work Program and Annual Budget document.

Ms. Kimbrough reported that each year, MAG submits the Work Program to the Government Finance Officers Association for the Distinguished Budget award. She noted that this will be the 11th year in a row that MAG will submit the document.

Chair Neely thanked Ms. Kimbrough for her report. No questions from the Council were noted.

Councilmember Esser moved approval of the resolution adopting the Draft FY 2011 MAG Unified Planning Work Program and Annual Budget and the member dues and assessments. Mayor LeVault seconded, and the motion carried unanimously.

Chair Neely thanked Ms. Kimbrough for the good work on the budget.

8. Sustainable Communities Planning Grant Program

Amy St. Peter, MAG Human Services Manager, reported on the Sustainable Communities Planning Grant Program. She said that the purpose of this presentation is to inform the Committee about activities undertaken to determine the viability of a regional application through MAG.

Ms. St. Peter stated that the U.S. Department of Housing and Urban Development (HUD) is partnering with the U.S. Department of Transportation (DOT) and the Environmental Protection Agency (EPA) on the Sustainable Communities Planning Grant Program to support the development of regional plans for sustainable development. Ms. St. Peter stated that MAG may be eligible to apply for funding, which may position MAG well in the future if such plans become a requirement with the reauthorization of federal transportation funding.

Ms. St. Peter stated that approximately \$100 million is available nationally with up to \$5 million potentially available for large metropolitan areas, such as MAG. She noted that a 20 percent match is required and HUD has indicated it is considering awarding about 40 grants. Ms. St. Peter commented that this grant process is anticipated to be very competitive, and she added that many in this region have expressed interest in applying or partnering for the grant.

Ms. St. Peter stated that the application process could conclude in as soon as 60 days. She indicated that the Notice of Funding Availability (NOFA) is due in mid-June. Ms. St. Peter remarked that the advance notice published by HUD in March did not define an eligible applicant or region and such clarification will help determine if MAG is the most appropriate applicant for the region.

Ms. St. Peter stated that at the request of the MAG Executive Committee, staff convened meetings with community partners, other councils of governments and with the officers of the MAG technical committees to collect information about current activities that are relevant to this grant. Ms. St. Peter stated that some support has been expressed for submitting a consolidated application on behalf of the Sun Corridor. She noted that the Central Arizona Association of Governments (CAAG) Regional Council took action to support submitting a consolidated application with MAG if MAG decides to

move forward with this approach. Ms. St. Peter reported on the concerns expressed that MAG scope for an application as the Sun Corridor would not include all of the MAG region.

Ms. St. Peter stated that feedback received so far indicates support for developing green housing and jobs along high capacity transit lines such as commuter rail, light rail, and the proposed intercity rail from Phoenix to Tucson. She reported that in the meeting with the technical committee officers it was expressed that it was important to focus on the entire region, to consider infill development, to specifically identify the impact desired by the plan, and to leverage existing efforts proposed in the MAG FY 2011 Unified Planning Work Program. Ms. St. Peter noted that since HUD did not define "region," there is the opportunity for cities, towns, even neighborhoods, to submit an application, however, there are concerns that multiple applications from within the region could dilute MAG's chances for an award.

Ms. St. Peter noted concerns have also been expressed for relinquishing local control. Ms. St. Peter stated that when the NOFA is received, staff will review it carefully to ensure no local control will be relinquished in exchange for federal funds. She advised that HUD is looking for applications with a focus on regional plans for sustainable development. Ms. St. Peter stated that guidance on MAG's role in a potential regional application is requested of the Regional Council.

Supervisor Wilcox stated that the County has been looking at this application also. She expressed her interest in applying for the Sun Corridor, and said that partnerships have a better chance of success. Supervisor Wilcox stated that a lot of groundwork has been covered and with the studies already completed, she felt there possibly could be funding for the Sun Corridor.

Councilmember Wolcott said that she thought this grant was about partnerships and she expressed that she was intrigued by the Sun Corridor report reviewed at the Joint Planning Advisory Council meeting. Councilmember Wolcott stated that she felt the Sun Corridor aspect had merit, but she wanted to ensure that the full Sun Corridor was being considered, otherwise, there was no point in moving forward.

Mayor Rogers asked if \$2 million grants could be awarded to small metropolitan areas, separate from the \$5 million for large metropolitan areas. Ms. St. Peter replied that was correct, 25 percent of the total amount was available to smaller areas, which could apply for up to \$2 million each. Mayor Rogers expressed that she supported maximizing any funding opportunities presented through this grant and she hoped the Regional Council would support the efforts of the smaller communities who are working together to apply for the \$2 million planning funds.

Chair Neely stated that MAG is looking at submitting for the larger grant and individual cities could apply for the smaller grant. She asked Mr. Smith for guidance on this, since MAG has only so many staff. Mr. Smith replied that MAG does not want to pursue something not supported by the region. He added that the NOFA has not even been received yet, and it is unknown what HUD might end up doing. Mr. Smith stated that MAG would try to support individual cities' efforts. He added that the technical committees expressed support for working on commuter rail lines and the land use issues surrounding the corridors in an effort to boost ridership. Mr. Smith stated that they also supported the bike paths along the canal system as an alternative to move people. He noted that the Sun Corridor in Pinal County was to look at land use alternatives around the commuter rail lines going to Tucson and CAAG wanted to do an economic impact cluster area. He said he was not sure of PAG's goal. Mr. Smith stated that

MAG staff is requesting guidance from the Regional Council. He said that MAG could include pull back and not do anything until the NOFA is received. Mr. Smith stated that MAG does not want to be in competition with any of the cities.

Chair Neely requested that staff survey the cities if they are intending to apply individually. She said that MAG cannot commit to a three-county application without additional dialogue. Chair Neely indicated there was some interest, but also some opposition because they wanted to apply individually. She said that the results of the survey could be brought back to the Executive Committee with some discussion of direction.

Mayor Rogers stated that she looked at the \$5 million grant and the \$2 million grant as not competitive with each other. She thought that some of the smaller cities could work together that might access some of that funding with the assistance of MAG. Mayor Rogers stated that she realized this was down the road.

10. Legislative Update

No report was required.

11. Request for Future Agenda Items

Topics or issues of interest that the Regional Council would like to have considered for discussion at a future meeting will be requested.

No requests were noted.

12. Comments from the Council

An opportunity will be provided for Regional Council members to present a brief summary of current events. The Regional Council is not allowed to propose, discuss, deliberate or take action at the meeting on any matter in the summary, unless the specific matter is properly noticed for legal action.

Chair Neely noted that at each place was the Nominating Committee report that will move to the June Regional Council meeting for a vote.

13. Adjournment

Councilmember Presmyk moved, Councilmember Esser seconded, and the Regional Council meeting adjourned at 7:00 p.m.

Chair

Secretary

Project Status Report
Transportation Projects – MAG Region JUNE 23, 2010
American Recovery & Reinvestment Act (ARRA) Funding

On February 17, 2009, President Obama signed the American Recovery and Reinvestment Act (ARRA) of 2009. The national Highway Infrastructure Investment component of the legislation is \$27.5 billion. All projects in the MAG region have been obligated.

For the highway portion, the Arizona Department of Transportation (ADOT) has 120 days to obligate 50 percent of the funding, and a year – by March 2, 2010, to obligate the remaining funds. Of the ADOT portion, \$129.4 million was directed for Highway projects in the MAG Region. The legislation also sub-allocates 30 percent of the funding (\$156.57 million) to local jurisdictions. The amount being sub-allocated to the MAG Region is \$104.6. Metropolitan planning organizations and Local Agencies have one year to obligate the funds, by March 2, 2010.

The MAG regional portion for transit is \$66.4 million. The legislation requires that 50 percent of the transit funds be obligated within 180 days, and the remainder to be obligated within one year by March 2, 2010.

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Project Status Report

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Project Status Report

The Project Status Report highlights three areas of project details as noted below:

Project Information: Lists information about the project as reported on in the MAG Transportation Improvement Program (TIP) including the project location and description.

Project Funding: Explains the project funding sources and amounts as listed in the MAG TIP.

Project Development Status: This section reports on the status of project development steps. This section will most likely change in the future as projects are under construction. The project development steps are:

- Project Approved by MAG RC (Date): Project approved by the MAG Regional Council for inclusion in the current MAG TIP
- Design & Federal Clearances: The required design and federal clearances have been complete or have estimated completion dates. Or other notes may be provided regarding status with FHWA or FTA. Check mark indicates that work is completed.
- Obligate: The project has obligated, which means that the Federal Highway Administration agrees that the project has completed the necessary federal steps and the federal funds can be promised for the project. This date is the projected obligation date based on submittal of final PS&E. Actual date will depend on FHWA processing time.
- Advertise Date – The date the project scheduled to be advertised.
- Award Date – The date the project is awarded to contractor.
- Estimated Completion – The contractor has estimated that construction will be completed by this date.

**This information can also be found at the MAG Website:
<http://www.mag.maricopa.gov/detail.cms?item=9615>**

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Project Funding			Project Development Status						Comments		
TIP #	Federal ID	Project Location	Project Description	Fund Type	Federal - ARRA	Total	Award Amount	Approval Date	Design	Environmental	Obligated	Advertise Date		Award Date	Estimated Complete
State Sponsored Projects - Roadways															
DOT09-815	010-B(205)	I-10: Verrado Way - Sarival Rd	Construct General Purpose Lane	ARRA	\$27,635.1	\$27,635.1	\$27,635.1	05/27/09	✓	✓	✓	✓	7/17/09	2/12/2011	Admin Mod: Change project costs from \$28.2M to \$26.3M.
DOT09-818	017-A(207)	I-17: SR74-Anthem Way	Construct General Purpose Lane	ARRA	\$13,994.1	\$13,994.1	\$13,994.1	05/27/09	✓	✓	✓	✓	6/19/09	5/31/2010	Admin Mod: Change project costs from \$13.4M to \$13.3M
DOT09-6C00R	060-B(201)	US 60: SR 303L - 99th Ave	Road Widening	ARRA	\$23,899.3	\$23,923.5	\$23,923.5	03/25/09	✓	✓	✓	✓	11/20/09	12/31/2011	Admin Mod: Change project costs from \$45.0M to \$22.3M
DOT07-323	101-A(203)	99th Ave from I-10 to MC-85	Road Widening	STP-AZ & ARRA	\$1,519.1	\$2,251.2		04/22/09	✓	✓	✓	✓	6/18/10		Regional cost includes \$652,890 in Toleson local ARRA funds. Bids open
DOT09-801	060-B(201)	US 60: 99th Ave to Thunderbird Rd (within the city limits of El Mirage)	Transportation Landscaping Enhancement	ARRA	\$212.8	\$212.8	\$212.8	04/22/09	✓	✓	✓	✓	11/20/09	12/31/2011	El Mirage local ARRA funds used for local costs in ADOT project
DOT07-332	060-B(200)	US 60: 99th Ave - 83rd Ave	Road Widening	ARRA	\$8,046.8	\$8,046.8	\$8,046.8	03/25/09	✓	✓	✓	✓	8/14/09	10/31/2010	Admin Mod: Change project costs from \$11.2 mill to \$7.6M.
DOT06-613	085-B(200)	SR 85: Southern Ave - I 10	Widen roadway, adding 2 through lanes	ARRA	\$11,147.3	\$11,147.3	\$11,147.3	05/27/09	✓	✓	✓	✓	9/18/09	11/26/2010	Admin Mod: Change project costs from \$18.6 mill to \$11.0M - pending contract award
DOT12-840	101-A(204)	101 (Agua Fria Fwy) at Union Hills Dr/Beardsley Rd	Construct traffic interchange, construct new frontage road and Texas U-Turn structure over L101	ARRA, STP MAG & Local	\$5,667.4	\$17,173.9	\$17,173.9	04/22/09	✓	✓	✓	✓	10/16/09	7/31/2011	Admin Mod: Change project costs from \$27.5 mill to \$17.1M
DOT08-673	074-A(200)	74: US-60 (Grand Ave) to Loop 303 (Estrella Fwy); MP 20-22	Construct eastbound and westbound passing lanes	ARRA	\$2,440.9	\$2,440.9	\$2,324.6	05/27/09	✓	✓	✓	✓	10/16/09	09/31/2011	Admin Mod: Change project costs from \$3.9 mill to \$2.3M
DOT12-841	101-A(206)	Loop 101: Northern to Grand SB	Auxiliary lane - 3 miles	ARRA	\$2,186.1	\$2,186.1		09/30/09	✓	✓	✓	✓	5/4/10		
DOT10-815	101-A(201)	Loop 101: Olive Avenue	TI Improvements	ARRA	\$2,172.4	\$2,172.4	\$2,172.4	09/30/09	✓	✓	✓	✓	3/19/10		Admin Mod: Change project costs from \$3M mill to \$2.17M - pending contract award
DOT10-6C32	074-A(201)	SR 74: MP 13 - MP 15	Construct Passing Lanes	ARRA	\$3,395.0	\$3,395.0		09/30/09	✓	✓	✓	✓			Bids open 6/11/10
DOT10-816	017-A(211)	I-17: I-10 to Indian School	Southbound Roadway Improvements	ARRA	\$1,100.0	\$1,100.0		09/30/09	✓	✓	✓	✓	6/18/10		Bids open 5/20/10
DOT10-813	101-A(205)	Loop 101: 51st Ave to 27th Ave EB	Auxiliary lane	ARRA	\$2,085.1	\$2,085.1		09/30/09	✓	✓	✓	✓	5/4/10		Bids open 4/9/10
DOT10-828	087-B(205)A	SR 87: Four Peaks - Dos S Ranch Road	Construct Roadway Improvements	ARRA	\$18,500.0	\$18,500.0		09/30/09	✓	✓	✓	✓	5/21/10		Bids open 4/30/10

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Project Funding			Project Development Status						Comments		
TIP #	Federal ID	Project Location	Project Description	Fund Type	Federal - ARRA	Total	Award Amount	Approval Date	Design	Environmental	Obligated	Advertise Date		Award Date	Estimated Complete
State Sponsored Projects - Roadways															
DOT08-828	087-A(206)A	SR 87: MP 211.8 to 213.0	Repair cut slopes for erosion control	ARRA	\$1,600.0	\$1,600.0		12/09/09	✓	✓	✓	✓	5/21/10		To be done in conjunction with project SR 87: Four Peaks - Dos S Ranch Road
DOT08-839	143-A()	143 Hohokam: SR 143/Sky Harbor Blvd TI	TI Improvements, Adding Ramps	ARRA	\$35,100.0	\$35,100.0		12/09/09	✓	✓					
DOT10-851		US 60: San Domingo - Whitmann	Pavement Preservation	ARRA	\$9,000.0	\$9,000.0		02/24/10	✓	✓					State project to be funded with Local ARRA STP-AZ funds will be used if full amount of ARRA funds are not available
					\$160,701.4	\$172,964.1	\$106,630.3								

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Fund Type	Project Funding			Project Development Status						Comments	
TIP #	Federal ID	Project Location	Project Description		Federal - ARRA	Estimated Total	Construction Award Amount	Approval Date	Design	Environment	Obligate	Bid Open Date	Award Date		Estimated Complete
Local Projects - Roadway															
APJ09-801	APJ-0(201)	Ironwood Drive: Southern Avenue to 16th Avenue	Design and Reconstruction of Pavement	ARRA	\$1,348.3	\$1,348.3	\$1,499.1	4/22/09	✓	✓	✓	2/3/10	4/24/10		
AVN09-801	AVN-0(206)	Dysart Road-I-10 to Indian School Road	Preliminary engineering, design and construction for Mill & Replace	ARRA	\$2,035.2	\$2,035.2	\$1,681.9	4/22/09	✓	✓	✓	3/5/10	4/6/10		Permanent striping will be completed by the end of June. Video detection cameras are scheduled for delivery in early July.
AVN09-802	AVN-0(207)	Dysart Road -Van Buren to the I-10	Preliminary engineering, design and construction for Mill & Replace	ARRA & Local	\$179.7	\$401.8	N/A	4/22/09	N/A	N/A	N/A	N/A	N/A	N/A	Combined with AVN09-801
BKY09-801	BKY-0(202)	Various Locations Townwide - Functionally Classified Roads	Pre-engineer/Design and Pavement Rehabilitation and Preservation	ARRA	\$1,621.9	\$1,621.9	\$1,118.9	4/22/09	✓	✓	✓	2/12/10	3/19/10		Crews are working on the milled shoulder on Yuma Road. Contractor has completed second lift of slurry seal on Broadway Road.
CFR09-801	CFE-0(200)	Intersection of Tom Darlington Drive and Ridgeview Place	Pre-engineer/Design and construct Pedestrian crossing	ARRA	\$35.0	\$35.0	N/A	4/22/09	N/A	N/A	N/A	N/A	N/A	N/A	Combined Project: ARRA-CFE-0(200),Town of Carefree has been combined with Cave Creek Road ARRA-CFE-0(201)A.
CFR09-802	CFE-0(201)	Cave Creek Road: Scopa Trail to Carefree Eastern Border	Pre-engineer/Design and construct, repair and restoration of Cave Creek Road	ARRA	\$553.3	\$553.3	\$440.8	4/22/09	11/12/09	✓	✓	3/12/10	4/6/10		Partnering/Preconstruction meeting was held on May 20, 2010. Crews are working on crack sealing, milling and AC replacement.
CVK09-807	CVK-0(201)	Various Locations - Functionally Classified Roadways	Pre-Engineer/Design and Construct Pavement Rehab projects	ARRA	\$614.8	\$614.8	\$491.4	5/27/09	✓	✓	✓	4/2/10	5/4/10		Construction work will start on June 21st.
CHN120-07C	CHN-0(025)	Chandler Blvd/Dobson Road Intersection, and Dobson Road from Chandler Blvd to Frye Road	Intersection and Capacity Improvement	ARRA, Local & RARF	\$2,288.7	\$7,629.0	\$5,244.0	4/22/09	✓	✓	✓	2/5/10	3/25/10	Feb-11	
CHN09-801	CHN-0(211)	Price Road from Germann Road south to Queen Creek Road	Design and reconstruction of pavement	ARRA	\$3,678.9	\$3,678.9	\$2,313.0	4/22/09	✓	✓	✓	3/3/10	4/22/10	Nov-10	
ELM09-801	ELM-0(202)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and Mill and Replace Existing Road.	ARRA	\$952.8	\$952.8	\$571.8	4/22/09	✓	✓	✓	4/16/10	5/21/10		Contract time begins June 23, 2010. Partnering/Preconstruction meeting is scheduled for June 28.
FTH07-301	FTH-0(203)	Shea Blvd. (Palisades Blvd. to Fountain Hills Blvd.)	Widen for 3rd (westbound) lane, bike lane, sidewalk, and turn pockets.	ARRA, STP, & Local	\$1,081.6	\$3,376.6	\$1,746.7	6/24/09	✓	✓	✓	12/11/09	2/19/10		Paving is tentatively scheduled for the end of June.
GBD09-801	GBD-0(201)	Pima Street/SR-85 Various Locations	Design and Construct Signage Improvements	ARRA	\$33.0	\$33.0	N/A	4/22/09	N/A	N/A	N/A	N/A	N/A	N/A	Combined with GBD09-802
GBD09-802	GBD-0(200)	Pima Street/SR-85 Various Locations	Design and Construct Pedestrian and Landscape Improvements	ARRA	\$339.5	\$339.5	\$389.3	4/22/09	✓	✓	✓	4/23/10	5/21/10		
GBD09-803	GBD-0(203)	Gila Bend Airport on SR-85	Design and Construct Carpool and Transit Park & Ride Lot	ARRA	\$170.0	\$170.0	\$239.9	5/27/09	✓	✓	✓	4/2/10	5/4/10		
GRC09-801	GRI-0(200)	Various Locations - Functionally Classified Roadways	Pre-Engineer/Design and Construct Pavement Rehab projects	ARRA	\$561.3	\$561.3	\$504.1	4/22/09	✓	✓	✓	4/9/10	5/4/10		Tentative Contractor start date is June 28, 2010. Contractor is working on obtaining required GRIC permits.

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Project Funding				Project Development Status						Comments	
TIP #	Federal ID	Project Location	Project Description	Fund Type	Federal - ARRA	Estimated Total	Construction Award Amount	Approval Date	Design	Environment	Obligate	Bid Open Date	Award Date		Estimated Complete
Local Projects - Roadway															
GLB09-801	GIL-0(203)	Various Locations - Functionally Classified Roadways	Pre-Engineer/Design and Construct Nova Chip Overlays- arterial roadways	ARRA	\$5,306.3	\$5,306.3	\$4,179.4	4/22/09	✓	✓	✓	2/12/10	3/19/10		Contractor has started paving and will continue for 15-20 days. Final striping will be completed in August 2010.
GLN09-801	GLN-0(219)	Various Locations Citywide - Functionally Classified Roadways	New traffic signal cabinets and controllers	ARRA	\$1,100.0	\$1,100.0	\$1,527.5	4/22/09	✓	✓	✓	4/23/10	5/21/10		Preconstruction meeting is scheduled for June 10, 2010.
GLN09-802	GLN-0(218)	Various Locations Citywide - Functionally Classified Roadways	Modernize traffic signals	ARRA	\$550.0	\$550.0	N/A	4/22/09	✓	✓	✓	N/A	N/A	N/A	Combined with GLN-0(219)
GLN09-803	GLN-0(217)	Various Locations Citywide - Functionally Classified Roadways	CCTV Camera Installations	ARRA	\$90.0	\$90.0	N/A	4/22/09	✓	✓	✓	N/A	N/A	N/A	Combined with GLN-0(219)
GLN09-804	GLN-0(215)	Camelback Rd. - 47th to 83rd Aves.	Install wireless communication with traffic signals	ARRA	\$230.0	\$230.0	\$250.7	4/22/09	✓	✓	✓	4/16/10	5/21/10		Partnering and Preconstruction meeting is scheduled on June 15, 2010.
GLN09-805	GLN-0(216)	Bethany Home Rd. - 63rd to 83rd Aves.	Install wireless communication with traffic signals	ARRA	\$200.0	\$200.0	N/A	4/22/09	✓	✓	✓	N/A	N/A	N/A	Combined with GLN-0(215)
GLN09-806	GLN-0(211)	Glendale Ave. - 51st to 66th Aves.	Pre-Engineer/Design and construct pavement overlay	ARRA	\$1,170.0	\$1,170.0	\$1,414.7	4/22/09	✓	✓	✓	4/23/10	5/11/10		Estimated start date for construction is June 24th.
GLN09-807	GLN-0(212)	Litchfield Rd. - Missouri to Northern Ave.	Pre-Engineer/Design and construct pavement surface treatment	ARRA	\$510.0	\$510.0	\$560.3	4/22/09	✓	✓	✓	5/14/10	6/18/10		This project is scheduled to be awarded at the June 18, 2010, board meeting.
GLN09-808	GLN-0(214)	25 Miles on Arterial Streets	Install thermoplastic pavement markings	ARRA	\$358.4	\$358.4	\$239.0	4/22/09	✓	✓	✓	4/23/10	5/21/10		Partnering and preconstruction meeting is scheduled on June 15, 2010.
GLN08-604	GLN-0(033)	63rd Avenue at Loop 101 Expressway	Design and construct multi-use overpass over Loop 101 (Agua Fria Fwy) (Phase 2)	ARRA, CMAQ, & Local	\$1,850.0	\$5,407.4	\$2,520.0	4/22/09	✓	✓	✓	3/5/10	4/16/10		Temporary concrete barrier setup for median pier construction on L 101 is scheduled for June 24, 2010.
GDY09-801	GDY-0(202)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and construct mill, patch and replace	ARRA & Local	\$782.4	\$798.4	\$623.5	4/22/09	✓	✓	✓	3/26/10	4/16/10		Tentative start date for contractor is June 21st.
GDL09-801	GUA-0(200)	Various Locations Townwide - Functionally Classified Roadways	Design and Mill & Asphalt overlay roadways	ARRA	\$634.0	\$634.0	\$548.1	4/22/09	✓	✓	✓	4/9/10	5/4/10		Contract time starts on June 7, 2010.
LPK09-801	LPK-0(201)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and mill and replace pavement resurfacing/reconstruction	ARRA	\$614.0	\$614.0	\$455.9	4/22/09	✓	✓	✓	4/2/10	5/21/10		Contract work to start end of June and the field office is working on setting up partnering and preconstruction meeting. construction is complete, final close-out in process. (This is an ADOT TE project, so ADOT will keep savings in their TE program, if any.)
MMA09-725	MMA-0(201)	Bush Hwy from User Pass Rd to Stewart Mtn Rd	Design and construct bicycle lane	TEA-ARRA	\$750,000	\$1,117,817	\$552.4	5/27/09	✓	✓	✓	3/24/10	7/21/09	Mar-10	
MMA09-801	MMA-0(210)	Various Locations Countywide - Functionally Classified Roadways	Pre-Engineer/Design and construct AR Overlay	ARRA & Local	\$6,469.2	\$6,478.1	\$9,399.6	4/22/09	✓	✓	✓	2/18/10	3/24/10		

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Project Funding				Project Development Status						Comments	
TIP #	Federal ID	Project Location	Project Description	Fund Type	Federal - ARRA	Estimated Total	Construction Award Amount	Approval Date	Design	Environment	Obligate	Bid Open Date	Award Date		Estimated Complete
Local Projects - Roadway															
MES09-801R	MES-0(209)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and pavement reconstruct and ADA upgrades	ARRA	\$1,610.9	\$1,610.9	\$967.2	5/27/09	✓	✓	✓	3/11/10	4/5/10		
MES09-802R	MES-0(210)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and construct mill and replace pavement	ARRA	\$970.7	\$970.7	\$1,281.2	5/27/09	✓	✓	✓	2/3/10	3/22/10	Aug-10	
MES09-803	MES-0(211)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and pavement reconstruct and ADA upgrades, Group 1	ARRA	\$2,559.3	\$2,559.3	\$2,336.4	5/27/09	✓	✓	✓	2/10/10	4/5/10	Sep-10	
MES09-804	MES-0(212)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and pavement reconstruct and ADA upgrades, Group 2	ARRA	\$2,333.3	\$2,333.3	\$1,975.7	5/27/09	✓	✓	✓	2/3/10	3/22/10	Jun-10	
MES09-805	MES-0(213)	Various Locations Citywide - Functionally Classified Roadways	Pre-Engineer/Design and pavement reconstruct and ADA upgrades Group 3	ARRA	\$3,310.6	\$3,310.6	\$3,476.4	5/27/09	✓	✓	✓	2/3/10	3/22/10	Nov-10	
PVY09-801	PVY-0(202)	Various Locations Townwide - Functionally Classified Roadways	Pre-Engineer/Design and construct pavement resurface projects	ARRA & Local	\$823.2	\$823.8		4/22/09	✓	✓	✓	6/25/10			
PEO100-07AC1	PEO-0(206)	Beardsley Rd Connection: Loop 101 (Agua Fria Fwy) to Beardsley Rd at 83rd Av/Lake Pleasant Pkwy	Construct Beardsley Road extension and bridge over New River	ARRA, STP-MAG & Local	\$2,850.4	\$11,489.7	\$7,919.3	4/22/09	✓	✓	✓	10/22/09	12/18/09		Crews are also working on screen walls, curb and gutter placement and grading for roundabout construction.
PEO09-801	PEO-0(205)	Various Locations	Pavement Preservation: Major Arterial mill, overlay and re-striping	ARRA & Local	\$1,130.1	\$1,396.3	\$1,848.3	6/24/09	✓	✓	✓	3/12/10	4/16/10		Paving operations are scheduled to begin June 14th and will be completed by June 23rd.
PHX07-316	PHX-0(209)	7th St & McDowell Rd	Design & Construction of Intersection Improvements	ARRA & CMAQ	\$1,000.0	\$2,256.0	\$748.9	4/22/09	✓	✓	✓	10/27/09	11/18/09	Jul-10	
PHX09-801	PHX-0(237)	Various Locations (North Area) - Functionally Classified Roadways	Design & Construction of Pavement Preservation	ARRA	\$7,136.2	\$7,136.2	\$5,190.0	4/22/09	✓	✓	✓	1/26/10	3/3/10	Dec-10	
PHX09-802	PHX-0(238)	Various Locations (Central Area) - Functionally Classified Roadways	Design & Construction of Pavement Preservation	ARRA	\$7,150.0	\$7,150.0	\$5,261.1	4/22/09	✓	✓	✓	1/26/10	3/3/10	Dec-10	
PHX09-803	PHX-0(239)	Various Locations (South Area) - Functionally Classified Roadways	Design & Construction of Pavement Preservation	ARRA	\$7,150.0	\$7,150.0	\$5,095.9	4/22/09	✓	✓	✓	1/26/10	3/3/10	Dec-10	
PHX09-804	PHX-0(229)	Various Locations - (North Area)	Design & Construction of Removal/Replacement of Existing ADA Ramps or Construction of New ADA Ramps	ARRA	\$1,750.0	\$1,750.0	\$1,185.9	4/22/09	✓	✓	✓	2/2/10	3/3/10	Dec-10	
PHX09-805	PHX-0(230)	Various Locations - (South Area)	Design & Construction of Removal/Replacement of Existing ADA Ramps or Construction of New ADA Ramps	ARRA	\$1,750.0	\$1,750.0	\$1,270.2	4/22/09	✓	✓	✓	2/2/10	3/3/10	Dec-10	
PHX09-806	PHX-0(231)	11 Locations Citywide	Design & Construct Bridge Deck Rehabilitations	ARRA	\$2,250.0	\$2,250.0	\$1,521.8	4/22/09	✓	✓	✓	3/23/10	TBD	Dec-10	
PHX09-807	PHX-0(232)	6 Locations Citywide	Design & Construct Bridge Joint Rehabilitations	ARRA	\$1,250.0	\$1,250.0	\$444.9	4/22/09	✓	✓	✓	2/9/10	TBD	Dec-10	

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information				Fund Type	Project Funding			Project Development Status							Comments
TIP #	Federal ID	Project Location	Project Description		Federal - ARRA	Estimated Total	Construction Award Amount	Approval Date	Design	Environment	Obligate	Bid Open Date	Award Date	Estimated Complete	
Local Projects - Roadway															
PHX09-808	PHX-0(236)	Citywide Corridors	Inventory / Programming & Procure / Install Traffic Control Signs	ARRA	\$3,000.0	\$3,000.0	\$1,675.4	4/22/09	✓	✓	✓	3/23/10	TBD	Dec-10	
PHX09-809	PHX-0(234)	Citywide Corridors	Design & Procure/Install Fiber Optic Backbone System	ARRA	\$1,500.0	\$1,500.0	\$482.3	4/22/09	✓	✓	✓	3/9/10	TBD	Dec-10	
PHX09-810	PHX-0(233)	Citywide Corridors	Design & Procure/Install CCTV	ARRA	\$1,000.0	\$1,000.0	\$578.6	4/22/09	✓	✓	✓	6/15/10	6/21/10	Feb-11	
PHX09-811	PHX-0(235)	Citywide Corridors	Design & Procure/Install Wireless Communications	ARRA	\$500.0	\$500.0		4/22/09	✓	✓	✓	6/22/10		Feb-11	
QNC09-801	QCR-0(204)	Combs Rd: UPRR/Rittenhouse Rd to approx. 1,000 ft west of Gantzel Rd	Pre-Engineer/Design and construct resurfacing roadway	ARRA	\$227.3	\$227.3	N/A	4/22/09	N/A	N/A	N/A	N/A	N/A	N/A	
QNC09-802	QCR-0(205)	Various Locations on Rittenhouse Rd	Pre-Engineer/Design and construct resurfacing roadway and shoulder paving	ARRA	\$805.8	\$805.8	\$816.6	4/22/09	✓	✓	✓	4/16/10	5/21/10		Contractor is scheduled to start on June 23rd and the Field office is working on scheduling partnering/ preconstruction conference.
SRP09-801	SRI-0(200)	Various Locations - Functionally Classified Roadways	Design & Construction of Pavement Preservation/Chip-Seal	ARRA	\$653.9	\$653.9	\$663.2	5/27/09	✓	✓	✓	3/26/10	4/16/10		Contractor anticipates entire work to take approximately 2- 2½ weeks. Work anticipated to start on July 6, 2010.
SCT09-802	SCT-0(209)	Various Locations	Preliminary engineering, design and construction for Mill & Replace	ARRA	\$4,600.0	\$4,600.0	\$3,870.5	7/22/09	✓	✓	✓	3/2/10			
SCT12-813	SCT-0(206)	Various Locations in Southern Scottsdale	Replace traffic signal controllers and cabinets	ARRA, & Local	\$439.6	\$500.0	\$461.7	4/22/09	✓	✓	✓	3/12/10			
SUR09-801	SUR-0(208)	Bell Road-Parkview to West City Limit	Pre-Engineer/Design and construct pavement Reconstruction and ITS Conduit Installation	ARRA	\$2,933.4	\$2,933.4	\$2,812.0	4/22/09	✓	✓	✓	3/5/10	4/6/10		Contractor has started working on this project.
TMP09-801	TMP-0(211)	Baseline Road between Kyrene Road and the Union Pacific Railroad, over the Western Canal	Construct replacement bridge over the Western Canal	ARRA, & Local	\$4,362.6	\$6,000.0	\$2,083.1	4/22/09	✓	✓	✓	3/23/10*	4/22/10	Jan-10	Contract Award date April 22, 2010. Notice to proceed May 1, 2010.
WKN09-801	WBG-0(200)	North Vulture Mine Rd: US 60 to Northern Town Limits	Design and Complete Pavement Mill and Replace	ARRA	\$644.1	\$644.1		4/22/09	✓	✓	✓				Waiting for utility improvements.
YTN09-801	YTN-0(200)	Peoria Ave: 111th Avenue west by 1950 feet/approx. 115th Avenue	Pre-Engineer/Design and construct mill and replace - pavement resurfacing	ARRA	\$645.9	\$645.9	\$324.9	4/22/09	✓	✓	✓	4/23/10	5/21/10		Contract work to start end of June and the field office is working on setting up partnering and preconstruction meeting.
					\$101,695.7	\$126,113.1									

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
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Project Information			Project Funding			Project Development Status						Comments	
TIP #	Project Location	Project Description	Federal - ARRA	Estimated Total	Award Amount	Approval Date	Design	Environment	Obligated	Bid Opened	Award Date		Estimated Complete
Local Projects - Transit Projects													
AVN09-804T	Citywide	Purchase 2 replacement dial-a-ride vehicles	\$126.0	\$126.0		6/24/09	NA	✓	✓				
GDY05-202T	I-10: Litchfield Rd to Dysart Rd (ADOT Basin between Litchfield and Dysart)	Park and Ride Land Acquisition	\$352.2	\$1,847.1		6/24/09	✓	✓	✓			Mar-10	The design is completed. The EA is completed. The land was acquired. Estimated construction cost is about \$5M.
GDY06-204T	I-10: Litchfield Rd to Dysart Rd (ADOT Basin between Litchfield and Dysart)	Construct regional park-and-ride (I/10 - Litchfield)	\$2,036.2	\$4,193.8		6/24/09	✓	✓	✓			Mar-10	The design is completed. The EA is completed. The land was acquired. Estimated construction cost is about \$5M.
GDY08-800T	I-10: Litchfield Rd to Dysart Rd (ADOT Basin between Litchfield and Dysart)	Acquire land- regional park and ride	\$186.5	\$977.6		6/24/09	✓	✓	✓			Mar-10	The design is completed. The EA is completed. The land was acquired. Estimated construction cost is about \$5M.
GLN10-807T	Citywide	Operating Assistance - Glendale	\$4.6			3/2/10	NA	NA	✓	NA			
GLN10-808T	Citywide	ADA Complimentary Assistance - Glendale	\$70.3			3/2/10	NA	NA	✓	NA			
MES08-801T	Loop 202/Power	Construct regional park-and-ride (Loop 202/Power)	\$517.8	\$1,800.0		9/30/09		✓	✓				Admin Mod: Modify project costs to lower amount and change funding type to ARRA-Transit and 5309.
MES10-801T	US60/Country Club	Park-and-Ride design	\$367.5	\$367.5		9/30/09		✓	✓				Amend: Add new ARRA-Transit project to list.
MES10-802T	US60/Country Club	Park-and-Ride land acquisition	\$3,238.3	\$3,238.3		9/30/09		✓	✓				Amend: Add new ARRA-Transit project to list.
MES10-803T	Loop 202/Power	Design regional park-and-ride (Loop 202/Power)	\$765.0	\$765.0		9/30/09		✓	✓				Amend: Add new ARRA-Transit project to list.
MES10-804T	Gilbert/McDowell	Design regional park-and-ride	\$765.0	\$765.0		9/30/09		✓	✓				Amend: Add new ARRA-Transit project to list.
MES10-805T	Gilbert/McDowell	Construct regional park-and-ride	\$517.8	\$2,289.0		9/30/09		✓	✓				Amend: Add new ARRA-Transit project to list.
MES10-809T	Country Club/US 60	Park-and-Ride construction	\$3,228.8	\$3,228.8		3/25/09	✓	✓	✓				Admin Mod: Modify project costs to lower amount.
PEO10-803T	Citywide	ADA Complimentary Assistance - Peoria	\$0.7			3/2/10	NA	NA	✓	NA			
PHX08-704T	27th Ave/Baseline Rd	27th Ave/Baseline Park and Ride Construct	\$1,100.0	\$1,100.0		5/27/09		✓	✓	✓		Jun-12	Staff is reviewing a revised design scope of work and fee proposal from Premier Engineering.
PHX08-705T	I-17/Happy Valley	Happy Valley/I-17 Park and Ride - construct	\$5,500.0	\$5,500.0		3/25/09	✓	✓	✓	✓		Dec-10	Construction is in it's initial stage. Contractor is clearing the site and beginning excavation for underground utilities.
PHX09-611T	Regionwide	Preventive Maintenance	\$5,400.0	\$11,964.0		3/25/09	NA	NA	✓	✓		Jun-10	Ongoing
PHX09-837T	Bell Rd/SR-51	Bus access crossover	\$640.1	\$640.1		3/25/09	✓	✓	✓	✓		Jul-10	Comments on the revised scope of work by the Deputy Director were forwarded to EAS on March 18. A cost analysis on the proposal and a negotiation summary/memorandum will be prepared by EAS.

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information			Project Funding			Project Development Status							Comments
TIP #	Project Location	Project Description	Federal - ARRA	Estimated Total	Award Amount	Approval Date	Design	Environment	Obligated	Bid Opened	Award Date	Estimated Complete	
Local Projects - Transit Projects													
PHX09-838T	Pecos Road/40th Street	Pecos/40th St Park and Ride Expansion	\$3,000.0	\$3,000.0		3/25/09	✓	✓	✓	✓		Dec-10	Contractor finished laying out underground utilities, concrete curbs and sidewalks, and is getting ready to grade site for aggregate base course placement.
PHX09-839T	Regionwide	Intelligent Transportation System Enhancement: Regional Transit Stop Data Overhaul	\$300.0	\$300.0		3/25/09	NA	✓	✓	✓		Sep-10	Operational Review has been completed by Trapeze was on site March 2 - 5, 2010 installing the BSM software and providing training on System Admin, field staff using laptop and GPS for adding and updating bus stops, and map updates. Issue list has been cr
PHX09-840T	Citywide	Bus Stop Improvements	\$4,321.2	\$4,321.2		3/25/09	✓	✓	✓	✓		Dec-11	Southwest Fabrication received the Notice to Proceed work on 2/22/10. SW is now repairing concrete transit pads and is manufacturing transit furniture. The first batch of new furniture is scheduled to be placed at sites by the end of April
PHX10-804T	Citywide	Operating Assistance - Phoenix	\$870.7			3/2/10	NA	NA	✓	NA		Mar-10	
PHX10-805T	Citywide	ADA Complimentary Assistance - Phoenix	\$972.0			3/2/10	NA	NA	✓	NA		Mar-10	
PHX10-818T	Central Avenue/Van Buren	Central Station Transit Center Refurbishments	\$5,000.0	\$5,000.0		3/25/09		✓	✓	ü		Jan-11	The construction plans were approved on March 16 after one review. The Statement of Readiness for Central Station has been approved by Budget & Research. Discussions are continuing on the revised CA services proposal from the consultant team. A draft RCA
SCT09-803T	Loop 101/Scottsdale Rd	Park-and-Ride construction	\$5,000.0	\$5,000.0		3/25/09	✓	✓	✓				Receiving FTA guidance on Scottsdale's request to secure a lease for potential site. Environmental documentation underway. Part of second 50%.
SCT10-801T	Citywide	Operating Assistance - Scottsdale	\$20.4			3/2/10	NA	NA	✓	NA			
TMP09-806T	East Valley Operations and Maintenance Facility	Expansion/ Updgrade	\$6,500.0	\$6,500.0		3/25/09	✓	✓	✓			Mar-11	Final Design Contract Awarded
TMP10-801T	Citywide	Operating Assistance - Tempe	\$331.0			3/2/10	NA	NA	✓	NA			

PROJECT STATUS REPORT TRANSPORTATION PROJECTS IN MAG REGION
American Recovery & Reinvestment Act (ARRA) Funding
JUNE 23 2010

Project Information					Project Funding			Project Development Status						Comments		
TIP #	Federal ID	Project Location	Project Description	Length	Fund Type	Federal - ARRA	Estimated Total	Award Amount	Approval Date	Design	Environment	Obligated	Advertise Date		Award Date	Estimated Complete
Local Projects - Transportation Enhancements																
CHN09-805	CHN-0(014)	Paseo Trail, Consolidated Canal: Galveston to Pecos Rd.	Construction of multi-use path	n/a	TEA-ARRA	\$750,000	\$1,161,610	\$893.6	5/27/09	✓	✓	✓	6/4/10			Estimate based on low bid plus contingency.
GLB04-303R	GIL-0(015)	Canal Crossing Project	Design and construction pedestrian bridges over canal crossing	n/a	TEA-ARRA	\$270,000	\$680,000	\$297.6	5/27/09	✓	✓	✓	9/9/09	9/18/09		
GLB08-801	GIL-0(202)	Heritage District Downtown Ped Project	Design and construct sidewalks, landscaping and other pedestrian improvements	1	TEA-ARRA	\$578,670	\$578,670	\$376.0	5/27/09	✓	✓	✓	9/9/09			Adjusted to include contingency.
GLN08-611	GLN-0(201)	Old Roma Alley	Design and construct pedestrian enhancements and landscape	0.05	TEA-ARRA	\$732,562	\$732,562	\$437.5	5/27/09	✓	✓	✓	12/3/09			Not yet issued. Pending City Signatures.
MMA09-725	MMA-0(201)	Bush Hwy from Usery Pass Rd to Stewart Mtn Rd	Design and construct bicycle lane	4.6	TEA-ARRA	\$750,000	\$1,117,817	\$561.1	5/27/09	✓	✓	✓	6/25/09	7/21/09	Dec-09	Construction complete
ME509-806	MES-0(021)	Consolidated Canal Pathway, 8th Street and Lindsay	Design and construct 12-foot wide multi-use pathway with lighting and signing	1.3	TEA-ARRA	\$750,000	\$1,509,375	\$678.0	6/24/09	✓	✓	✓	6/3/10			Estimate based on low bid plus contingency.
SCT09-703	SCT-0(200)	Crosscut Canal, Thomas Rd to Indian School Rd	Construct new pedestrian/bicycle bridge and multi-use path	0.75	ARRA; TEA-ARRA	\$1,632,333	\$3,117,272	\$763.5	5/27/09	✓	✓	✓				Project is using \$750,000 TE ARRA funds plus \$882,333 MAG ARRA funds.
SCT09-801	SCT-0(203)	Downtown Canal Bank Improvements	Design and construct transportation enhancements to connect Sun Circle Trail to Goldwater Underpass	n/a	TEA-ARRA	\$600,000	\$625,402	\$284.0	5/27/09	✓	✓	✓	11/2/09			Includes estimated salaries and overhead
TMP09-704	TMP-0(202)	Crosscut Canal from Papago Park to Mouer Park - Tempe	Design and construct multi-use path (phase II)	1	TEA-ARRA	\$750,000	\$1,400,000	\$1,644.6	5/27/09	✓	✓	✓	5/19/10			
						\$5,181,232	\$7,805,436									

MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

Update to the Federal Functional Classification System

SUMMARY:

The MAG funding suballocation for the MAG Region from the American Recovery and Reinvestment Act (ARRA) requires projects to adhere to the requirements established in the Surface Transportation Program (STP). As such, ARRA and/or STP funded projects must be located on a facility that is classified as an urban collector or rural major collector or higher in the federal functional classification hierarchy.

MAG has received a request from the City of Chandler to add one project to the federal functional classification system. The City of Chandler has requested that Airport Boulevard from Germann Road to Queen Creek Road be classified as a Major Collector. The classification requests are necessary for the ARRA/STP funded projects to proceed.

PROS & CONS:

PROS: Approval of this classification of this roadway will allow a project to receive federal funds and proceed in a timely manner.

CONS: None.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: Roadway projects that wish to utilize transportation federal ARRA and/or MAG-STP funds need to be located on a roadway that is federally functionally classified as one of the following: Urban Principal Arterial, Urban Minor Arterial, Urban Collector, Rural Principal Arterial, Rural Minor Arterial or Rural Major Collector.

POLICY: This request is in accord with Federal regulations regarding the coordination of the development and amendment of federal functional classifications between local governmental agencies and state highway agencies.

ACTION NEEDED:

Approval of the City of Chandler request to classify Airport Boulevard as a Major Collector in the federal functional classification system.

PRIOR COMMITTEE ACTIONS:

Management Committee: On June 9, 2010, the Management Committee recommended approval of the City of Chandler request to classify Airport Boulevard as a Major Collector in the federal functional classification system.

MEMBERS ATTENDING

- Mark Pentz, Chandler, Chair
- Carl Swenson, Peoria, Vice Chair
- # Matt Busby for George Hoffman, Apache Junction
- Charlie McClendon, Avondale
- Stephen Cleveland, Buckeye
- Gary Neiss, Carefree
- * Usama Abujbarah, Cave Creek
- Spencer Isom for B.J. Cornwall, El Mirage
- * Phil Dorchester, Fort McDowell Yavapai Nation
- Rick Davis, Fountain Hills
- Rick Buss, Gila Bend
- * David White, Gila River Indian Community
- Michelle Gramley for Collin DeWitt, Gilbert
- Brent Stoddard for Ed Beasley, Glendale
- # Mark Gaillard for John Fischbach, Goodyear
- Bill Hernandez, Guadalupe

- Darryl Crossman, Litchfield Park
- Christopher Brady, Mesa
- Jim Bacon, Paradise Valley
- David Cavazos, Phoenix
- # John Kross, Queen Creek
- * Bryan Meyers, Salt River Pima-Maricopa Indian Community
- * Dave Richert, Scottsdale
- # Michael Celaya for Mark Coronado, Surprise
- Charlie Meyer, Tempe
- * Reyes Medrano, Tolleson
- Gary Edwards, Wickenburg
- Lloyce Robinson, Youngtown
- Robert Samour for John Halikowski, ADOT
- Mike Sabatini for David Smith, Maricopa Co.
- Bryan Jungwirth for David Boggs, Valley Metro/RPTA

* Those members neither present nor represented by proxy.
 # Participated by telephone conference call. + Participated by videoconference call.

Transportation Review Committee: On May 27, 2010, the Transportation Review Committee recommended approval of the proposed update to the federal functional classification system.

MEMBERS ATTENDING

- Peoria: David Moody
- ADOT: Robert Samour for Floyd Roehrich
- Avondale: David Fitzhugh
- Buckeye: Scott Lowe
- Chandler: Dan Cook for Patrice Kraus
- El Mirage: Pat Dennis for Lance Calvert
- Fountain Hills: Randy Harrel
- Gila Bend: Eric Fitzer for Rick Buss
- Gila River: Sreedevi Samudrala for Doug Torres
- Gilbert: Michelle Gramley for Tami Ryall
- Glendale: Terry Johnson
- Goodyear: Cato Esquivel
- Guadalupe: Gino Turrubiarres

- Litchfield Park: Paul Ward for Woody Scoutten
- Maricopa County: John Hauskins
- # Mesa: Scott Butler
- * Paradise Valley: Bill Mead
- Phoenix: Rick Naimark
- Queen Creek: Tom Condit
- RPTA: Bryan Jungwirth
- Scottsdale: Dave Meinhart
- Surprise: Bob Beckley
- Tempe: Chris Salomone
- Valley Metro Rail: John Farry
- * Wickenburg: Rick Austin
- Youngtown: Grant Anderson for Lloyce Robinson

* Not present
 # - Attended by Audioconference

CONTACT PERSON:

Stephen Tate, Transportation Planner III, (602) 254-6300.



Chandler, Arizona
Where Values Make The Difference



May 6, 2010

Mr. Steve Tate, Transportation Planner
Maricopa Association of Governments
302 North First Avenue
Phoenix, Arizona 85003

Subject: Federal Classification of Airport Boulevard

Dear Steve,

Attached is the Functional Classification Worksheet for the proposed federal functional classification of Airport Boulevard as an Urban Collector. Also attached is the requested map showing the location of the roadway.

Please process this request for functional classification through the MAG and ADOT process.

If you need any additional information or have any questions please give me a call at 480-782-3403.

Respectfully,

Daniel W. Cook, PE
Deputy Public Works Director

Attachments: Functional Classification Worksheet, Location Map

Mailing Address:
Mail Stop 403
PO Box 4008
Chandler, Arizona 85244-4008

**Public Works Department
Administration**
Telephone (480) 782-3400
Fax (480) 782-3415

Location:
215 East Buffalo Street
Chandler, Arizona 85225



Functional Classification Worksheet

Road Name: Airport Boulevard **Length:** 1.16 miles

Limits (termini): Germann Road on the north, and Queen Creek Road on the south

Current Functional Classification: Urban Collector

Proposed Functional Classification: Urban Collector

Most recent traffic count (ADT): 3010 ADT **Year:** 2009

Is this request consistent with the transportation plan? (circle one) Yes or No

Has the local agency (owner) committed to funding any construction projects on this route (i.e. new construction, improvements, etc) , excluding or outside of any Federal funds? (circle one) Yes or No

If yes, attach a copy of any documentation to this request

Has the regional planning body approved this request? (circle one) Yes or No

If yes, attach a copy of either a) an adopted resolution passed by the regional planning body; or b) an approved motion shown in the minutes from an official meeting of the regional planning body.

Request Criteria: Describe the reason for this request below (attach additional pages as necessary). If applicable, provide information on any specific traffic generators, population/housing changes (official Census or DES estimates), private or public development in the area, commercial/industrial activity and any other pertinent information that will help to justify this request. Please cite specific data and data sources for all figures used in the justification. Attach a map of the area with the route indicated on the map. Maps may be printed from the ADOT website at <http://tpd.az.gov>. Legible, handwritten notes on the map are acceptable.

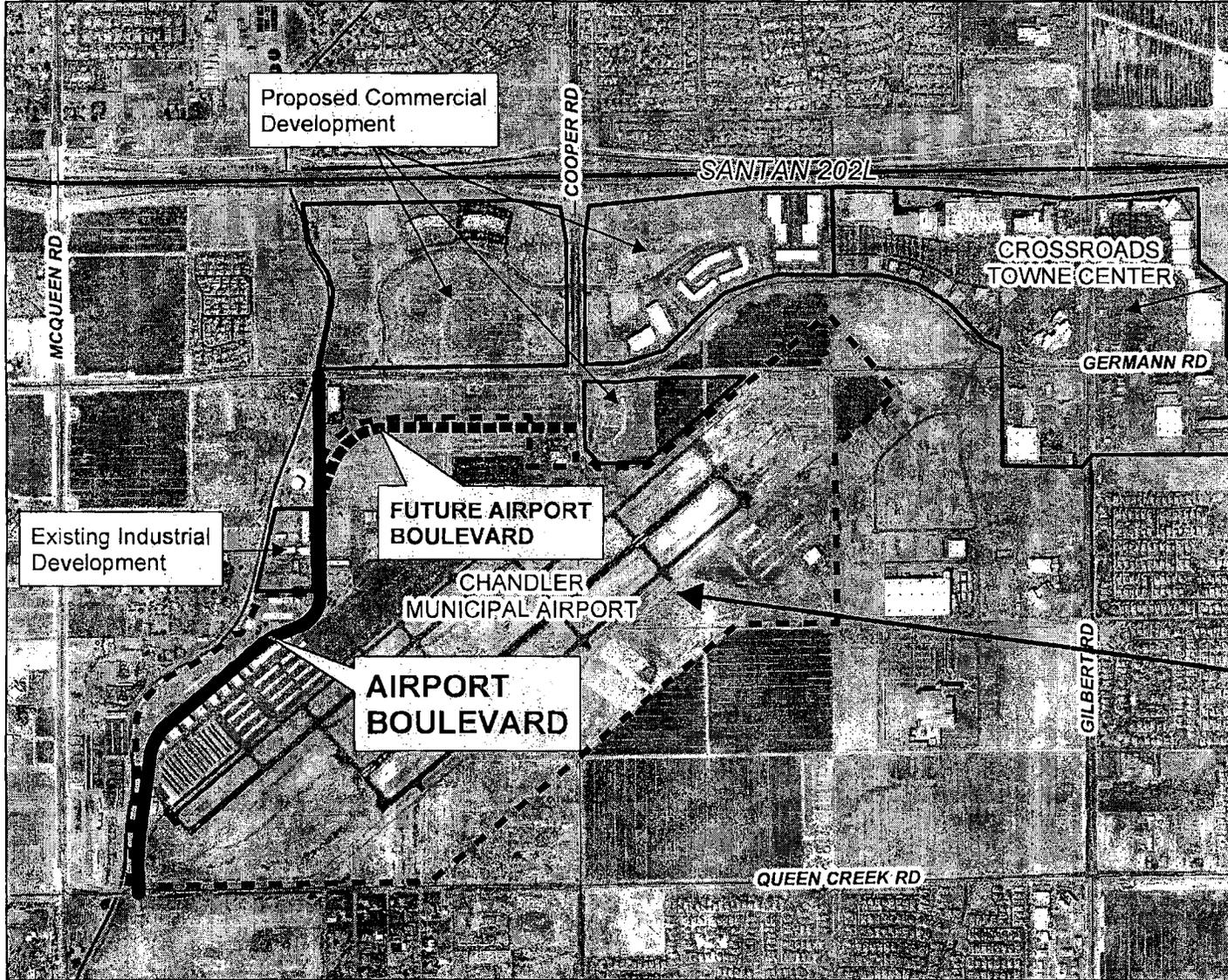
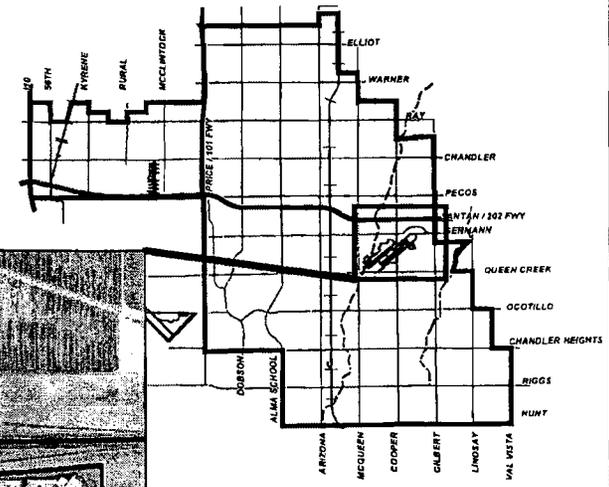
Airport Boulevard is an existing urban collector street in the City of Chandler, AZ. This roadway was constructed in 1985 and extends between the urban principal arterial streets of Queen Creek Road on the south and Germann Road on the north. Airport Boulevard functions as a feeder to the arterial streets from the existing industrial development and the Chandler Municipal Airport (CHD). Chandler Municipal Airport is a very busy general aviation reliever airport to Phoenix Sky Harbor Airport. Chandler Municipal Airport has about 204,000 annual air traffic operations that ranks CHD as the 50th busiest airport and the 21st busiest general aviation airport in the United States. Additionally, CHD has two parallel runways, 449 based aircraft, 234 aircraft storage hangers, 303 aircraft tie-down locations, and generates an economic impact to the City of Chandler of over \$53.4 million based on a 2002 ADOT and Arizona State University study. There is approximately 100,000 square feet of industrial development that feeds traffic directly onto Airport Boulevard. This is currently enough vacant land area to an additional 300,000 square feet of industrial and commercial development. Approximately one mile east of Airport Boulevard is an existing retail development that has over 1.5 million square feet of space. Traffic from the retail development also uses Airport Boulevard to travel between Germann Road and Queen Creek Road. To the north and east of the Airport Boulevard there is a developing commercial center that could develop about 2 to 3 million square feet of commercial space; some of the traffic from this development would also use Airport Boulevard.

Currently the City of Chandler is planning on an improvement to a portion of Airport Boulevard with an estimated cost of \$2.3 million. The ADOT has committed approximately \$1.3 million and the City of Chandler has committed a grant match of approximately \$230,000.



Chandler • Arizona

AIRPORT BOULEVARD



1.5 Million Square Feet Commercial/Retail Space

CHANDLER MUNICIPAL AIRPORT STATISTICS	
Annual Air Traffic Operations (Calendar Year 2009)	204,000
FAA Ranking of all towered airports	50th busiest
FAA Ranking of all GA towered airports	21st busiest
Total Based Aircraft	449
Total number of aircraft storage hangar units	234
Total number of aircraft storage tiedown units	303
Economic Impact of CHD based on 2002 ADOT / ASU analysis	\$53.4 million



MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

Project Changes/Amendments and Administrative Modifications to the Fiscal Year 2008-2012 MAG Transportation Improvement Program

SUMMARY:

The Fiscal Year (FY) 2008-2012 Transportation Improvement Program (TIP) and Regional Transportation Plan 2007 Update were approved by the MAG Regional Council on July 25, 2007. Since that time, there have been requests from member agencies to modify projects in the program.

The proposed amendments and administrative modifications to the FY 2008-2012 TIP are listed in the attached Table. To move forward with project implementation for FY 2010, the Arizona Department of Transportation (ADOT) has requested a new pavement preservation project, a new ITS project, and a new TEA project.

All transit projects, except for VMR09-821T, VMR10-703TR2, and VMR10-704T, were heard and voted on for approval at the MAG Transit Committee. The requests to modify the three projects mentioned above happened after the Transit Committee met when working with MAG on fiscal constraint and finalization of information for FTA grants. All of the transit requests are fiscal requests and MAG has verified no positive or negative impact to associated revenue and expenditures.

The ADOT led projects and transit projects were heard and recommended for approval at the May 27, 2010, Transportation Review Committee. The twelve ARRA related project change requests were presented for the first time at the MAG Management Committee. These requested changes are related to reconciling funds available from lower project bids/awards. There is no negative fiscal impact to the ARRA local program.

Since the MAG Management Committee met, there have been an additional 21 requests for project changes related to ARRA funds and 12 requests for project changes related to federal funded transit projects. The allocations and apportionments for Federal Transit Administration (FTA) funds for 5307, 5309, CMAQ-flex, and STP-flex were finalized, and the MAG Transit Committee met on June 10, 2010, and June 22, 2010, to discuss and recommend approval of project modifications to meet the federal funding apportionments for the 2010 Program of Projects. These new project change requests are highlighted in the attached table.

PUBLIC INPUT:

None.

PROS & CONS:

PROS: Approval of this TIP amendment and administrative modification will allow the projects to proceed in a timely manner.

CONS: None.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: Projects that wish to utilize transportation federal funds need to be shown in the TIP in the year that they expect to commence and may need to undergo an air quality conformity analysis or consultation.

POLICY: This amendment and administrative modification request is in accord with MAG guidelines.

ACTION NEEDED:

Approval of amendments and administrative modifications to the FY 2008-2012 Transportation Improvement Program, and as appropriate, to the Regional Transportation Plan 2007 Update.

PRIOR COMMITTEE ACTIONS:

MAG Transit Committee: On June 22, 2010, the MAG Transit Committee recommended approval to amend and modify the FY 2008-2012 MAG TIP for projects to be funded with 5307, 5309 - FGM, and STP-flex federal funds in 2010 for the 2010 Program of Projects.

MEMBERS ATTENDING

- Phoenix: Debbie Cotton, Chair
 - * ADOT: Mike Normand
 - Avondale: Rogene Hill
 - #Buckeye: Andrea Marquez
 - Chandler: RJ Zeder
 - * El Mirage: Pat Dennis
 - Gilbert: Michelle Gramley for Tami Ryall
 - Glendale: Cathy Colbath
 - * Goodyear: Cato Esquivel
 - Maricopa County: Mitch Wagner
 - * Mesa: Mike James
 - Paradise Valley: William Mead
 - * Peoria: David Moody for Maher Hazine
 - # Queen Creek: Wendy Kaserman
 - Scottsdale: Theresa Huish
 - * Surprise: Michael Celaya
 - Tempe: Robert Yabes for Jyme Sue McLaren
 - # Tolleson: Chris Hagen
 - Valley Metro Rail: John Ferry for Wulf Grote
 - Regional Public Transportation Authority: Carol Ketcherside
- * Members neither present nor represented by proxy. + - Attended by Videoconference
 # - Attended by Audioconference

MAG Management Committee: On June 9, 2010, the MAG Management Committee recommended approval of the projects found in the Highway and Transit tables for amendments and administrative modifications to the FY 2008-2012 Transportation Improvement Program, as appropriate, to the Regional Transportation Plan 2007 Update.

MEMBERS ATTENDING

- Mark Pentz, Chandler, Chair
- Carl Swenson, Peoria, Vice Chair
- #Matt Busby for George Hoffman, Apache Junction
- Charlie McClendon, Avondale
- Stephen Cleveland, Buckeye
- Gary Neiss, Carefree
- * Usama Abujbarah, Cave Creek
- Spencer Isom for B.J. Cornwall, El Mirage
- * Phil Dorchester, Fort McDowell Yavapai Nation
- Rick Davis, Fountain Hills
- Rick Buss, Gila Bend
- * David White, Gila River Indian Community
- Michelle Gramley for Collin DeWitt, Gilbert
- Brent Stoddard for Ed Beasley, Glendale
- #Mark Gaillard for John Fischbach, Goodyear
- Bill Hernandez, Guadalupe
- Darryl Crossman, Litchfield Park
- Christopher Brady, Mesa
- Jim Bacon, Paradise Valley
- David Cavazos, Phoenix
- # John Kross, Queen Creek
- * Bryan Meyers, Salt River Pima-Maricopa Indian Community
- * Dave Richert, Scottsdale
- # Michael Celaya for Mark Coronado, Surprise
- Charlie Meyer, Tempe
- * Reyes Medrano, Tolleson
- Gary Edwards, Wickenburg
- Lloyce Robinson, Youngtown
- Robert Samour for John Halikowski, ADOT
- Mike Sabatini for David Smith, Maricopa County
- Bryan Jungwirth for David Boggs, Valley Metro/RPTA

* Those members neither present nor represented by proxy.

#Participated by telephone conference call.

+ Participated by videoconference call.

Transportation Review Committee: On May 27, 2010, the Transportation Review Committee (TRC) recommended approval of the projects found in the Highway and Transit tables for amendments and administrative modifications to the FY2008-2012 Transportation Improvement Program, as appropriate, to the Regional Transportation Plan 2007 Update.

MEMBERS ATTENDING

Peoria: David Moody
ADOT: Robert Samour for Floyd Roehrich
Avondale: David Fitzhugh
Buckeye: Scott Lowe
Chandler: Dan Cook for Patrice Kraus
El Mirage: Pat Dennis for Lance Calvert
Fountain Hills: Randy Harrel
Gila Bend: Eric Fitzer for Rick Buss
Gila River: Sreedevi Samudrala for Doug Torres
Gilbert: Michelle Gramley for Tami Ryall
Glendale: Terry Johnson
Goodyear: Cato Esquivel
Guadalupe: Gino Turrubiarres
Litchfield Park: Paul Ward for Woody Scoutten

Maricopa County: John Hauskins
Mesa: Scott Butler
* Paradise Valley: Bill Mead
Phoenix: Rick Naimark
Queen Creek: Tom Condit
RPTA: Bryan Jungwirth
Scottsdale: Dave Meinhart
Surprise: Bob Beckley
Tempe: Chris Salomone
Valley Metro Rail: John Farry
* Wickenburg: Rick Austin
Youngtown: Grant Anderson for Lloyce Robinson

EX-OFFICIO MEMBERS ATTENDING

* Street Committee: Dan Cook

* ITS Committee: Debbie Albert

Bicycle/Pedestrian Committee: Peggy Rubach

* Transportation Safety Committee: Kerry Wilcoxon

* Members neither present nor represented by proxy.

+ - Attended by Videoconference

- Attended by Audioconference

MAG Transit Committee: On May 13, 2010, the MAG Transit Committee recommended approval of the requested project changes for GDY08-801T, PHX08-610T, new 2008 preventative maintenance project in Phoenix, PHX07-310T, new 2009 mid life rehab project in Phoenix, VMT09-650T, VMT09-642T, VMR09-825T, and VMR09-826T.

MEMBERS ATTENDING

Phoenix: Debbie Cotton
ADOT: Mike Normand
Avondale: Rogene Hill
Buckeye: Andrea Marquez
* Chandler: RJ Zeder
El Mirage: Pat Dennis
Gilbert: Ken Maruyama for Tami Ryall
Glendale: Cathy Colbath, Chair
Goodyear: Cato Esquivel
Maricopa County: Mitch Wagner
Mesa: Mike James

* Paradise Valley: William Mead
Peoria: David Moody for Maher Hazine
* Queen Creek: Wendy Kaserman
Scottsdale: Theresa Huish
* Surprise: Michael Celaya
Tempe: Jyme Sue McLaren
Tolleson: Chris Hagen
Valley Metro Rail: Jim Mathien for Wulf Grote
Regional Public Transportation Authority:
Bryan Jungwirth for Carol Ketcherside

* Members neither present nor represented by proxy.

+ - Attended by Videoconference

- Attended by Audioconference

CONTACT PERSON:

Eileen O. Yazzie, Transportation Programming Manager, (602) 254-6300.

Request for Project Change - 2008-2012 MAG Transportation Improvement Program

MAG Regional Council 6-22-10

Highway Projects

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
New	ADOT	MAG Regionwide	Evaluation of Intelligent Transportation System (ITS) in the MAG Region	2010		CMAQ			\$ 211,232	\$ 12,768	\$ 224,000	Establish a new ITS evaluation project (Advertise this project in August 2010).
New	ADOT	85: SR85/I-10 TI Ramps	Pavement Preservation	2010		NH			\$ 707,250	\$ 42,750	\$ 750,000	Establish a new pavement preservation project in FY10.
New	ADOT	17: at Central Arizona Project (CAP)	Construct pedestrian bridge	2010		STP-TEA			\$ 2,862,005	\$ 172,995	\$ 3,035,000	Establish a new pedestrian bridge project. Utilize Transportation Enhancements close out funds.

ARRA Related Highway Projects

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
AVN09-801	Avondale	Dysart Rd: I-10 to Indian School Rd.	Prelim. Engineering, design, and construction of mill & replace	2010		ARRA		\$1,681,855			\$1,681,855	De-obligate ARRA funds due to bid savings.
AVN10-801ABS	Avondale	Avondale City Hall Traffic Operations Center (TOC)	Construct Interim TOC	2010	n/a	ARRA		\$328,224			\$328,224	Program ARRA savings to project.
CHN09-801	Chandler	Price Road from Germann Road south to Queen Creek Road	Design and reconstruction of pavement	2010		ARRA		\$1,927,461			\$1,927,461	Transfer \$1,736,438.30 to Chandler/Dobson
CHN120-07C	Chandler	Chandler Blvd/Dobson Road Intersection, and Dobson Road from Chandler Blvd to Frye Road.	Intersection and Capacity Improvement	2010		ARRA		\$3,949,421		\$420,546	\$4,369,967	Transfer ARRA Price Road Savings to Chandler/Dobson Intersection
ELM09-801	El Mirage	Various Locations	Pre-Engineer/Design and mill & replace existing road	2010		ARRA		\$566,808			\$566,808	De-obligate ARRA funds due to bid savings.
ELM08-801ABS	El Mirage	El Mirage Rd: Olive to Cactus	Micro-seal pavement surface	2010		ARRA		\$217,750			\$217,750	Program ARRA savings to project.
FTH07-301	Fountain Hills	Shea Blvd.: Palisades Blvd. to fountain Hills Blvd.	Widen for third (westbound) climbing lane and bicycle lane.	2010	2	STP-MAG	\$131,000		\$2,164,000		\$2,295,000	De-obligate all ARRA funds (\$1,081,614) due to bid savings.
FTH11-101ABS	Fountain Hills	Shea Blvd.: Saguaro Blvd. to Fountain Hills Blvd.	Mill and overlay	2010	2	ARRA	\$65,379	\$1,081,614			\$1,146,993	Program ARRA savings to project.
GLB09-801	Gilbert	Various Locations - Functionally Classified Roadways	Pre-Engineer/Design and Construct Nova Chip Overlays - arterial roadways	2010	14.88 miles	ARRA		\$4,366,610			\$4,366,610	Reduce ARRA/project costs by \$939,703 from \$5,306,313 to \$4,366,610
GLB06-201R											\$0	Reprogramming of ARRA savings,
GLB07-302	Gilbert	Eastern Canal	Santan Vista Trail			ARRA	\$200,000	\$939,703			\$1,139,703	\$939,703 for a current CMAQ

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
GLB05-107R				2010	3 miles	CMAQ			\$1,685,769		\$1,685,769	project
GLN08-801ABS	Glendale	Bell Rd. Pavement Overlay: 51st Ave. to 59th Ave.	Pavement Preservation, sidewalks, curb & gutter overlay, and re-striping	2010	1	ARRA		\$767,480			\$767,480	Program ARRA savings to project.
GLN08-802ABS	Glendale	Various Locations Citywide	Upgrade traffic signal controllers	2010	n/a	ARRA		\$235,750			\$235,750	Program ARRA savings to project.
GLN08-803ABS	Glendale	Bell Rd. Pavement Overlay: 59th Ave. to 70th Ave.	Pavement Preservation, sidewalks, curb & gutter overlay, and re-striping	2010	1	ARRA		\$1,114,548			\$1,114,548	Program ARRA savings to project.
LPK10-801ABS	Litchfield Park	Litchfield Rd: Wigwam Blvd to Camelback Rd	10ft Wide Concrete Pathway	2010	1	ARRA		\$128,146			\$128,146	Program ARRA savings to project.
LPK09-801	Litchfield Park	Various Locations	Pre-Engineer/Design and mill & replace existing road	2010		ARRA		\$455,905			\$455,905	De-obligate ARRA funds due to bid savings.
MES09-801R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$267,227			\$267,227	De-obligate ARRA funds, \$604,433 due to bid savings
MES09-802R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and construct mill & replace pavement	2010		ARRA		\$1,281,173			\$1,281,173	Program additional ARRA funds \$350,759 to project
MES09-803R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$2,336,394			\$2,336,394	De-obligate ARRA funds, \$183,453 due to bid savings
MES09-804R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$1,975,734			\$1,975,734	De-obligate ARRA funds \$318,045 due to bid savings
MES09-805R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$3,476,393			\$3,476,393	Program additional ARRA funds \$205,838 to project
MES09-809	Mesa	Phase 4A	Fiber, cameras, detection, cabinets, controllers.	2010	4	CMAQ/ ARRA		\$549,334	\$644,031		\$1,193,365	Lower CMAQ from \$651,254 to \$644,031, and add ARRA Savings \$549,334.
PEO09-801	Peoria	Various Locations	Pavement Preservation: Major arterial mill, overlay, and re-striping	2010	4.7	ARRA		\$2,042,695			\$2,042,695	De-obligate ARRA funds due to bid savings.
PEO10-805	Peoria	Lake Pleasant Parkway; Dynamite to CAP	Design New Four Lane Arterial Roadway	2010		ARRA/STP-AZ	\$465,719	\$351,706	\$1,907,325		\$2,724,750	Allocate ARRA savings as STP-AZ from Pavement Preservation to LPP design as part of Peoria's local match.
PHX07-315	Phoenix	7th Ave at the ACDC Canal	Multi-Use Underpass	2010		CMAQ/ ARRA	\$258,300	\$900,000	\$1,750,000		\$2,908,300	Program ARRA savings to project.
PHX07-306	Phoenix	7th St. at McDowell	Widen Intersection	2010		CMAQ/ ARRA	\$1,244,000	\$100,000	\$1,256,000		\$2,600,000	Program ARRA savings to project.
YTN09-801	Youngtown	Peoria Ave: 111th Ave west by 1950 ft/ to 115th Ave	Design & Complete Pavement mill & replace	2010		ARRA		\$324,920			\$324,920	De-obligate ARRA funds due to bid savings.
YTN09-802	Youngtown	Peoria Ave & 115th Ave Re-alignment	Design	2010	400'	STP-AZ	\$7,355	\$0	\$137,145	\$0	\$144,500	Program ARRA savings as STP-AZ to project.

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
Transit Projects												
TIP #	Agency	Project Location	Project Description	Fiscal Year	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
GDY08-801T	Goodyear	Citywide	Purchase bus <30 feet - 5 expand	2008		CMAQ	\$37,000		\$438,000	\$0	\$475,000	Delete Project
PHX08-610T	Phoenix	Regionwide	Purchase bus: < 30 foot - 5 expand (dial-a-ride)	2008		5307	\$79,000		\$316,000	\$0	\$395,000	Delete Project
New	Phoenix	Various locations	Preventive maintenance	2008		5307	\$1,696,808		\$6,787,232		\$8,484,040	Increase funds by \$316k from bus purchase program (PHX08-610T).
PHX07-310T	Phoenix	Various locations	Upgrade LNG fuel station - North Division	2009		5307	\$300,000		\$1,200,000		\$1,500,000	Delete Project
New	Phoenix	Various locations	Mid Life Bus Engine Rehabilitation	2009		5307	\$300,000		\$1,200,000		\$1,500,000	Add New project using funds from PHX07-310T.
VMT09-650T	Valley Metro	Regionwide	Reimbursement of bus: standard - 5 expand (Gilbert, Power) advance purchased in 2008	2009		5307	\$0		\$13,630,000	-\$13,630,000	\$0	Reduce from 8 to 5 buses due to reduction in service
VMT09-642T	Valley Metro	Regionwide	Associated capital maintenance	2009		5307	\$2,088,000		\$8,352,000	\$0	\$10,440,000	Increase PM funds due to reduction in bus purchases. Move match from regional to local.
VMR09-821T	Valley Metro Rail	Tempe	Fixed guideway corridor - Tempe South - Preliminary Engineering/FEIS	2009		CMAQ-Flex			\$2,726,000	\$174,000	\$2,900,000	Delete Project
VMR10-703TR2	Valley Metro Rail	Main Street Corridor	Fixed guideway corridor - Central Mesa - Preliminary Engineering/FEIS	2010		CMAQ-Flex			\$10,000,000	\$2,500,000	\$12,500,000	Admin Mod: Adjust federal funding amounts from \$5 mill to \$10 mill.
VMR10-704T	Valley Metro Rail	Tempe	Fixed guideway corridor - Tempe South - Preliminary Engineering/FEIS	2010		CMAQ-Flex			\$2,726,000	\$681,500	\$3,407,500	Admin Mod: Adjust federal funding amounts from \$5 mill to \$2.726 mill.
VMR09-825T	VM Rail	Central Phoenix / East Valley (CP/EV) 20-mile light rail transit starter line	Expand METRO light rail hours of service from 11:00pm on Friday and Saturday evenings to 2:00am on Saturday and Sunday mornings.	2009		CMAQ-Flex			\$300,000	\$0	\$300,000	Delete Project
VMR09-826T	VM Rail	Central Phoenix / East Valley (CP/EV) 20-mile light rail transit starter line	Installation of additional METRO ticket vending machines and stand alone fare validation systems	2009		CMAQ-Flex			\$600,000	\$0	\$600,000	Increase funds by \$300K to \$600K from deleted VMR09-825T project.
GLN10-608T	Glendale	Regionwide	Preventive Maintenance	2010		5307	\$48,873		\$197,144	\$0	\$246,017	Increase project costs

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change
MAG10-901T	MAG	Regionwide	Preventive Maintenance	2010		5307	\$588,607		\$2,074,797	\$0	\$2,663,404	Delete project and allocate funds to operators: Glendale, Peoria, Phoenix, Surprise, Tempe, and Valley Metro/RPTA based on current % split for preventative maintenance.
PEO10-603T	Peoria	Regionwide	Preventive Maintenance	2010		5307	\$16,491		\$66,522	\$0	\$83,013	Increase project costs
PHX10-615T	Phoenix	Regionwide	Preventive Maintenance	2010		5307	\$2,179,489		\$8,791,644	\$0	\$10,971,133	Increase project costs
SUR10-606T	Surprise	Regionwide	Preventive Maintenance	2010		5307	\$2,184		\$8,810	\$0	\$10,994	Increase project costs
TMP10-604T	Tempe	Regionwide	Preventive Maintenance	2010		5307	\$73,044		\$294,645	\$0	\$367,689	Increase project costs
VMT10-901TB	Valley Metro	Regionwide	Purchase bus: standard 40 foot - 19 replace (Tempe)	2010		5307	\$0		\$7,649,825	\$1,799,055	\$9,448,881	Split into 2 projects funded with 5307 and STP-Flex
VMT10-655T	Valley Metro	Regionwide	Preventive Maintenance	2010		5307	\$291,665		\$1,176,519	\$0	\$1,468,184	Increase project costs
GLN11-809TA	Glendale	Bell/L101	Acquire right of way regional park-and-ride (Bell/L101)	2010		5309-FGM			\$2,287,742	\$571,935	\$2,859,677	Decrease project costs and fund remaining costs with STP-Flex
GLN11-809TC	Glendale	Bell/L101	Acquire right of way regional park-and-ride (Bell/L101)	2010		STP-Flex	\$0		\$448,323	\$2,328,341	\$2,776,664	New project funded with STP-flex
VMT10-665T	Valley Metro	Regionwide	Purchase vanpools: 45 replace	2010		STP-Flex	\$0		\$1,482,750	\$0	\$1,482,750	Adjust costs
VMT10-901TC	Valley Metro	Regionwide	Purchase bus: standard 40 foot - 3 replace (Tempe)	2010		5307	\$0		\$1,402,460	\$55,028	\$1,457,487	New project funded with STP-flex

MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

Interim Closeout of the Federal Fiscal Year 2010 MAG Federally Funded Program

SUMMARY:

The interim closeout has established the availability of \$2.204 million in unobligated MAG Federal funds for the current Federal Fiscal Year (FFY).

By May 2010, member agencies submitted requests to defer or delete federal funds from projects for approximately \$14.5 million, which were approved by MAG Regional Council in May 2010. There are an additional \$4.9 million of requests to defer or remove federal funds from projects being heard at the June - July committee cycle. The TRC recommended approval of the new requests on May 27, 2010. Please see Table A for information about requests to defer projects or remove federal funding.

There were 28 projects submitted to MAG for closeout funds. Of the 28 projects, two projects were identified to be advanced; one project was requested to advance a portion of construction funds for a new design project for FFY 2010; seven projects that requested to be advanced and an increase of funds; nine projects scheduled for FFY 2010 that requested additional funds; and nine projects requested new funds. These are identified in Table B titled 'List of Project Funding Requests.'

There were two motions made at the May 28, 2010, Transportation Review Committee (TRC) meeting. The first was to recommend approval of additional projects to be deferred from FFY 2010 to FFY 2011 or later, approval of additional projects requesting removal of Federal funds, and advancing the priority 1 and 1A projects to FFY 2010. The second motion was to recommend allocating the closeout funds deleted from FFY 2010 projects by the cities of Glendale and Mesa to those cities respectively.

Upon further review and coordination with the cities of Glendale and Mesa, Glendale has decided not to delete project GLN07-779, but rather defer it to 2011. Table A has been modified and this request is bolded. This leaves the Glendale allocation at \$63,000. These funds will still go to the Skunk Creek project. Additionally, Mesa has changed its request upon further analysis of project readiness and priorities. The Closeout funds of \$2,141,307 will go toward projects MES10-810 and MES08-604. Table B has been modified and this request is highlighted.

PUBLIC INPUT:

None.

PROS & CONS:

PROS: Approval of these recommendations will allow for additional and accelerated transportation projects to be funded in the MAG region.

CONS: There is no guarantee that sufficient funds will be available in the following fiscal year to cover any or all of the deferred projects. Uncertainty over the reauthorization of the federal legislation makes this problem more acute.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: Action to close out the FFY 2010 MAG federally funded program is needed to ensure that all MAG federal funds are fully used in a timely and equitable manner. These actions may include any necessary amendments or administrative adjustments to the FY 2008-2012 MAG TIP to allow the projects to proceed.

POLICY: Previously adopted MAG policies on the allocation of uncommitted and redistributed federal funds to projects have been followed.

ACTION NEEDED:

Approval of additional projects to be deferred from FFY 2010 to FFY 2011 or later and additional projects requesting removal of federal funds; advancing the three projects submitted for priority 1 and 1A projects to FFY 2010; allocating the \$2.204 million from deleted projects in FFY 2010 by the cities of Glendale and Mesa to fund Glendale's GLN09-609 with \$63,000 and fund Mesa's, MES08-604 and MES10-810 with \$2,141,307; and amending and modifying the FY 2008-2012 Transportation Improvement Program, and as appropriate, the Regional Transportation Plan 2007 Update.

PRIOR COMMITTEE ACTIONS:

MAG Management Committee: On June 9, 2010, the MAG Management Committee recommended approval of additional projects to be deferred from FFY 2010 to FFY 2011 or later and additional projects requesting removal of federal funds; advancing the three projects submitted for priority 1 and 1A projects to FFY 2010; allocating the \$2.204 million from deleted projects in FFY 2010 by the cities of Glendale and Mesa to fund Glendale's GLN09-609 with \$63,000 and fund Mesa's, MES08-604 and MES10-810 with \$2,141,307; and amending and modifying the FY 2008-2012 Transportation Improvement Program, and as appropriate, the Regional Transportation Plan 2007 Update.

MEMBERS ATTENDING

- | | |
|---|--|
| Mark Pentz, Chandler, Chair | Darryl Crossman, Litchfield Park |
| Carl Swenson, Peoria, Vice Chair | Christopher Brady, Mesa |
| # Matt Busby for George Hoffman,
Apache Junction | Jim Bacon, Paradise Valley |
| Charlie McClendon, Avondale | David Cavazos, Phoenix |
| Stephen Cleveland, Buckeye | # John Kross, Queen Creek |
| Gary Neiss, Carefree | * Bryan Meyers, Salt River Pima-Maricopa
Indian Community |
| * Usama Abujbarah, Cave Creek | * Dave Richert, Scottsdale |
| Spencer Isom for B.J. Cornwall, El Mirage | # Michael Celaya for Mark Coronado, Surprise |
| * Phil Dorchester, Fort McDowell Yavapai
Nation | Charlie Meyer, Tempe |
| Rick Davis, Fountain Hills | * Reyes Medrano, Tolleson |
| Rick Buss, Gila Bend | Gary Edwards, Wickenburg |
| * David White, Gila River Indian Community | Lloyce Robinson, Youngtown |
| Michelle Gramley for Collin DeWitt, Gilbert | Robert Samour for John Halikowski, ADOT |
| Brent Stoddard for Ed Beasley, Glendale | Mike Sabatini for David Smith,
Maricopa County |
| # Mark Gaillard for John Fischbach, Goodyear | Bryan Jungwirth for David Boggs,
Valley Metro/RPTA |
| Bill Hernandez, Guadalupe | |

* Those members neither present nor represented by proxy.
Participated by telephone conference call. + Participated by videoconference call.

Transportation Review Committee: There were two motions made at the May 28, 2010 Transportation Review Committee (TRC) meeting. The first was to recommended approval of additional projects to be deferred from FFY 2010 to FFY 2011 or later, approve additional projects requesting removal of Federal funds, and to advance the priority 1 and 1A projects to FFY 2010. The second was to recommend to allocate the closeout funds deleted from FFY 2010 projects by the cities of Glendale and Mesa to those cities respectively. The Glendale allocation of \$196,035 to be programmed for the Skunk Creek project adding that the City of Mesa should coordinate with MAG on programming Mesa's allocation of the \$2,141,307.

MEMBERS ATTENDING

Peoria: David Moody
ADOT: Robert Samour for Floyd
Roehrich
Avondale: David Fitzhugh
Buckeye: Scott Lowe
Chandler: Dan Cook for Patrice Kraus
El Mirage: Pat Dennis for Lance Calvert
Fountain Hills: Randy Harrel
Gila Bend: Eric Fitzer for Rick Buss
Gila River: Sreedevi Samudrala for Doug
Torres
Gilbert: Michelle Gramley for Tami Ryall
Glendale: Terry Johnson
Goodyear: Cato Esquivel
Guadalupe: Gino Turrubiarres

Litchfield Park: Paul Ward for Woody
Scoutten
Maricopa County: John Hauskins
Mesa: Scott Butler
* Paradise Valley: Bill Mead
Phoenix: Rick Naimark
Queen Creek: Tom Condit
RPTA: Bryan Jungwirth
Scottsdale: Dave Meinhart
Surprise: Bob Beckley
Tempe: Chris Salomone
Valley Metro Rail: John Farry
* Wickenburg: Rick Austin
Youngtown: Grant Anderson for Lloyce
Robinson

EX-OFFICIO MEMBERS ATTENDING

* Street Committee: Dan Cook
* ITS Committee: Debbie Albert

Bicycle/Pedestrian Committee: Peggy
Rubach
* Transportation Safety Committee: Kerry
Wilcoxon

* Members neither present nor represented by proxy.

+ - Attended by Videoconference
- Attended by Audioconference

CONTACT PERSON:

Eileen Yazzie, (602) 254-6300.

TABLE A
Federal Fiscal Year 2010 Closeout - Requests to Defer Projects, Remove Federal Funds/Delete Projects

PROJECTS NEEDING ACTION

Requests to Defer Projects - First Request

Per the Programming Principles, Member agencies are allowed a one time deferral without justification.

Closeout Request	Section	Agency	ID	Location	Work	Length	Funding	FedCost	LocCost	Total	MAG Mode	Original Yr Prog. for Const.	# of Times Deferred (not including this request)	Year Requested to be deferred	Have Letter?	Will the project be complete?
Defer	Highway	El Mirage	ELM13-903	Dysart Ranchettes area: Varney Rd, Peoria Ave, Dysart Rd, El Mirage	Paving dirt roads	3.4	CMAQ	\$ 1,250,000	\$ 1,750,000	\$ 3,000,000	AQ/TDM	2010	0	2011	Not needed	
Defer	Highway	Glendale	GLN07-779	Various Locations: Camelback Rd, Litchfield Rd, Olive Ave, Greenway Rd, 83rd Ave, 75th Ave	Pave dirt shoulders	5.17	CMAQ	\$ 133,035	\$ 133,035	\$ 266,070	AQ or TDM	2008	2	2011	Pending	
Defer	Highway	Phoenix	PHX10-845	Salt River: 24th Street to I-10/Tempe Drain	Construct Multi-use path		CMAQ	\$ 801,606	\$ 480,100	\$ 1,281,706	Bike/Ped	2010	0	2011	Not needed	
Defer	Highway	Tempe	TMP10-620	Broadway Rd: Rural Rd to Mill Ave	Construct pedestrian and bicycle facilities improvements	1	CMAQ	\$ 2,571,780	\$ 2,571,780	\$ 5,143,560	Bike/Ped	2010	0	2011	Not needed	
Defer	Highway	Tempe	TMP10-629	Salt River: I-10/Tempe Drain to Priest	Construct Multi-use path	1.1	CMAQ	\$ 120,000	\$ 400,000	\$ 520,000	Bike/Ped	2010	0	2011	Not needed	
								\$ 4,876,421								

Requests to Remove Federal Funds from the Project/Delete Project

Closeout Request	Section	Agency	ID	Location	Work	Length	Funding	FedCost	LocCost	Total	MAG Mode	Original Yr Prog. for Const.	# of Times Deferred (not including this request)	Year Requested to be deferred	Have Letter?	Will the project be complete?
Delete	Highway	Glendale	GLN08-605	Glendale Ave: Loop 101 to Luke AFB	Pave access points	5	CMAQ	\$ 63,000	\$ 27,000	\$ 90,000	AQ or TDM					No
								\$ 63,000								

PROJECTS ALREADY APPROVED BY REGIONAL COUNCIL TO BE DEFERRED

Requests to Defer Projects - Second Time or More

Per the Programming Principles, If a project is requesting to be deferred for the second time or more, a deferral justification letter is to be submitted. Please see attached letters.

Closeout Request	Section	Agency	ID	Location	Work	Length	Funding	FedCost	LocCost	Total	MAG Mode	Original Yr Prog. for Const.	# of Times Deferred (not including this request)	Year Requested to be deferred	Have Letter?	Will the project be complete?
Defer	Highway	Chandler	CHN07-601	Commonwealth Ave: Hamilton St to Ithaca	Pave dirt road	0.2	CMAQ	\$ 325,000	\$ 1,075,000	\$ 1,400,000	AQ or TDM	2007	2	2011	Yes	
Defer	Highway	El Mirage	ELM09-802	125th Ave and 127th Ave: Varney Rd to Peoria Ave	Pave unpaved roads	1	CMAQ	\$ 381,031	\$ 1,102,252	\$ 1,483,283	AQ or TDM	2009	1	2011	Yes	
Defer	Highway	Fort McDowell Yavapai Nation	FTM09-903C	Various Locations on Fort McDowell Yavapai Nation	Construct Pave dirt road	2.5	CMAQ	\$ 375,000	\$ 24,000	\$ 399,000	AQ or TDM	2007	3	2011	Yes	
Defer	Highway	Glendale	GLN09-610	Downtown alley north of Glendale Ave between 57th Ave and 57th Dr	transform existing service alleyway into a safe environment for pedestrian circulation and limited vehicular traffic	0.04	CMAQ	\$ 240,721	\$ 103,166	\$ 343,887	Ped	2009	1	2012	Yes	

TABLE A

Federal Fiscal Year 2010 Closeout - Requests to Defer Projects, Remove Federal Funds/Delete Projects

Defer	Highway	Glendale	GLN07-311	Alley 250 ft north of Glendale Ave: 58th Ave to 57th Dr	Design and construct alley improvements and pedestrian walkway	0.05	CMAQ	\$ 75,000	\$ 75,000	\$ 150,000	Ped	2007	2	2012	Yes	
Defer	Highway	Mesa	MES07-315	Southern Ave at Country Club Dr	Add 1 right turn lane and three bus pullouts.	0.45	CMAQ	\$ 910,000	\$ 3,437,000	\$ 4,347,000	Street	2007	2	2013	Yes	
Defer	Highway	Mesa	MES08-603	Longmore: Broadway Rd to Main St (EVIT)	Design and construct bicycle path to connect Broadway Rd with Main St and the Light Rail Station	0.5	CMAQ	\$ 1,082,739	\$ 388,961	\$ 1,471,700	Bicycle	2008	2	2011	Yes	
Defer	Highway	Phoenix	PHX09-619	19th Ave at Greenway Rd	Construct multi-use path and bridge (phase 2)	0.04	CMAQ	\$ 1,010,000	\$ 2,174,100	\$ 3,184,100	Bicycle	2009	1	2011	Yes	
Defer	Highway	Phoenix	PHX09-624	Various locations	Construct regional ITS fiber optic backbone, phase B-1	30	CMAQ	\$ 665,000	\$ -	\$ 665,000	ITS	2009	1	2011	Yes	
Defer	Highway	Phoenix	PHX09-871	Various locations	Pave unpaved alleys	18	CMAQ	\$ 466,667	\$ 200,000	\$ 666,667	AQ or TDM	2009	1	2011	Yes	
Defer	Highway	Phoenix	PHX09-872	Various locations	Pave unpaved roads	3	CMAQ	\$ 1,050,000	\$ 450,000	\$ 1,500,000	AQ or TDM	2009	1	2011	Yes	
Defer	Highway	Scottsdale	SCT07-606	Dynamite Blvd: Pima Road to Alma School Road	Install Vertical Curb and Gutter	3	CMAQ	\$ 500,000	\$ 500,000	\$ 1,000,000	AQ or TDM	2007	3	2011	Yes	
Defer	Highway	Surprise	SUR09-820	Rural Area West of 219th Ave between Pinnacle Peak & Deer Valley	Pave unpaved roads	3.27	CMAQ	\$ 1,602,302	\$ 686,700	\$ 2,289,002	AQ or TDM	2009	1	2011	Yes	
								\$ 8,683,460								

PROJECTS ALREADY APPROVED BY REGIONAL COUNCIL TO BE DEFERRED

Requests to Defer Projects - First Request
Per the Programming Principles, Member agencies are allowed a one time deferral without justification.

Defer	Highway	Buckeye	BKY10-801	Miller Road: Hazen Rd to I-10 and Monroe (MC85): Miller Rd to Apache Rd	Interconnect Traffic Signals	6	CMAQ	\$ 210,000	\$ 90,000	\$ 300,000	ITS	2010	0	2013	Not needed	
Defer	Highway	Buckeye	BKY11-801	North Watson Road and MC85 Phase I and Phase II	Pave Unpaved Road	0.22	CMAQ	\$ 64,456	\$ 3,896	\$ 68,352	AQ or TDM	2011	0	2013	Not needed	
Defer	Highway	Chandler	CHN13-901	Various Locations in the City of Chandler	Paving dirt alleys	10	CMAQ	\$ 350,000	\$ 589,000	\$ 939,000	AQ or TDM	2010	0	2011	Not needed	
Defer	Highway	Fort McDowell Yavapai Nation	FTM13-901	Various Locations on Fort McDowell Yavapai Nation	Paving dirt roads	4.7	CMAQ	\$ 700,000	\$ 1,650,000	\$ 2,350,000	AQ or TDM	2010	0	2011	Not needed	
Defer	Highway	Phoenix	PHX10-633	Various locations	Construct regional ITS fiber optic backbone, phase B-2	30	CMAQ	\$ 665,000	\$ -	\$ 665,000	ITS	2010	0	2011	Yes-Not Needed	
Defer	Highway	Phoenix	PHX13-904	Various Locations in the City of Phoenix: 44 miles of dirt alleys	Paving dirt alleys	44	CMAQ	\$ 1,200,000	\$ 920,000	\$ 2,120,000	AQ or TDM	2010	0	2011	Yes-Not Needed	
Defer	Highway	Scottsdale	SCT10-617R	Scottsdale Rd: Earll Dr to Chaparral Rd	Upgrade sidewalks and add bicycle lanes	3	CMAQ	\$ 510,696	\$ 2,540,741	\$ 3,051,437	Ped	2010	0	2011	Yes-Not Needed	
								\$ 3,700,152								

PROJECTS ALREADY APPROVED BY REGIONAL COUNCIL TO BE DELETED

Requests to Remove Federal Funds from the Project/Delete Project

Delete	Highway	Mesa	MES06-203C	Pepper Pl: Lewis St to Robson St	Construct multi-use path	0.5	CMAQ	\$ 305,961	\$ 93,039	\$ 399,000	Bicycle	Request to Delete Project			No	
Delete	Highway	Mesa	MES07-314	South Canal: Val Vista Dr to Greenfield Rd	Construct multi-use path. Development of multi-use path system (MUP)	1.25	CMAQ	\$ 541,800	\$ 232,200	\$ 774,000	Bicycle	Request to Delete Project			No	
Delete	Highway	Mesa	MES09-605	Grand St: Broadway Rd to 6th Ave (Nuestro neighborhood phase 1)	Improve pedestrian facilities	0.25	CMAQ	\$ 441,041	\$ 189,018	\$ 630,059	Ped	Request to Delete Project			No	
Delete	Highway	Mesa	MES10-608	South Canal: McDowell Rd to Val Vista Dr	Construct new multi-use path on the north bank	1.5	CMAQ	\$ 852,505	\$ 568,337	\$ 1,420,842	Bicycle	Request to Delete Project			No	
								\$ 2,141,307								

TABLE B

FY 2010 Closeout - List of Project Funding Requests

Priority Key: 1 = Advance, 1A = Advance portion of construction segment for a new design phase, 2 = Advance and request additional funding, 3 = New CMAQ project requested.

Priority	Agency	TIPIDN	Mode	Location	Project Description	Current Programming			Change in FY 2010 Federal Funding			
						Year	Federal	Local	Total	New/Increased	Advanced	Total
1	Litchfield Park	LPK13-901	AQ or TDM	Wigwam Boulevard at Litchfield Road Bypass	Acquire right of way and construct multi-use underpass	2013	800,000	471,000	1,271,000	-	800,000	800,000
1	Surprise	SUR11-715	ITS	Peoria Ave: Litchfield Rd to Jackrabbit Rd	Design and construct fibre optic cable interconnection of existing and future ITS facilities	2011	700,000	1,000,000	1,700,000	-	700,000	700,000
1A	Surprise	SUR11-714*	Pedestrian	Bell Road US60(Grand Avenue) to 114th Avenue	Design of Multiuse Path	2012	1,000,000	500,000	1,500,000	-	175,000	175,000
1-2	Chandler	CHN08-610C	Bicycle	Loop 101 (Price Freeway) at Galveston Street	Construct multi-use path and bridge over the Loop 101 (Price Freeway) at Galveston Street	2012	1,164,992	1,315,808	2,480,800	318,250	1,164,992	1,483,242
1-2	Chandler	CHN14-101	Bicycle	Chandler: Galveston Street Bicycle & Pedestrian Bridge	Construct Bicycle/Pedestrian Bridge, Phase 2	2014	2,056,758	64,942	2,121,700	-	2,056,758	2,056,758
1-2	Mesa	MES 12-815	ITS	Brown Rd., Higley Rd. (ITS Phase 5)	Fiber, cameras, detection, cabinets, controllers.	2012	659,994	1,934,406	2,594,400	1,934,406	659,994	2,594,400
1-2	Mesa	MES 11-703	ITS	Citywide	New Cabinets, Controllers	2011	700,000	500,000	1,200,000	500,000	700,000	1,200,000
1-2	Mesa	MES 13-905	Bicycle	Consolidated Canal: Lindsay Road to Baseline Road	Design and Construct 10-foot wide concrete path	2012	1,099,000	471,000	1,570,000	471,000	1,099,000	1,570,000
1-2	Mesa	MES08-602R	Pedestrian	North-South Pedestrian Path: Phase 2 - 1st Street to Convention Center	Construct pedestrian improvements	2011	253,673	83,717	337,390	83,717	253,673	337,390
1-2	Phoenix	PHX13-903	Pedestrian	32 nd St: Washington St to McDowell Rd	Design 32 nd St Pedestrian Enhancement (Washington St to McDowell Rd)	2013	373,000	480,150	853,150	480,150	373,000	853,150

TABLE B

FY 2010 Closeout - List of Project Funding Requests

Priority Key: 1 = Advance, 1A = Advance portion of construction segment for a new design phase, 2 = Advance and request additional funding, 3 = New CMAQ project requested.

Priority	Agency	TIIDN	Mode	Location	Project Description	Current Programming				Change in FY 2010 Federal Funding		
						Year	Federal	Local	Total	New/Increased	Advanced	Total
2	Buckeye	BK07-704	AQ or TDM	Various Locations: MC85/Monroe, Southern Ave., Apache Rd.	Pave Dirt Shoulders	2010	\$113,000	\$187,000	\$300,000	169,900	-	169,900
2	Glendale	GLN09-609	Bicycle	Skunk Creek/Union Hills Dr.	Design and construct multi- use path under Union Hills Dr.	2010	161,772	147,228	309,000	196,035	-	196,035
2	Mesa	MES10-810	ITS	Alma School Rd., Southern Ave., Baseline Rd., Guadalupe Rd. (ITS Phase 4B)	Fiber, cameras, detection, cabinets, controllers.	2010	709,973	1,893,027	2,603,000	1,670,307	-	1,670,307
2	Mesa	MES09-809	ITS	Dobson Road, Broadway Road, Alma School Road (ITS Phase 4A)	Fiber, cameras, detection, cabinets, controllers.	2010	651,254	992,746	1,644,000	992,746	-	992,746
2	Mesa	MES08-604	ITS	East Mesa (Various Locations)	New CCTV, Video Detection Cameras	2010	838,700	359,400	1,198,100	359,400	-	359,400
2	Phoenix	PHX07-315	Pedestrian	7 th Ave at the ACDC Canal	Construct multi-use underpass	2010	1,750,000	1,158,300	2,908,300	1,189,932	-	1,189,932
2	Tempe	TMP10-620	Pedestrian	Broadway Road - Rural Road to Mill Avenue	Construct bicycle and pedestrian facility improvements	2010	2,571,780	2,571,780	5,143,560	1,200,000	-	1,200,000
2	Tempe	TMP10-803	ITS	Citywide	Install Video Detection System	2010	305,568	138,969	444,537	138,969	-	138,969
2	Tempe	TMP10-629	Pedestrian	Salt River: SR143 Hohokam Freeway to Priest Drive	Construct multi-use path	2010	400,000	40,000	440,000	1,250,000	-	1,250,000
3	Avondale	New	Transit	City of Avondale City Center Design	100% design plans for the MAG/Avondale Transit Study					\$630,000	\$0	\$630,000
3	Avondale	New	ITS	City of Avondale MOSC Building	Construction of Interim Traffic Operation Center					888,000	-	888,000
3	Glendale	New	Pedestrian	Downtown Alleyway: 58th Ave. to 57th Ave.	Design downtown alleyway for safe pedestrian circulation					\$200,000	\$0	\$200,000
3	Glendale	New	ITS	Various Locations Citywide	22 CCTV cameras and 6 Ethernet installations					\$207,460	\$0	\$207,460
3	Glendale	New	ITS	Various Locations Citywide	Modernize traffic signals					\$707,250	\$0	\$707,250

TABLE B

FY 2010 Closeout - List of Project Funding Requests

Priority Key: 1 = Advance, 1A = Advance portion of construction segment for a new design phase, 2 = Advance and request additional funding, 3 = New CMAQ project requested.

Priority	Agency	TIPIDN	Mode	Location	Project Description	Current Programming				Change in FY 2010 Federal Funding		
						Year	Federal	Local	Total	New/Increased	Advanced	Total
3	Glendale	New	ITS	Various Locations Citywide	Upgrade traffic signal controllers					\$235,750	\$0	\$235,750
3	Scottsdale	New	ITS	Various Locations	Purchase and install traffic signal controllers and cabinets					\$550,000	\$0	\$550,000
3	Surprise	New	ITS	Surprise TMC	Develop an ITS Strategic Plan document in line with regional ITS planning efforts.					\$190,000	\$0	\$190,000
3	Valley Metro	New	Transit	Arizona Avenue/Country Club Drive	First year operating costs for the Arizona Avenue/Country Club Drive bus rapid transit (BRT) route.					\$811,288	\$0	\$811,288
Total							\$ 16,309,464	\$ 14,309,473	\$ 30,618,937	\$ 15,374,560	\$ 7,982,417	\$ 23,356,977

MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

Conformity Consultation

SUMMARY:

The Maricopa Association of Governments is conducting consultation on a conformity assessment for an amendment and administrative modification to the FY 2008-2012 MAG Transportation Improvement Program (TIP). The proposed amendment and administrative modification involve several projects, including a new Arizona Department of Transportation pavement preservation project on State Route 85, a new region wide Intelligent Transportation Systems project, a new Transportation Enhancement project located at Interstate-17 and the Central Arizona Project, and transit projects. The amendment includes projects that may be categorized as exempt from conformity determinations. The administrative modification includes minor project revisions that do not require a conformity determination. Comments on the conformity assessment are now requested by June 30, 2010.

Since the June 9, 2010 Management Committee meeting, MAG has received requests to add several new American Recovery and Reinvestment Act (ARRA) projects and Federal Transit Administration (FTA) funded projects in FY 2010 to the amendment and administrative modification. The amendment includes projects that may be categorized as exempt from conformity determinations. The administrative modification includes minor project revisions that do not require a conformity determination. A revised table containing all of the projects is provided in the attached interagency consultation memorandum.

PUBLIC INPUT:

An opportunity for public comment was provided at the June 9, 2010 Management Committee meeting and no public comments were received.

PROS & CONS:

PROS: Interagency consultation for the amendment and administrative modification notifies the planning agencies of project modifications to the TIP.

CONS: The review of the conformity assessment requires additional time in the project approval process.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: The amendment and administrative modification may not be considered until the consultation process for the conformity assessment is completed.

POLICY: Federal transportation conformity regulations require interagency consultation on development of the transportation plan, TIP, and associated conformity determinations to include a process involving the Metropolitan Planning Organization, State and local air quality planning agencies, State and local transportation agencies, Environmental Protection Agency, Federal Highway Administration, and the Federal Transit Administration. Consultation on the conformity assessment has been conducted in accordance with federal regulations, MAG Conformity Consultation Processes adopted by the Regional Council in February 1996 and MAG Transportation Conformity Guidance and Procedures adopted by the Regional Council in March 1996. In addition, federal guidance is followed in response to court rulings regarding transportation conformity.

ACTION NEEDED:

Consultation.

PRIOR COMMITTEE ACTIONS:

Management Committee: This item was on the agenda of the June 9, 2010 MAG Management Committee meeting for consultation.

MEMBERS ATTENDING

- | | |
|---|--|
| Mark Pentz, Chandler, Chair | Bill Hernandez, Guadalupe |
| Carl Swenson, Peoria, Vice Chair | Darryl Crossman, Litchfield Park |
| # Matt Busby for George Hoffman,
Apache Junction | Christopher Brady, Mesa |
| Charlie McClendon, Avondale | Jim Bacon, Paradise Valley |
| Stephen Cleveland, Buckeye | David Cavazos, Phoenix |
| Gary Neiss, Carefree | # John Kross, Queen Creek |
| * Usama Abujbarah, Cave Creek | * Bryan Meyers, Salt River Pima-Maricopa
Indian Community |
| Spencer Isom for B.J. Cornwall, El
Mirage | * Dave Richert, Scottsdale |
| * Phil Dorchester, Fort McDowell Yavapai
Nation | # Michael Celaya for Mark Coronado,
Surprise |
| Rick Davis, Fountain Hills | Charlie Meyer, Tempe |
| Rick Buss, Gila Bend | * Reyes Medrano, Tolleson |
| * David White, Gila River Indian
Community | Gary Edwards, Wickenburg |
| Michelle Gramley for Collin DeWitt,
Gilbert | Lloyce Robinson, Youngtown |
| Brent Stoddard for Ed Beasley, Glendale | Robert Samour for John Halikowski,
ADOT |
| # Mark Gaillard for John Fischbach,
Goodyear | Mike Sabatini for David Smith,
Maricopa County |
| | Bryan Jungwirth for David Boggs,
Valley Metro/RPTA |

* Those members neither present nor represented by proxy.
Participated by telephone conference call. + Participated by videoconference call.

CONTACT PERSON:

Dean Giles, Air Quality Planning Program Specialist III, (602) 254-6300.



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June 23, 2010

TO: Leslie Rogers, Federal Transit Administration
Robert Hollis, Federal Highway Administration
John Halikowski, Arizona Department of Transportation
Benjamin Grumbles, Arizona Department of Environmental Quality
David Boggs, Regional Public Transportation Authority
Debbie Cotton, City of Phoenix Public Transit Department
Stephen Banta, Valley Metro Rail
Max Porter, Maricopa County Air Quality Department
Maxine Brown, Central Arizona Association of Governments
Donald Gabrielson, Pinal County Air Quality Control District
Gregory Nudd, U.S. Environmental Protection Agency, Region IX
Other Interested Parties

FROM: Dean Giles, Air Quality Planning Program Specialist

SUBJECT: CONSULTATION ON A CONFORMITY ASSESSMENT FOR A PROPOSED
AMENDMENT AND ADMINISTRATIVE MODIFICATION TO THE FY 2008-2012 MAG
TRANSPORTATION IMPROVEMENT PROGRAM

On June 1, 2010, the Maricopa Association of Governments distributed a memorandum for consultation on a conformity assessment for an amendment and administrative modification to the FY 2008-2012 MAG Transportation Improvement Program (TIP). The proposed amendment and administrative modification involve several projects, including a new Arizona Department of Transportation pavement preservation project on State Route 85, a new region wide Intelligent Transportation Systems project, a new Transportation Enhancement project located at Interstate-17 and the Central Arizona Project, and transit projects. Since that time, MAG has received requests to add several new American Recovery and Reinvestment Act (ARRA) projects and Federal Transit Administration (FTA) funded projects in FY 2010 to the amendment and administrative adjustment. A revised table containing all of the projects is attached. Comments on the conformity assessment are now requested by June 30, 2010.

MAG has reviewed the projects for compliance with the federal conformity rule and has found that consultation is required on the conformity assessment. The amendment includes projects that may be categorized as exempt from conformity determinations. The administrative modification includes minor project revisions that do not require a conformity determination. The conformity finding of the TIP and the associated Regional Transportation Plan 2007 Update, as amended, that was made by the Federal Highway Administration and Federal Transit Administration on March 9, 2010 remains unchanged by this action. The conformity assessment is being transmitted for consultation to the agencies listed above and other interested parties. If you have any questions or comments, please contact me at (602) 254-6300.

A Voluntary Association of Local Governments in Maricopa County

City of Apache Junction ▲ City of Avondale ▲ Town of Buckeye ▲ Town of Carefree ▲ Town of Cave Creek ▲ City of Chandler ▲ City of El Mirage ▲ Fort McDowell Yavapai Nation ▲ Town of Fountain Hills ▲ Town of Gila Bend
Gila River Indian Community ▲ Town of Gilbert ▲ City of Glendale ▲ City of Goodyear ▲ Town of Guadalupe ▲ City of Litchfield Park ▲ Maricopa County ▲ City of Mesa ▲ Town of Paradise Valley ▲ City of Peoria ▲ City of Phoenix
Town of Queen Creek ▲ Salt River Pima-Maricopa Indian Community ▲ City of Scottsdale ▲ City of Surprise ▲ City of Tempe ▲ City of Tolleson ▲ Town of Wickenburg ▲ Town of Youngtown ▲ Arizona Department of Transportation

Attachment

cc: Ira Domsy, Arizona Department of Environmental Quality
Jennifer Toth, Arizona Department of Transportation
Mark Hodges, Arizona Department of Transportation

ATTACHMENT

CONFORMITY ASSESSMENT FOR A PROPOSED AMENDMENT AND ADMINISTRATIVE MODIFICATION TO THE FY 2008-2012 MAG TRANSPORTATION IMPROVEMENT PROGRAM

The federal transportation conformity rule (40 CFR 93.105) requires interagency consultation when making changes to a Transportation Improvement Program (TIP) and Transportation Plan. The consultation processes are also provided in the Arizona Conformity Rule (R18-2-1405). This information is provided for consultation as outlined in the MAG Conformity Consultation Processes document adopted by the MAG Regional Council on February 28, 1996. In addition, federal guidance is followed in response to court rulings regarding transportation conformity.

The amendment includes projects that may be categorized as exempt from conformity determinations. Types of projects considered exempt are defined in the federal transportation conformity rule at 40 CFR 93.126. The administrative modification includes minor project revisions that do not require a conformity determination. Examples of minor project revisions include design, right-of-way, and utility projects. The proposed amendment and administrative modification to the FY 2008-2012 MAG Transportation Improvement Program includes the projects on the attached table. The project number, agency, and description is provided, followed by the conformity assessment.

MAG has reviewed the projects for compliance with the federal conformity rule and consultation is required on the conformity assessment. The projects are not expected to create adverse emission impacts or interfere with Transportation Control Measure implementation. The conformity finding of the TIP and the associated Regional Transportation Plan that was made by the Federal Highway Administration and Federal Transit Administration on March 9, 2010 remains unchanged by this action.

Amendment and Administrative Modification to the FY 2008-2012 MAG Transportation Improvement Program

Highway Projects

TIP #	Agency	Project Location	Project Description	FY	Length (miles)	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
New	ADOT	MAG Region wide	Evaluation of Intelligent Transportation System (ITS) in the MAG Region	2010		CMAQ			\$ 211,232	\$ 12,768	\$ 224,000	Establish a new ITS evaluation project (Advertise this project in August 2010).	The project is considered exempt under the category "Specific activities which do not involve or lead directly to construction, such as: Planning and technical studies." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
New	ADOT	85: SR85/I-10 TI Ramps	Pavement Preservation	2010		NH		\$ 707,250		\$ 42,750	\$ 750,000	Establish a new pavement preservation project in FY10.	The project is considered exempt under the category "Pavement resurfacing and/or rehabilitation." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
New	ADOT	17: at Central Arizona Project (CAP)	Construct pedestrian bridge	2010		STP-TEA			\$ 2,862,005	\$ 172,995	\$ 3,035,000	Establish a new pedestrian bridge project. Utilize Transportation Enhancements close out funds.	The project is considered exempt under the category "Bicycle and pedestrian facilities." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
AVN09-801	Avondale	Dysart Rd: I-10 to Indian School Rd	Prelim. Engineering, design, and construction of mill & replace	2010		ARRA		\$ 1,681,855			\$ 1,681,855	De-obligate ARRA funds due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
AVN10-801ABS	Avondale	Avondale City Hall Traffic Operations Center (TOC)	Construct Interim TOC	2010	n/a	ARRA		\$ 328,224			\$ 328,224	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
CHN09-801	Chandler	Price Road from Germann Road south to Queen Creek Road	Design and reconstruction of pavement	2010		ARRA	-	\$ 1,942,461		-	\$ 1,942,461	Transfer \$1,736,438.30 to Chandler/Dobson	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
CHN120-07C	Chandler	Chandler Blvd/Dobson Road Intersection, and Dobson Road from Chandler Blvd to Frye Road.	Intersection and Capacity Improvement	2010		ARRA		\$ 3,949,421		\$ 420,546	\$ 4,369,967	Transfer ARRA Price Road Savings to Chandler/Dobson Intersection	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
ELM09-801	El Mirage	Various Locations	Pre-Engineer/Design and mill & replace existing road	2010		ARRA		\$ 566,808			\$ 566,808	De-obligate ARRA funds due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
ELM08-801ABS	El Mirage	El Mirage Rd: Olive to Cactus	Micro-seal pavement surface	2010		ARRA		\$ 217,750			\$ 217,750	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
FTH07-301	Fountain Hills	Shea Blvd.: Palisades Blvd. to Fountain Hills Blvd.	Widen for third (westbound) climbing lane and bicycle lane.	2010	2	STP-MAG	\$ 131,000		\$ 2,164,000		\$ 2,295,000	De-obligate all ARRA funds (\$1,081,614) due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
FTH11-101ABS	Fountain Hills	Shea Blvd.: Saguario Blvd. to Fountain Hills Blvd.	Mill and overlay	2010	2	ARRA	\$ 65,379	\$ 1,081,614			\$ 1,146,993	Program ARRA savings to project.	The project is considered exempt under the category "Pavement resurfacing and/or rehabilitation." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length (miles)	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
GLB09-801	Gilbert	Various Locations - Functionally Classified Roadways	Pre-Engineer/Design and Construct Nova Chip Overlays - arterial roadways	2010	14.88	ARRA		\$ 4,366,610			\$ 4,366,610	Reduce ARRA/project costs by \$939,703 from \$5,306,313 to \$4,366,610	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLB06-201R	Gilbert	Eastern Canal	Santan Vista Trail										
GLB07-302 GLB05-107R				2010	3	ARRA CMAQ	\$ 200,000	\$ 939,703		\$ 1,139,703		\$ 1,685,769	Reprogramming of ARRA savings, \$939,703 for a current CMAQ project.
GLN08-801ABS	Glendale	Bell Rd. Pavement Overlay: 51st Ave. to 59th Ave.	Pavement Preservation, sidewalks, curb & gutter overlay, and re-striping	2010	1	ARRA		\$ 767,480			\$ 767,480	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN08-802ABS	Glendale	Various Locations Citywide	Upgrade traffic signal controllers	2010	n/a	ARRA		\$ 235,750			\$ 235,750	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN08-803ABS	Glendale	Bell Rd. Pavement Overlay: 59th Ave. to 70th Ave.	Pavement Preservation, sidewalks, curb & gutter overlay, and re-striping	2010	1	ARRA		\$ 1,114,548			\$ 1,114,548	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
LPK10-801ABS	Litchfield Park	Litchfield Rd: Wigwam Blvd to Camelback Rd	10ft Wide Concrete Pathway	2010	1	ARRA		\$ 128,146			\$ 128,146	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
LPK09-801	Litchfield Park	Various Locations	Pre-Engineer/Design and mill & replace existing road	2010		ARRA		\$ 455,905			\$ 455,905	De-obligate ARRA funds due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-801R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$ 267,227			\$ 267,227	De-obligate ARRA funds, \$604,433 due to bid savings	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-802R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and construct mill & replace pavement	2010		ARRA		\$ 1,281,173			\$ 1,281,173	Program additional ARRA funds \$350,759 to project	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-803R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$ 2,336,394			\$ 2,336,394	De-obligate ARRA funds, \$183,453 due to bid savings	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-804R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$ 1,975,734			\$ 1,975,734	De-obligate ARRA funds \$318,045 due to bid savings	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-805R	Mesa	Mesa - Various Locations Citywide - Functionally Classified Roadways	Pre-Eng/Design and pavement reconstruct & ADA upgrades	2010		ARRA		\$ 3,476,393			\$ 3,476,393	Program additional ARRA funds \$205,838 to project	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length (miles)	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
MES09-809	Mesa	Phase 4A	Fiber, cameras, detection, cabinets, controllers.	2010	4	CMAQ/ARRA		\$ 549,334	\$ 644,031		\$ 1,193,365	Lower CMAQ from \$651,254 to \$644,031, and add ARRA Savings \$549,334.	A minor project revision is needed to revise funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PEO09-801	Peoria	Various Locations	Pavement Preservation: Major arterial mill, overlay, and re-striping	2010	4.7	ARRA		\$ 2,042,695			\$ 2,042,695	De-obligate ARRA funds due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PEO10-805	Peoria	Lake Pleasant Parkway; Dynamite to CAP	Design New Four Lane Arterial Roadway	2010		ARRA / STP-AZ	\$ 465,719	\$ 351,706	\$ 1,907,325		\$ 2,724,750	Allocate ARRA savings as STP-AZ from Pavement Preservation to LPP design as part of Peoria's local match.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX07-315	Phoenix	7th Ave at the ACDC Canal	Multi-Use Underpass	2010		CMAQ/ARRA	\$ 258,300	\$ 900,000	\$ 1,750,000		\$ 2,908,300	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX07-306	Phoenix	7th St. at McDowell	Widen Intersection	2010		CMAQ/ARRA	\$ 1,244,000	\$ 100,000	\$ 1,256,000		\$ 2,600,000	Program ARRA savings to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
YTN09-801	Youngtown	Peoria Ave: 111th Ave west by 1950 ft/ to 115th Ave	Design & Complete Pavement mill & replace	2010		ARRA		\$ 324,920			\$ 324,920	De-obligate ARRA funds due to bid savings.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
YTN09-802	Youngtown	Peoria Ave & 115th Ave Re-alignment	Design	2010	0.08	STP-AZ	\$ 7,355	\$ -	\$ 137,145	\$ -	\$ 144,500	Program ARRA savings as STP-AZ to project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

Transit Projects

GDY08-801T	Goodyear	Citywide	Purchase bus <30 feet - 5 expand	2008		CMAQ	\$ 37,000		\$ 438,000	\$ -	\$ 475,000	Delete Project	The project is considered exempt under the category "Purchase of new buses and rail cars to replace existing vehicles or for minor expansions of the fleet." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX08-610T	Phoenix	Region wide	Purchase bus: < 30 foot - 5 expand (dial-a-ride)	2008		5307	\$ 79,000		\$ 316,000	\$ -	\$ 395,000	Delete Project	The project is considered exempt under the category "Purchase of new buses and rail cars to replace existing vehicles or for minor expansions of the fleet." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
New	Phoenix	Various locations	Preventive maintenance	2008		5307	\$ 1,696,808		\$ 6,787,232		\$ 8,484,040	Increase funds by \$316,000 from bus purchase program (PHX08-610T).	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length (miles)	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
PHX07-310T	Phoenix	Various locations	Upgrade LNG fuel station - North Division	2009		5307	\$ 300,000		\$ 1,200,000		\$ 1,500,000	Delete Project	The project is considered exempt under the category "Reconstruction or renovation of transit buildings and structures (e.g. rail or bus buildings, storage and maintenance facilities, stations, terminals, and ancillary structures." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
New	Phoenix	Various locations	Mid Life Bus Engine Rehabilitation	2009		5307	\$ 300,000		\$ 1,200,000		\$ 1,500,000	Add New project using funds from PHX07-310T.	The project, that includes rehabilitation with clean diesel engines, is considered exempt under the category "Rehabilitation of transit vehicles." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT09-650T	Valley Metro	Region wide	Reimbursement of bus: standard - 5 expand (Gilbert, Power) advance purchased in 2008	2009		5307	\$ -		\$ 13,630,000	\$ (13,630,000)	\$ -	Reduce from 8 to 5 buses due to reduction in service	The project is considered exempt under the category "Purchase of new buses and rail cars to replace existing vehicles or for minor expansions of the fleet." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT09-642T	Valley Metro	Region wide	Associated capital maintenance	2009		5307	\$ 2,088,000		\$ 8,352,000	\$ -	\$ 10,440,000	Increase PM funds due to reduction in bus purchases. Move match from regional to local.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMR09-821T	Valley Metro Rail	Tempe	Fixed guide way corridor - Tempe South Preliminary Engineering/FEIS	2009		CMAQ-Flex			\$ 2,726,000	\$ 174,000	\$ 2,900,000	Delete Project	A minor project revision is needed to delete project. Project is replaced with VMR10-704T. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMR10-703TR2	Valley Metro Rail	Main Street Corridor	Fixed guide way corridor - Central Mesa Preliminary Engineering/FEIS	2010		CMAQ-Flex			\$ 10,000,000	\$ 2,500,000	\$ 12,500,000	Admin Mod: Adjust federal funding amounts from \$5 million to \$10 million.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMR10-704T	Valley Metro Rail	Tempe	Fixed guide way corridor - Tempe South Preliminary Engineering/FEIS	2010		CMAQ-Flex			\$ 2,726,000	\$ 681,500	\$ 3,407,500	Admin Mod: Adjust federal funding amounts from \$5 million to \$2.726 million.	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMR09-825T	VM Rail	Central Phoenix / East Valley (CP/EV) 20-mile light rail transit starter line	Expand METRO light rail hours of service from 11:00pm on Friday and Saturday evenings to 2:00am on Saturday and Sunday mornings.	2009		CMAQ-Flex			\$ 300,000	\$ -	\$ 300,000	Delete Project	A minor project revision is needed to delete project. Project has been implemented with local funding sources. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMR09-826T	VM Rail	Central Phoenix / East Valley (CP/EV) 20-mile light rail transit starter line	Installation of additional METRO ticket vending machines and stand alone fare validation systems	2009		CMAQ-Flex			\$ 600,000	\$ -	\$ 600,000	Increase funds by \$300,000 to \$600,000 from deleted VMR09-825T project.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN10-608T	Glendale	Regionwide	Preventive Maintenance	2010		5307	\$ 48,873		\$ 197,144	\$ -	\$ 246,017	Increase 5307 costs from \$120,108 to \$197,144.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length (miles)	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
MAG10-901T	MAG	Regionwide	Preventive Maintenance	2010		5307	\$ 588,607		\$ 2,074,797	\$ -	\$ 2,663,404	Delete project and allocate funds to operators: Glendale, Peoria, Phoenix, Surprise, Tempe, and Valley Metro/RPTA based on current % split for preventative maintenance.	The project is considered exempt under the category "Operating assistance to transit agencies." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PEO10-603T	Peoria	Regionwide	Preventive Maintenance	2010		5307	\$ 16,491		\$ 66,522	\$ -	\$ 83,013	Increase 5307 costs from \$40,528 to \$66,522.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX10-615T	Phoenix	Regionwide	Preventive Maintenance	2010		5307	\$ 2,179,489		\$ 8,791,644	\$ -	\$ 10,971,133	Increase 5307 costs from \$5,356,220 to \$8,791,644.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SUR10-606T	Surprise	Regionwide	Preventive Maintenance	2010		5307	\$ 2,184		\$ 8,810	\$ -	\$ 10,994	Increase 5307 costs from \$5,368 to \$8,810.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
TMP10-604T	Tempe	Regionwide	Preventive Maintenance	2010		5307	\$ 73,044		\$ 294,645	\$ -	\$ 367,689	Increase 5307 costs from \$179,510 to \$294,645.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT10-901TB	Valley Metro	Regionwide	Purchase bus: standard 40 foot - 19 replace (Tempe)	2010		5307	\$ -		\$ 7,649,825	\$ 1,799,055	\$ 9,448,881	Split into 2 projects funded with 5307 and STP-Flex	The project is considered exempt under the category "Purchase of new buses and rail cars to replace existing vehicles or for minor expansions of the fleet." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT10-655T	Valley Metro	Regionwide	Preventive Maintenance	2010		5307	\$ 291,665		\$ 1,176,519	\$ -	\$ 1,468,184	Increase 5307 costs from \$716,782 to \$1,176,519.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN11-809TA	Glendale	Bell/L101	Acquire right of way regional park-and-ride (Bell/L101)	2010		5309-FGM			\$ 2,287,742	\$ 571,935	\$ 2,859,677	Decrease project costs and fund remaining costs with STP-Flex	A minor project revision is needed to reduce project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN11-809TC	Glendale	Bell/L101	Acquire right of way regional park-and-ride (Bell/L101)	2010		STP-Flex	\$ -		\$ 448,323	\$ 2,328,341	\$ 2,776,664	New project funded with STP-flex	A minor project revision is needed to add new funding source to project. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT10-665T	Valley Metro	Regionwide	Purchase vanpools: 45 replace	2010		STP-Flex	\$ -		\$ 1,482,750	\$ -	\$ 1,482,750	Adjust costs	A minor project revision is needed to adjust project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
VMT10-901TC	Valley Metro	Regionwide	Purchase bus: standard 40 foot - 3 replace (Tempe)	2010		5307	\$ -		\$ 1,402,460	\$ 55,028	\$ 1,457,487	New project funded with STP-flex	The project is considered exempt under the category "Purchase of new buses and rail cars to replace existing vehicles or for minor expansions of the fleet." The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
BKY10-801	Buckeye	Miller Road: Hazen Rd to I-10 and Monroe (MC85): Miller Rd to Apache Rd	Interconnect Traffic Signals	2013	6	CMAQ	\$ 90,000		\$ 210,000		\$ 300,000	Defer Project from 2010 to 2013	A minor project revision is needed to defer project to FY 2013. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
BKY11-801	Buckeye	North Watson Road and MC85 Phase I and Phase II	Pave Unpaved Road	2013	0.22	CMAQ	\$ 3,896		\$ 64,456		\$ 68,352	Defer Project from 2010 to 2013	A minor project revision is needed to defer project to FY 2013. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
CHN07-601	Chandler	Commonwealth Ave: Hamilton St to Ithaca	Pave dirt road	2011	0.2	CMAQ	\$ 1,075,000		\$ 325,000	\$ -	\$ 1,400,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
CHN13-901	Chandler	Various Locations in the City of Chandler	Paving dirt alleys	2011	10	CMAQ	\$ 589,000		\$ 350,000	\$ -	\$ 939,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
ELM13-903	El Mirage	Dysart Ranchettes area: Varney Rd, Peoria Ave, Dysart Rd, El Mirage	Paving dirt roads	2011	3.4	CMAQ	\$ 1,750,000		\$ 1,250,000		\$ 3,000,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
ELM09-802	El Mirage	125th Ave and 127th Ave: Varney Rd to Peoria Ave	Pave unpaved roads	2011	1	CMAQ	\$ 1,102,252		\$ 381,031	\$ -	\$ 1,483,283	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
FTM09-903C	Fort McDowell Yavapai Nation	Various Locations on Fort McDowell Yavapai Nation	Construct Pave dirt road	2011	2.5	CMAQ	\$ 24,000		\$ 375,000	\$ -	\$ 399,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
FTM13-901	Fort McDowell Yavapai Nation	Various Locations on Fort McDowell Yavapai Nation	Paving dirt roads	2011	4.7	CMAQ	\$ 1,650,000		\$ 700,000	\$ -	\$ 2,350,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN07-779	Glendale	Various Locations: Camelback Rd, Litchfield Rd, Olive Ave, Greenway Rd, 83rd Ave, 75th Ave	Pave dirt shoulders	2011	5.17	CMAQ	\$ 133,035		\$ 133,035		\$ 266,070	Defer Project from 2008 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN08-605	Glendale	Glendale Ave: Loop 101 to Luke AFB	Pave access points	0	5	CMAQ	\$ 27,000		\$ 63,000		\$ 90,000	Delete Project from the TIP	The deletion of the project would not change the assumptions used in the regional emissions analysis. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
GLN09-610	Glendale	Downtown alley north of Glendale Ave between 57th Ave and 57th Dr	Transform existing service alleyway into a safe environment for pedestrian circulation and limited vehicular traffic	2012	0.04	CMAQ	\$ 103,166		\$ 240,721	\$ -	\$ 343,887	Defer Project from 2010 to 2012	A minor project revision is needed to defer project to FY 2012. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN07-311	Glendale	Alley 250 ft north of Glendale Ave: 58th Ave to 57th Dr	Design and construct alley improvements and pedestrian walkway	2012	0.05	CMAQ	\$ 75,000		\$ 75,000	\$ -	\$ 150,000	Defer Project from 2010 to 2012	A minor project revision is needed to defer project to FY 2012. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES07-315	Mesa	Southern Ave at Country Club Dr	Add 1 right turn lane and three bus pullouts.	2013	0.45	CMAQ	\$ 3,437,000		\$ 910,000	\$ -	\$ 4,347,000	Defer Project from 2010 to 2013	A minor project revision is needed to defer project to FY 2013. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES08-603	Mesa	Longmore: Broadway Rd to Main St (EVIT)	Design and construct bicycle path to connect Broadway Rd with Main St and the Light Rail Station	2011	0.5	CMAQ	\$ 388,961		\$ 1,082,739	\$ -	\$ 1,471,700	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES06-203C	Mesa	Pepper Pl: Lewis St to Robson St	Construct multi-use path	0	0.5	CMAQ	\$ 93,039		\$ 305,961	\$ -	\$ 399,000	Delete Project from the TIP	The project is considered exempt under the category "Bicycle and pedestrian facilities". The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES07-314	Mesa	South Canal: Val Vista Dr to Greenfield Rd	Construct multi-use path. Development of multi-use path system (MUP)	0	1.25	CMAQ	\$ 232,200		\$ 541,800	\$ -	\$ 774,000	Delete Project from the TIP	The project is considered exempt under the category "Bicycle and pedestrian facilities". The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES09-605	Mesa	Grand St: Broadway Rd to 6th Ave (Nuestro neighborhood phase 1)	Improve pedestrian facilities	0	0.25	CMAQ	\$ 189,018		\$ 441,041	\$ -	\$ 630,059	Delete Project from the TIP	The project is considered exempt under the category "Bicycle and pedestrian facilities". The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES10-608	Mesa	South Canal: McDowell Rd to Val Vista Dr	Construct new multi-use path on the north bank	0	1.5	CMAQ	\$ 568,337		\$ 852,505	\$ -	\$ 1,420,842	Delete Project from the TIP	The project is considered exempt under the category "Bicycle and pedestrian facilities". The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX10-845	Phoenix	Salt River: 24th Street to I-10/Tempe Drain	Construct Multi-use path	2011		CMAQ	\$ 480,100		\$ 801,606		\$ 1,281,706	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX09-619	Phoenix	19th Ave at Greenway Rd	Construct multi-use path and bridge (phase 2)	2011	0.04	CMAQ	\$ 2,174,100		\$ 1,010,000	\$ -	\$ 3,184,100	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
PHX09-624	Phoenix	Various locations	Construct regional ITS fiber optic backbone, phase B-1	2011	30	CMAQ	\$ -		\$ 665,000	\$ -	\$ 665,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX09-871	Phoenix	Various locations	Pave unpaved alleys	2011	18	CMAQ	\$ 200,000		\$ 466,667	\$ -	\$ 666,667	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX09-872	Phoenix	Various locations	Pave unpaved roads	2011	3	CMAQ	\$ 450,000		\$ 1,050,000	\$ -	\$ 1,500,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX10-633	Phoenix	Various locations	Construct regional ITS fiber optic backbone, phase B-2	2011	30	CMAQ	\$ -		\$ 665,000	\$ -	\$ 665,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
PHX13-904	Phoenix	Various Locations in the City of Phoenix: 44 miles of dirt alleys	Paving dirt alleys	2011	44	CMAQ	\$ 920,000		\$ 1,200,000	\$ -	\$ 2,120,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SCT07-606	Scottsdale	Dynamite Blvd: Pima Road to Alma School Road	Install Vertical Curb and Gutter	2011	3	CMAQ	\$ 500,000		\$ 500,000	\$ -	\$ 1,000,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SCT10-617R	Scottsdale	Scottsdale Rd: Earl Dr to Chaparral Rd	Upgrade sidewalks and add bicycle lanes	2011	3	CMAQ	\$ 2,540,741		\$ 510,696	\$ -	\$ 3,051,437	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SUR09-820	Surprise	Rural Area West of 219th Ave between Pinnacle Peak & Deer Valley	Pave unpaved roads	2011	3.27	CMAQ	\$ 686,700		\$ 1,602,302	\$ -	\$ 2,289,002	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
TMP10-620	Tempe	Broadway Rd: Rural Rd to Mill Ave	Construct pedestrian and bicycle facilities improvements	2011	1	CMAQ	\$ 2,571,780		\$ 2,571,780	\$ -	\$ 5,143,560	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
TMP10-629	Tempe	Salt River: I-10 /Tempe Drain to Priest	Construct Multi-use path	2011	1.1	CMAQ	\$ 400,000		\$ 120,000	\$ -	\$ 520,000	Defer Project from 2010 to 2011	A minor project revision is needed to defer project to FY 2011. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
MES10-810	Mesa	Phase 4B	Fiber, cameras, detection, cabinets, controllers.	2010	12.5	CMAQ			\$ 2,758,363		\$ 2,758,363	Increase federal CMAQ funds by \$2,048,390 (Closeout) from \$709,973 to \$2,758,363. Project is 100% funded with CMAQ	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

TIP #	Agency	Project Location	Project Description	FY	Length	Fund Type	Local Cost	ARRA Cost	Federal Cost	Regional Cost	Total Cost	Requested Change	Conformity Assessment
MES08-604	Mesa	East Loop ITS Device Deployment	New CCTV, Video Detection Cameras	2010	15	CMAQ	\$ 61,160		\$ 938,840		\$ 1,000,000	Increase CMAQ funds from Closeout, \$92,917, and the original CMAQ from Project MES09-809, \$7,223	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
GLN09-609	Glendale	Skunk Creek/Union Hills Dr.	Design and construct multi-use path under Union Hills Dr.	2010		CMAQ	\$ 398,772		\$ 210,228		\$ 609,000	Increase federal CMAQ funds by \$63,000 (Closeout) from \$147,228 to \$210,228.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SUR11-715	Surprise	Peoria Ave: Litchfield Rd to Jackrabbit Rd	Design and construct fiber optic cable interconnection of existing and future ITS facilities	2010			\$ 100,000		\$ 700,000		\$ 800,000	Advance project from 2011 to 2010	A minor project revision is needed to advance project to FY 2010. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
SUR11-714D	Surprise	Bell Road US60 (Grand Avenue) to 114th Avenue	Design of Multi-use Path	2010					\$ 175,000		\$ 175,000	Advance design portion of project from 2012 to 2010	A minor project revision is needed to advance project to FY 2010. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.
LPK05-101C	Litchfield Park	Litchfield Rd Bypass at Wigwam Boulevard	Construct bicycle underpass	2010		CMAQ	\$ 53,850	\$ -	\$ 1,686,420		\$ 1,740,270	Add \$800,000 of CMAQ funds from Closeout recommendation.	A minor project revision is needed to increase project funding. The conformity status of the TIP and Regional Transportation Plan 2007 Update would remain unchanged.

MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

Approval of Electric Vehicle Charging Infrastructure Deployment Guidelines

SUMMARY:

On April 16, 2009, MAG, ECotality and Nissan North America announced a zero emissions partnership that will help facilitate the introduction of electric vehicles (EVs) in the MAG region. The proposed infrastructure will utilize 220-volt charging stations at residential and commercial locations, as well as 440-volt fast-charge stations that could be strategically located to allow vehicles to fully charge in less than 26 minutes. The public and commercial charging systems will utilize the standardized plugs and connectors that have been adopted by major auto manufacturers and will be compatible with all plug-in vehicles.

On May 20, 2009, MAG staff provided an overview on the zero emission partnership initiative to the MAG Building Codes Committee. On August 5, 2009, ECotality North America, formerly known as Electric Transportation Engineering Corporation (eTec), a subsidiary of ECotality, Inc. and a leader in clean electric transportation and storage technologies, was selected by the U.S. Department of Energy for a grant of approximately \$99.8 million to undertake the largest deployment of EVs and charging infrastructure to date. The grant will be matched by the application's project participants to provide a total of approximately \$200 million to fund the initiative. ECotality, as the lead applicant for the proposal, partnered with Nissan North America to deploy EVs and the charging infrastructure that will support them along with all electric and plug-in hybrid electric vehicles. The project proposes to deploy charging infrastructure in major population areas that include Phoenix (AZ), Tucson (AZ), San Diego (CA), Portland (OR), Eugene (OR), Salem (OR), Corvallis (OR), Seattle (WA), Nashville (TN), Knoxville (TN), and Chattanooga (TN). In addition to implementation of charging infrastructure in the Phoenix and Tucson regions, ECotality plans to link the two metropolitan areas by implementing strategic fast-charge stations along Interstate-10 to create the first true implementation of an EV Corridor in North America.

On March 17, 2010, ECotality presented version 2.0 of the Electric Vehicle Charging Infrastructure Deployment Guidelines to the MAG Building Codes Committee. The Deployment Guidelines document is intended to create a common knowledge base of EV requirements for stakeholders involved in the implementation of EV charging infrastructure. ECotality's Deployment Guidelines provide the necessary background information for understanding EV requirements and the related codes, laws and standards for this effort. The document is the foundation for implementation of the EV Micro-Climate© program, providing the optimum infrastructure to support and encourage the adoption of electric vehicles in the MAG region. The MAG Building Codes Committee provided feedback and requested that ECotality update the document based on the feedback received.

At the May 19, 2010 MAG Building Codes Committee meeting, ECotality presented version 3.0 of the Electric Vehicle Infrastructure Deployment Guidelines, which incorporated the feedback received from members on version 2.0. The MAG Building Codes Committee further reviewed the document and voted to recommend approval of the document.

PUBLIC INPUT:

No public input has been received.

PROS & CONS:

PROS: ECOTality’s Electric Vehicle (EV) Charging Infrastructure Deployment Guidelines will create a common knowledge base of EV requirements for stakeholders involved in the implementation of EV charging infrastructure.

CONS: None.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: ECOTality’s Electric Vehicle (EV) Charging Infrastructure Deployment Guidelines provide the necessary background information for understanding EV requirements and the related codes, laws and standards to support and encourage the adoption of electric vehicles in the MAG region.

POLICY: ECOTality plans to link the two metropolitan areas by implementing strategic fast-charge stations along Interstate-10 that will allow for EV travel between Phoenix and Tucson and create the first true implementation of an EV Corridor in North America.

ACTION NEEDED:

Approval of the Electric Vehicle Charging Infrastructure Deployment Guidelines document version 3.0 as guidelines to the implementation of infrastructure that will support and encourage the adoption of electric vehicles in the MAG region.

PRIOR COMMITTEE ACTIONS:

The MAG Management Committee reviewed the Electric Vehicle Charging Infrastructure Deployment Guidelines Version 3.0 and recommended approval of the document at the June 9, 2010 meeting.

MEMBERS ATTENDING

- | | |
|--|--|
| Mark Pentz, Chandler, Chair | Darryl Crossman, Litchfield Park |
| Carl Swenson, Peoria, Vice Chair | Scott Butler for Christopher Brady, Mesa |
| # George Hoffman, Apache Junction | Jim Bacon, Paradise Valley |
| Charlie McClendon, Avondale | Karen Peters for David Cavazos, Phoenix |
| David Johnson for Stephen Cleveland,
Buckeye | John Kross, Queen Creek |
| Gary Neiss, Carefree | * Bryan Meyers, Salt River Pima-Maricopa
Indian Community |
| Wayne Anderson for Usama Abujbarah,
Cave Creek | Dave Richert, Scottsdale |
| Pat Dennis for B.J. Cornwall, El Mirage | Michael Celaya for Mark Coronado, Surprise |
| Alfonso Rodriguez for Phil Dorchester,
Fort McDowell Yavapai Nation | Jeff Kulaga for Charlie Meyer, Tempe |
| # Julie Ghatti for Rick Davis, Fountain Hills | * Reyes Medrano, Tolleson |
| Rick Buss, Gila Bend | # Gary Edwards, Wickenburg |
| * David White, Gila River Indian Community | # Mark Hannah for Lloyce Robinson,
Youngtown |
| Tami Ryall for Collin DeWitt, Gilbert | Steve Hull for John Halikowski, ADOT |
| Brent Stoddard for Ed Beasley, Glendale | Kenny Harris for David Smith,
Maricopa County |
| Mark Gaillard for John Fischbach, Goodyear | David Boggs, Valley Metro/RPTA |
| Bill Hernandez, Guadalupe | |

* Those members neither present nor represented by proxy.
Participated by telephone conference call. + Participated by videoconference call.

The MAG Building Codes Committee reviewed the Electric Vehicle Charging Infrastructure Deployment Guidelines Version 3.0 and recommended approval of the document at the May 19, 2010 meeting.

COMMITTEE MEMBERS ATTENDING

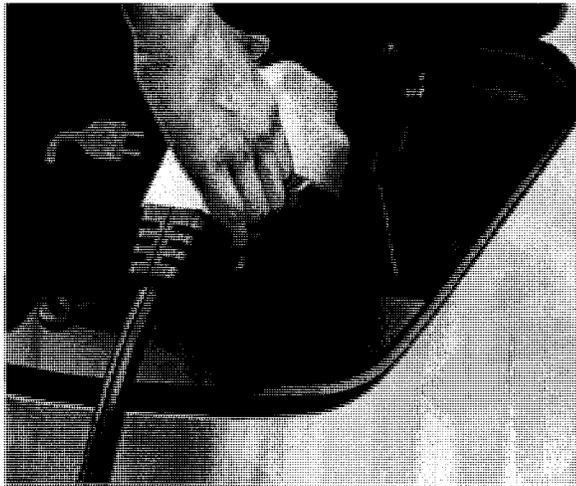
- Steven Hether, Mesa, Chair
- Ken Sowers, Avondale, Acting Chair
- Phil Marcotte, Buckeye
- * Mike Tibbett, Carefree
- Mike Baxley, Cave Creek
- # Alex Banachowski, Chandler
- * Mary Dickson, El Mirage
- * Peter Johnson, Fountain Hills
- * John Smith, Gila Bend
- * Jo Rene DeVeau, Gila River Indian Community
- # Ben Cox for Ray Patten, Gilbert
- Stephen Dudley for Bryan Woodcox, Glendale
- Bill King for Ed Kulik, Goodyear
- * Chuck Ransom, Litchfield Park
- Tom Ewers, Maricopa County
- Bob Lee, Paradise Valley
- # Dennis Chase for Dennis Marks, Peoria
- Tom Wandrie, Phoenix
- # Dean Wise, Queen Creek
- Michael Clack, Scottsdale
- # Michael Williams, Tempe
- # Mario Rochin, Tolleson
- John Stigsell, Youngtown
- Rick DeStefano, Wickenburg
- Bridget Jones for Rus Brock, Home Builders Association

- * Member neither present nor represented by proxy
- # Committee members participating via audioconference

CONTACT PERSON:

Heidi Bickart, MAG, (602) 254-6300

Electric Vehicle Charging Infrastructure Deployment Guidelines for the Greater Phoenix Area



May, 2010
Version 3.0



ELECTRIC TRANSPORTATION ENGINEERING CORPORATION

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Acronyms

BEV	Battery Electric Vehicle—Vehicle powered 100% by the battery energy storage system available on-board the vehicle.
CCID	Charge Current Interrupting Device—A device within EVSE to shut off the electricity supply if it senses a potential problem that could result in electrical shock to the user.
EV	Electric Vehicle
EREV	Extended Range Electric Vehicle—see <i>PHEV</i>
EVSE	Electric Vehicle Supply Equipment—Equipment that provides for the transfer of energy between electric utility power and the electric vehicle.
ICE	Internal Combustion Engine
kW	Kilowatts—A measurement of electric power. Used to denote the power an electrical circuit can deliver to a battery.
kWh	Kilowatt Hours—A measurement of total electrical energy used over time. Used to denote the capacity of an EV battery.
NEC	National Electric Code—Part of the National Fire Code series established by the National Fire Protection Association (NFPA) as NFPA 70. The NEC codifies the requirements for safe electrical installations into a single, standardized source.
NEMA	National Electrical Manufacturers Association—Group that develops standards for electrical products.
PHEV	Plug-in Hybrid Electric Vehicle—Vehicles utilizing a battery and an internal combustion engine (ICE) powered by either gasoline or diesel.
REEV	Range Extended Electric Vehicle—see <i>PHEV</i>
RTP	Real Time Pricing—a concept for future use whereby utility pricing is provided to assist a customer in selecting the lowest cost charge.
SAE	Society of Automotive Engineers—standards development organization for the engineering of powered vehicles.
TOU	Time of Use—an incentive-based electrical rate established by an electric utility.
V2G	Vehicle to Grid—a concept that allows the energy storage in electric vehicles to be used to support the electrical grid during peak electrical loads.
VAC	Voltage Alternating Current

Electric Vehicle Charging Infrastructure Deployment Guidelines

1. Introduction

Concerns about global warming, oil shortages, and increasing gas prices, along with the rapid rise of more fuel-efficient vehicles, are clear indicators of changing consumer preferences and automotive industry direction. As major automotive manufacturers plan to launch plug-in electric vehicles (EV) in 2010, the future of transportation is being propelled by a fundamental shift to cleaner and more efficient electric drive systems.

Electric Transportation Engineering Corporation (eTec), a subsidiary of ECOTality, has been involved in every North American EV initiative since 1989. With over two decades of experience in electric transportation, eTec is the most experienced and qualified solution provider for EVs and their supporting infrastructure. eTec's unparalleled EV infrastructure experience, combined with its expertise in batteries, battery charging, utility activities, and electric drive systems makes eTec a leader in electric transportation.

ECOTality and eTec developed the EV Micro-Climate® program as an integrated turnkey program to ensure an area is well equipped with the necessary infrastructure to support the consumer adoption of electric transportation. Beginning with extensive feasibility and infrastructure planning studies, the program provides a blueprint to create a rich EV infrastructure. The program is developed with all relevant stakeholders, including governmental organizations, utilities, private-sector businesses, and automotive manufacturers.

These Deployment Guidelines are not intended to be used as an installation manual or a replacement for approved codes and standards, but rather are intended to create a common knowledge base of EV requirements for stakeholders involved in the development and approval of EV charging infrastructure.

Electric vehicles have unique requirements that differ from internal combustion engine vehicles, and many stakeholders currently are not familiar with these requirements. eTec's *Electric Vehicle Charging Infrastructure Deployment Guidelines* provide the necessary background information for understanding EV requirements, and are the foundation upon which the EV Micro-Climate program builds in order to provide the optimum infrastructure to support and encourage the adoption of electric vehicles.

2. Electric Vehicle Technology

This section describes the basic electric vehicle technologies that are either available in the marketplace or coming to market in the near future. The focus of this section is on street-legal vehicles that incorporate a battery energy storage device with the ability to connect to the electrical grid for the supply of some or all of its fuel energy requirements. Two main vehicle configurations are described, along with the four main categories of vehicle applications. Vehicle categories and the relative size of their battery packs are discussed in relationship to recommended charging infrastructure.

A. Electric Vehicle Configurations

Battery Electric Vehicle (BEV)

Battery Electric Vehicles (BEVs) are powered 100% by the battery energy storage system available on-board the vehicle. The Nissan LEAF is an example of a BEV. Refueling the BEV is accomplished by connection to the electrical grid through a connector system that is designed specifically for this purpose. Most advanced BEVs have the ability to recapture some of the energy storage utilized through regenerative braking (Put simply, converting the propulsion motor into a generator when braking). When regenerative braking is applied, BEVs can typically recover 5 to 15 percent of the energy used to propel the vehicle to the vehicle speed prior to braking. Sometimes manufacturers also install solar photovoltaic (PV) panels on vehicle roofs. This provides a very small amount of energy relative to the requirements of propelling the vehicle, but integrating PV in the roof typically can provide enough power to operate some small accessory loads.

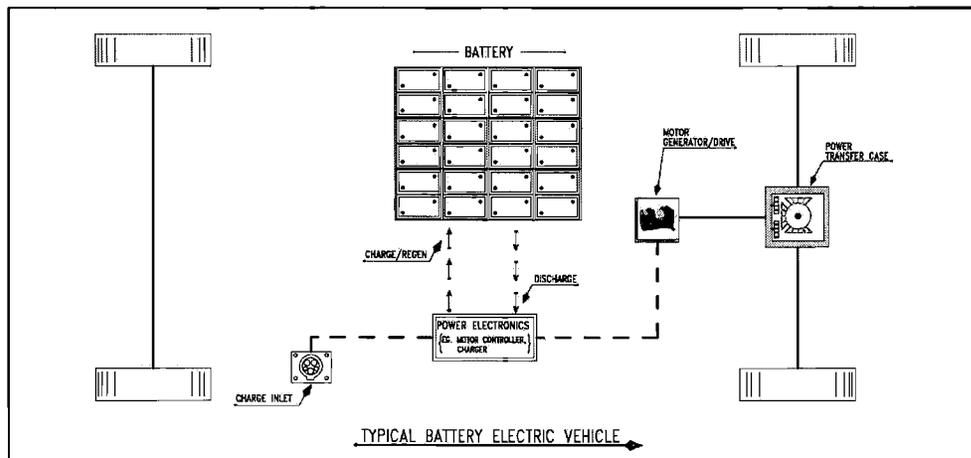


Figure 2-1 Battery Electric Vehicle

A typical BEV is shown in the block diagram in Figure 2-1. Since the BEV has no other significant energy source, the battery must be selected to meet the BEV range and power requirements. BEV batteries are typically an order of magnitude larger than the batteries in hybrid electric vehicles.

Plug-in Hybrid Electric Vehicle (PHEV)

PHEVs are powered by two energy sources. The typical PHEV configuration utilizes a battery and an internal combustion engine (ICE) powered by either gasoline or diesel. Within the PHEV family, there are two main design configurations, a *Series Hybrid* as depicted in Figure 2-2, and a *Parallel Hybrid* as depicted in Figure 2-3. The Series Hybrid vehicle is propelled solely by the electric drive system, whereas the Parallel Hybrid vehicle is propelled by both the ICE and the electric drive system. As with a BEV, a Series Hybrid will typically require a larger and more powerful battery than a Parallel Hybrid vehicle in order to meet the performance requirements of the vehicle solely based on battery power.

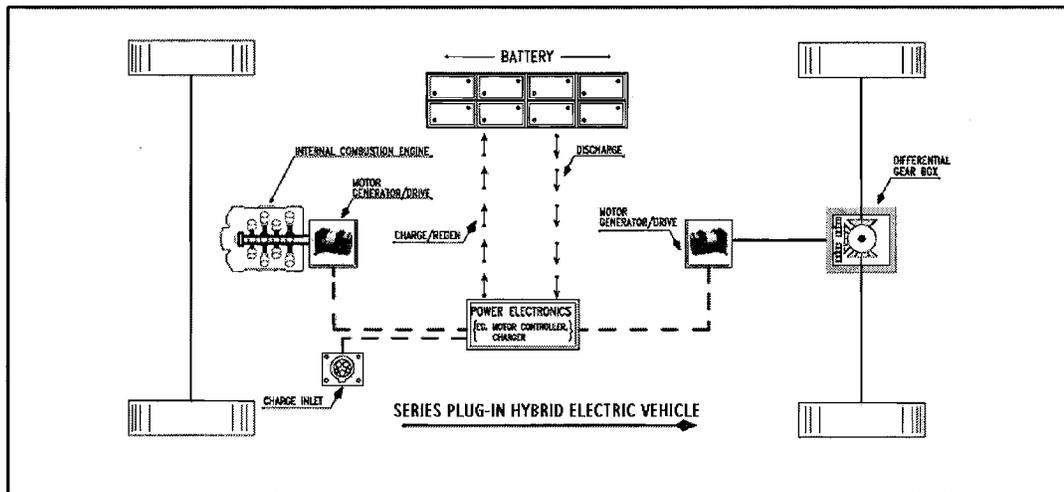


Figure 2-2 Series Plug-In Hybrid Vehicle Block Diagram

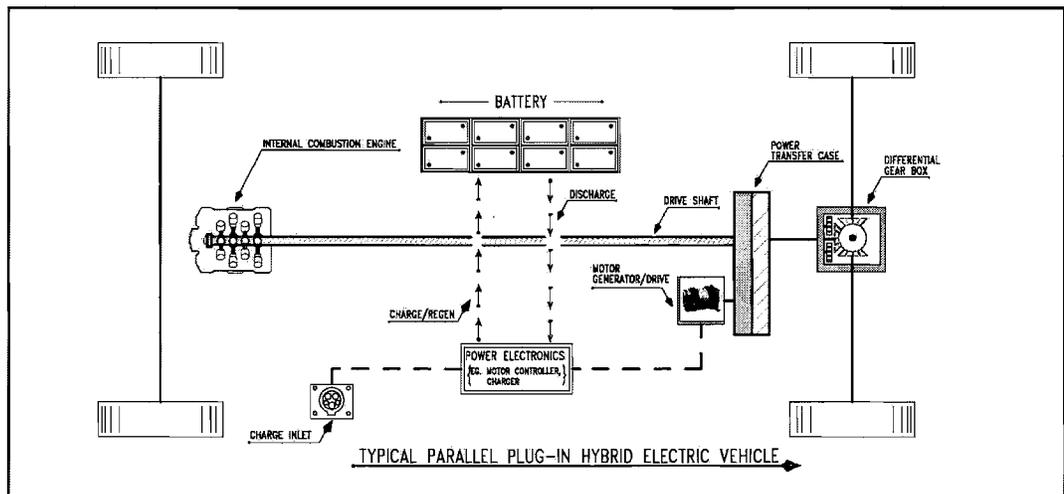


Figure 2-3 Parallel Plug-In Hybrid Vehicle Block Diagram

Manufacturers of PHEVs use different strategies in combining the battery and ICE and may utilize the battery only for the first several miles; an example of this strategy is the Chevy Volt, which has an ICE providing generating power for the duration of the vehicle range. Others may use the battery power for sustaining motion and the ICE for acceleration or higher-energy demands at highway speeds. Frequently, the vehicles employing the former strategy gain a designation such as *PHEV-20* to indicate that the first 20 miles are battery only. Other terms related to PHEVs may include *Range Extended Electric Vehicle* (REEV) or *Extended Range Electric Vehicle* (EREV).

B. Electric Vehicle Categories

EVs can be broken down into the following categories.

On-Road Highway Speed Vehicles

An *On-Road Highway Speed Vehicle* is an EV capable of driving on all public roads and highways. Performance of these On-Road vehicles is similar to ICE vehicles.

City Electric Vehicles

Traditionally, *City Electric Vehicles* have been BEVs that are capable of driving on most public roads, but generally are not driven on highways. Top speed is typically limited to 55 mph.

Neighborhood Electric Vehicles (NEVs)

Neighborhood Electric Vehicles (NEVs), also known as *Low Speed Vehicles* (LSVs), are BEVs that are limited to 25 mph and are allowed in certain jurisdictions to operate on public streets posted at 35 mph or less.

Commercial On-Road Highway Speed Vehicles

There are a number of commercial electric vehicles, including commercial trucks and buses. These vehicles are found as both BEVs and PHEVs. Performance and capabilities of these vehicles are specific to their applications.

The focus of the EV Micro-Climate program is on the first and last categories described above, the On-Road Highway Speed and Commercial On-Road Highway Speed Vehicles. Specialty vehicles such as electric motorcycles and bicycles require a different planning process.

C. Batteries

Battery Technology

Recent advancements in battery technologies will allow EVs to compete with ICE vehicles in performance, convenience, and cost. Although lead-acid technology serves many EV applications such as forklifts and airport ground support equipment very cost-effectively, the limitations on energy density and repeated cycles of charging and discharging make its application to on-road highway speed EVs less practical.

Today, most major car companies utilize nickel-metal-hydrate or various lithium-based technologies for their EVs. Lithium provides four times the energy of lead-acid and two times that of nickel-metal-hydrate. The materials for lithium-based batteries are generally considered abundant, non-

hazardous, and lower cost than nickel-based technologies. The current challenge with lithium-based technologies is increasing battery capacity while maintaining quality and cycle life and lowering production costs.

From an infrastructure standpoint, it is important to consider that, as battery costs are driven down over time, the auto companies will increase the size of the lithium-based battery packs and thus increase the range of electric vehicles.

Relative Battery Capacity

Battery size or capacity is measured in kilowatt hours (kWh). Battery capacity for electric vehicles will range from as little as 3 kWh to as large as 40 kWh or more. Typically, PHEVs will have smaller battery packs because they have more than one fuel source. BEVs rely completely on the storage from their battery pack for both range and acceleration, and therefore require a much larger battery pack than a PHEV for the same size vehicle.

Battery Charging Time

The amount of time to fully charge an EV battery is a function of the battery size and the amount of electric power or kilowatts (kW) that an electrical circuit can deliver to the battery. Larger circuits, as measured by voltage and amperage, will deliver larger amounts of kW. The common 110-120 volts AC (VAC), 15 amp circuit will deliver at minimum 1.1 kW to a battery. A 220-240 VAC, 40 amp circuit (similar to the circuit used for household appliances like dryers and ovens) will deliver at minimum 6 kW to a battery. Table 2-1 provides information on several different on-road highway speed electric vehicles, their battery pack size, and charge times at different power levels to replenish a depleted battery.

Table 2-1 EV Charge Times

EV Configuration	Battery Size (kWh)	Circuit Size and Power in kW Delivered to Battery			
		120 VAC, 15 amp 1.2 kW	120 VAC, 20 amp 1.6 kW	240 VAC, 40 amp 6.5 kW	480 VAC, 85 amp 60 kW
PHEV-10	4	3 h 20 m	2 h 30 m	35 m	n/a
PHEV-20	8	6 h 40 m	5 h	1 h 15m	n/a
PHEV-40	16	13 h 20 m	10 h	2 h 30 m	16 m
BEV	24	20 h	15 h	3 h 40 m	24 m
BEV	35	29 h 10 m	21 h 50 m	5 h 20 m	35 m
PHEV Bus	50	n/a	n/a	7 h 40 m	50 m

Note: Power delivered to battery calculated as follows: 120VAC x 12 amps x .85 eff.; 120VAC x 16 amps x .85 eff.; 240VAC x 32 amps x .85 eff.; 480VAC x $\sqrt{3}$ x 85 amps x .85 eff.

D. Automaker Plans

Many automakers have announced plans for the introduction of on-road highway speed EVs in the near future. A summary table of such plans is shown in Figure 2-4 below.

Plug-In Hybrid Electric Vehicles

Company	Model	Price	Battery Type	Battery Size	EV Range (miles)	PHEV Type	Market launch	Production Capacity
BYD	F3DM	\$21,915	Lithium-ion	-	62	-	2008	
BYD	F6DM	~\$22,000	Lithium-ion	-	62	-	2008	
Fisker	Karma	\$87,900	Lithium-ion	22 KWh	50	Series	2010	15k
Ford	Escape PHEV	-	Lithium-ion	10 KWh	30-40	-	2012	
GM	Chevrolet Volt	~\$40,000+	Lithium-ion	16 KWh	40	Series	2010	60k by 2012
Opel	Ampera	-	Lithium-ion	16 KWh	40	Series	2012	
Toyota	Prius	~\$48,000	Lithium-ion	-	12-18	Parallel	2010	20k-30k
Volkswagen	Golf Twin Drive	-	Lithium-ion	12 KWh	30	-	2010	20 car pilot

Electric Vehicles

Company	Model	Price	Battery Type	Battery Size	EV Range (miles)	Latest Model	Market launch	Production Capacity
BMW	Mini E	-	Lithium-ion	35 KWh	~100+	2009	n.d.	500 pilot
BYD	E6 EV	-	Lithium-ion	18 KWh	249	2009	2009	
Chery Auto	S18 EV	~\$15,000	Lithium-ion	12 KWh	90	2009	2009	
Chrysler	Dodge circuit	-	Lithium-ion	26 KWh	150-200	2010	2010	
Coda	EV Sedan	\$45,000	Lithium-ion	24 KWh	50-120	2010	2010	
Ford	Focus EV	-	Lithium-ion	-	100	2011	2011	
Mitsubishi	iMIEV	~\$46,000	Lithium-ion	16 KWh	100	2009	2009	20,000
Nissan	EV LEAF	~\$24k to ~\$34k*	Lithium-ion	24 KWh	100	2010	2010	150,000+
Renault	Fluence ZE (Better Place)	-	Lithium-ion	-	100	2011	2011	100,000
Smart	EV	-	Lithium-ion	-	70	2010	2010	
Subaru	Stella	\$47,900	Lithium-ion	9 KWh	55	2009	2009	~170 in 2009
Tesla	Model S	\$57,400	Lithium-ion	-	160-300	2011	2011	
Tesla	Roadster EV	\$109,000	Lithium-ion	53 KWh	244	2009	2009	
Think	City	\$28,000	Sodium or Li	-	110	2010	2010	2,500 (US)

Source: Company data, Credit Suisse estimates

Figure 2-4 Automaker PHEV and BEV Plans¹

¹ Credit Suisse "Electric Vehicles," Equity Research, *Energy Technology/Auto Parts & Equipment*, October 1, 2009.

3. Charging Requirements

This section covers the terminology and general requirements of Electric Vehicle Supply Equipment (EVSE). EVSE provides for the safe transfer of energy between the electric utility power and the electric vehicle.

A. Charging Components

The terms used to identify the components in the delivery of power to the vehicle are defined first.

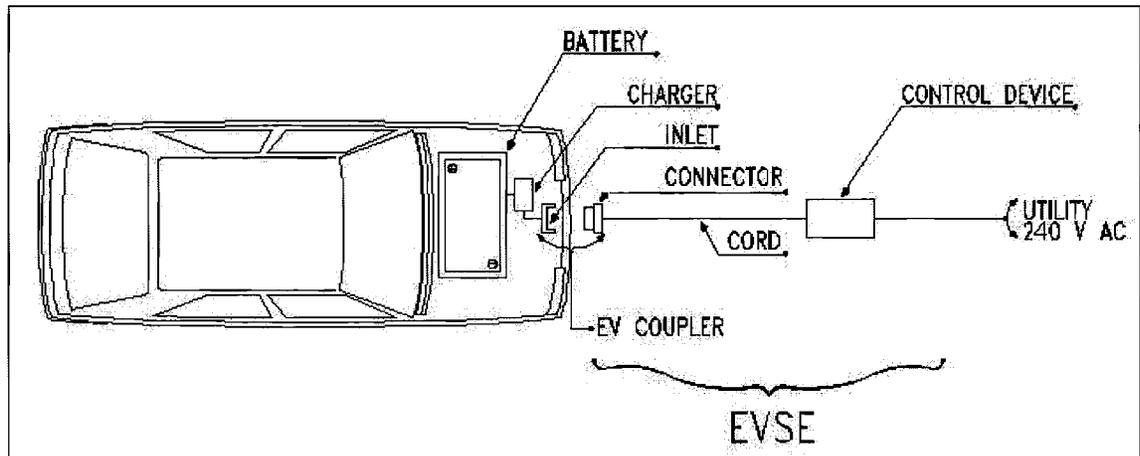
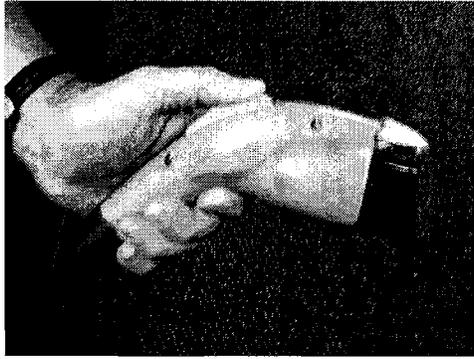


Figure 3-1 Level 2 Charging Diagram

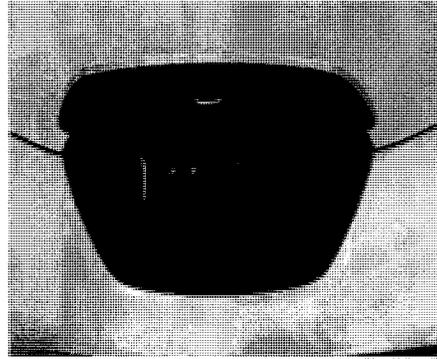
Power is delivered to the EV's onboard battery through the *EV inlet* to the *charger*. The charger converts Alternating Current (AC) to the Direct Current (DC) required to charge the battery. The charger and EV inlet are considered part of the EV. A *connector* is a device that, by insertion into an EV inlet, establishes an electrical connection to the electric vehicle for the purpose of charging and information exchange. The EV inlet and connector together are referred to as the *coupler*. The EVSE consists of the connector, cord, and interface to utility power. The interface between the EVSE and utility power will be directly "hardwired" to a control device as illustrated in Figure 3-1, or a plug and receptacle as illustrated in Figure 3-3.

In the 1990s, there was no consensus on EV inlet and connector design. Both conductive and inductive types of couplers were designed, and in both cases, different designs of each type were provided by automakers. At the present time, however, the Society of Automotive Engineers (SAE) has agreed that all vehicles produced by automakers in the United States will conform to a single design known as the J1772 Standard.²

² While the J1772 Standard will be utilized by all automakers in the United States, it may not be the standard used in other countries. This question is the subject of a harmonization project with the Canadian Codes. A common connector is also the goal of European, Asian, and North American designers.



J1772 Connector



J1772 Inlet (right side)

Figure 3-2 J1772 Connector and Inlet (Preliminary)

The J1772 Standard EV coupler is designed for 10,000 connections and disconnections with exposure to dust, salt, and water; it is able to withstand a vehicle driving over it and is corrosion resistant.

The J1772 Standard and National Electrical Code requirements ensure multiple safety layers for EV components, including:

- The EV coupler -
 - must be engineered to prevent inadvertent disconnection.
 - must have a grounded pole that is the first to make contact and the last to break contact.
 - must contain an interlock device that prevents vehicle startup while connected.
 - must be unique to electric vehicle charging and cannot be used for other purposes.
- The EV inlet -
 - must be de-energized until it is attached to the EVSE.
 - must de-energize prior to removal of the connector.
- The EVSE -
 - must be tested and approved for use by Underwriters Laboratory (UL) or a similar nationally-recognized, independent testing lab.
 - must be able to initiate area ventilation for those specific batteries that may emit potentially explosive gases.
 - must have a charge current interrupting device (CCID) that will shut off the electricity supply if it senses a potential problem that could result in electrical shock to the user.

In addition, when connected, the vehicle charger will communicate with the EVSE to identify the circuit rating (voltage and amperage) and adjust the charge to the battery accordingly. Thus, an EVSE that is capable of delivering 20 amps will deliver that current, even if it is connected to a 40 amp rated circuit.

The J1772 coupler and EV inlet will be used for both Level 1 and Level 2 charging levels, which are described below.

B. Charging Levels

In 1991, the Infrastructure Working Council (IWC) was formed by the Electric Power Research Institute (EPRI) to establish a consensus on several aspects of EV charging. Level 1, Level 2, and DC Fast Charging levels were defined by the IWC, along with the corresponding functionality requirements and safety systems. EPRI published a document in 1994 that describes the consensus items of the IWC.³

Note: For Levels 1 and 2, the conversion of the utility AC power to the DC power required for battery charging occurs in the vehicle's on-board charger. In Level 3 charging, or more correctly called DC Fast Charging, the conversion from AC to DC power typically occurs off-board so that DC power is delivered directly to the vehicle.⁴

Level 1 – 120 volt AC

The Level 1 method uses a standard 120 volts AC (VAC) branch circuit, which is the lowest common voltage level found in both residential and commercial buildings. Typical voltage ratings can be from 110 – 120 volts AC. Typical amp ratings for these receptacles are 15 or 20 amps.

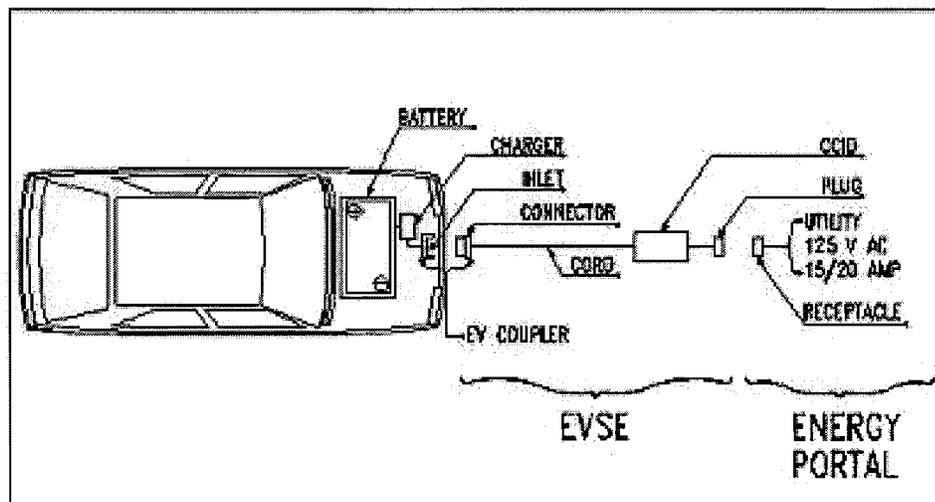


Figure 3-3 Level 1 Charging Diagram

³ "Electric Vehicle Charging Systems: Volume 2." *Report of the Connector and Connecting Station Committee*, EPRI, December 1994.

⁴ AC DC Fast Charging (delivering high-power AC directly to the vehicle) is defined within the SAE J1772 document, but this approach has not been implemented yet.

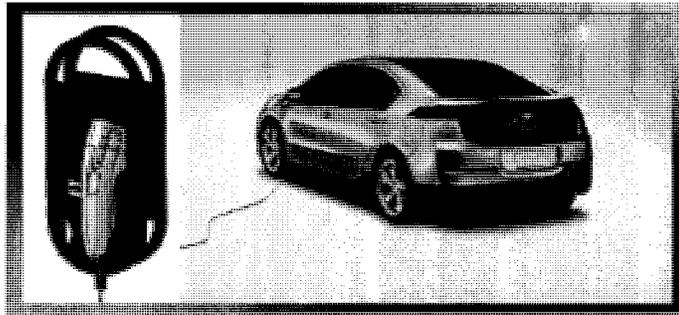


Figure 3-4 Level 1 Cord Set⁵

Level 1 charging typically uses a standard 3-prong electrical outlet (NEMA 5-15R/20R) to connect to premises wiring.

Future EV suppliers probably will provide a Level 1 Cord Set (125 VAC, 15 or 20 amps) with the vehicle. The Cord Set will use a standard 3-prong plug (NEMA 5-15P/20P), with a charge current interrupting device (CCID) located in the power supply cable within 12 inches of the plug. The vehicle connector at the other end of the cord will be the design identified in the J1772 Standard. This connector will mate properly with the vehicle inlet, also approved by J1772.

Because charge times can be very long at Level 1 (see Table 2-1), many EV owners will be more interested in Level 2 charging at home and in publicly available locations. Some EV manufacturers suggest their Level 1 Cord Set should be used only during unusual circumstances when Level 2 EVSE is not available, such as when parked overnight at a non-owner's home.

Several companies provide kits to convert internal combustion and hybrid vehicles to plug-in vehicles. Many of these conversions use a standard 3-prong electrical plug and outlet to provide Level 1 charging of their vehicles. With the standardization of EVs on the J1772 Standard and the higher level of safety afforded by a J1772-compliant charging station, existing vehicles will need to be retrofitted to accommodate a J1772 inlet in order to take advantage of the deployment of EVSE infrastructure.

Level 2 – 240 volt AC

Level 2 is typically described as the “primary” and “preferred” method for the EVSE for both private and publicly available facilities, and specifies a single-phase branch circuit with typical voltage ratings from 220 – 240 volts AC. The J1772-approved connector allows for current as high as 80 amps AC (100 amp rated circuit). However, current levels that high are rare; a more typical rating would be 40 amps AC, which allows a maximum current of 32 amps. This provides approximately 7.7 kW with a 240 VAC circuit.

⁵ Conceptual Design for Chevy Volt, *Electrifying the Nation*, PHEV Summit, Tony Posawatz, January 2009.

The higher voltage of Level 2 allows a much faster battery charge. Because of the higher voltage, Level 2 has a higher level of safety requirements than Level 1 under the National Electric Code (NEC), including the requirement that the connector and cord be hardwired to the control device and premises wiring, as illustrated in Figures 3-1 on and 3-3.



Figure 3-5 Level 2 Charging

Level 3 Charging (DC Fast Charging)

Level 3 Charging, or preferably, “DC Fast Charging”, is planned for commercial and public applications and is intended to perform in a manner similar to a commercial gasoline service station, in that recharge is rapid. Typically, DC Fast Charging would provide a 50% recharge in 10 to 15 minutes. DC Fast Charging typically uses an off-board charger to provide the AC to DC conversion. The vehicle’s on-board battery management system controls the off-board charger to deliver DC directly to the battery.

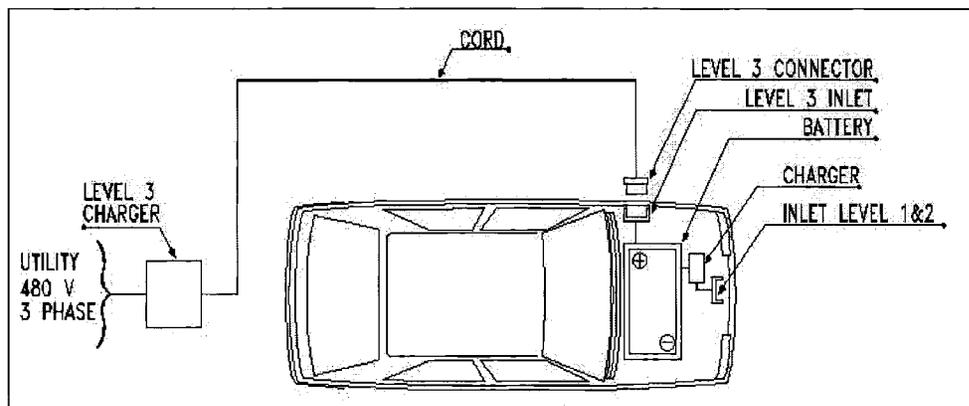


Figure 3-6 DC Fast Charging

This off-board charger is serviced by a three-phase circuit at 208, 480, or 600VAC. The SAE standards committee is working on a DC Fast Charging connector, but has placed the highest priority in getting the Level 1 and 2

connector approved first. The DC Fast Charger connector standard is expected to be approved in 2010.

eTec will be utilizing DC Fast Charging equipment in infrastructure developed in 2010.

DC Fast Charging was accomplished by eTec for the Chrysler EPIC in the 1990s and for industrial applications since 1998. Similar, though smaller, equipment will be used for the coming generation of EVs.

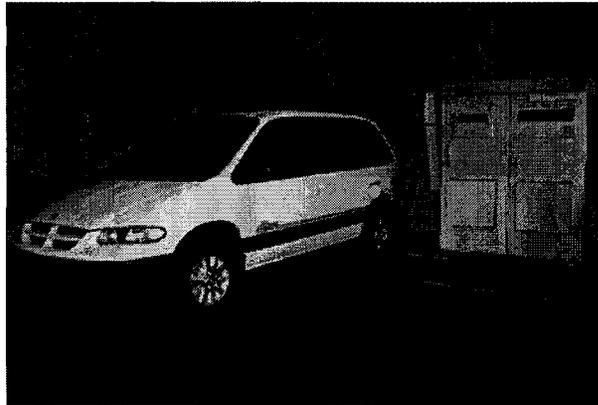


Figure 3-7 Chrysler EPIC DC Fast Charging (90kW) circa 1997

Note: Although it will be uncommon, a vehicle manufacturer may choose not to incorporate an on-board charger for Levels 1 and 2, and instead utilize an off-board DC charger for all power levels. In this case, the electric vehicle would have only a DC charge port. Another potential configuration that may be found, particularly with commercial vehicles, is providing 3-phase power directly to the vehicle. This configuration requires dedicated charging equipment that will be non-compatible with typical publicly available infrastructure.

C. Level 1 versus Level 2 Considerations

For a BEV owner (and some PHEV owners who choose the utility time-of-use rates), the preferred method of residential charging will be Level 2 (240VAC/single-phase power), providing the EV owner a reasonable charge time and also allowing the local utility the ability to shift load as necessary while not impacting the customer's desire to obtain a full charge by morning. For other PHEV owners, a dedicated Level 1 circuit may adequately meet the owner's charging needs.

BEV owners who have the opportunity for Level 2 charging at work or in public areas may find the vehicle battery remains at a higher charge and thus home charging time is not a concern and Level 1 will suffice. See Table 2-1 for relative battery sizes and estimated recharge times.

D. General Requirements

This section identifies the general requirements of EVSE.

Certification: EVSE will meet the appropriate codes and standards and will be certified and so marked by a Nationally Recognized Testing Laboratory (e.g., Underwriters Laboratories). Owners should be cautioned against using equipment that has not been certified for EV use.

Cord Length: The EVSE will provide a maximum of 25 feet of flexibility from the wall location to the EV Inlet. This figure was obtained by starting with the typical 15-foot car length and adding the 7-foot car width plus 3 feet to the EVSE's permanent location. The EV inlet location on each EV model will vary by manufacturer; however, this standard length should be sufficient to reach from a reasonably positioned EVSE to the inlet.

Tripping Hazard: An extended EV cord may present a tripping hazard, so the EVSE should be located in an area of minimum pedestrian traffic. An alternative would be installation of an overhead support or trolley system to allow the cord to hang above the vehicle in the location of the EV inlet.

Ventilation Requirements: If there are ventilation requirements, the EVSE will be required to energize a properly-sized ventilation system. This requirement is expected to be rare, since automobile manufacturers are expected to use non-gassing batteries. Some EV owners who convert their own vehicles to electric or purchase conversion vehicles may use gassing batteries, however. The approved EVSE will communicate with the vehicle and if ventilation is required but no ventilation system exists, the EVSE will not charge the vehicle. In multi-family or parking garage situations that may already have ventilation systems for exhaust of normal vehicle emissions, that system generally would be sufficient. However, calculations should verify this result. It also may be impractical to wire the charger to the ventilation controls or costly to run the system for a single vehicle charging. In these cases, it may be prudent to specify that the chargers are intended for non-gassing batteries only.

Energized Equipment: Unless de-energized by the local disconnect, the EVSE is considered electrically energized equipment. Because it operates above 50 volts, Part 19 Electrical Safety of the Occupational Health and Safety (OHS) Regulation requires guarding of live parts. EVSE may be positioned in a way that requires a physical barrier for its protection. Wheel stops are recommended to prevent a vehicle from contacting the EVSE. They also help position the EV in the optimum location for charging.

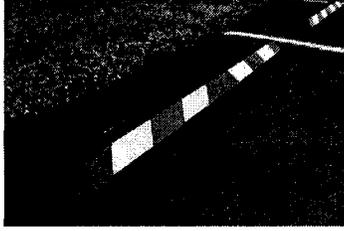


Figure 3-8 Wheel Stop⁶

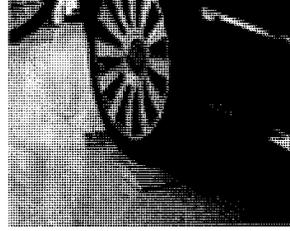


Figure 3-9 Garage Wheel Stop⁷

Shortest Run: In addition to the above requirements, the lowest-cost installation generally is the location closest to the electrical supply breaker, because it minimizes the conduit run to the charger.

Ergonomics/Ease of Use: Most EV owners will find it most convenient to have the EVSE located near the EV inlet. In some cases, it may be desirable to back into the garage, both minimizing the tripping hazard and reducing the electrical circuit run to the EVSE.

⁶ Rubberform Recycled Products LLC, www.rubberform.com

⁷ ProPark Garage Wheel Stop, www.organizeit.com

4. Charging Scenarios

A. Single Attached/Detached Garages

Power Requirements

Level 1: Dedicated branch circuit with NEMA 5-15R or 5-20R Receptacle.

Level 2: Dedicated branch circuit hardwired to a permanently-mounted EVSE with the following specifications: 240VAC/Single Phase, 4-wire (2 Hot, GND, and Neutral), 40 amp Breaker.

Cost Estimates

\$2,000 - \$2,500 for a generic installation. Costs will vary based on length of the circuit run, electrical panel upgrades, and other factors.

Level 2 Notes

- The breaker size recommended will meet the requirements of almost all BEVs and PHEVs. Some PHEVs with small battery packs (see Table 2.1) may only require a 20 or 30 amp breaker for their recommended EVSE, in which case the breaker can be easily changed.
- The Neutral may not be required by some EVSE, but since it is inexpensive to include and may be required in the future if a different vehicle is purchased, it is recommended.
- For new construction, bring the circuit to a dual gang box with a cover plate for future installation of EVSE.
- For new construction that is incorporating an advanced internet network within the home, an internet connection at the EVSE location would be advisable. For existing homes, the value of providing an internet connection at the EVSE location is unknown at this time and is left up to the individual homeowner. It is likely that wireless methods will be available where a hard connection is not available.
- Many Level 2 EVSE suppliers will provide controls in the EVSE to enable charging at programmable times to take advantage of off-peak power pricing. If not, homeowners may desire to install a timing device in this circuit to control charging times.

Siting Requirements

An indoor-rated EVSE is acceptable for an enclosed garage. The EV owner probably will prefer a particular location for the EV. However, the EV should be positioned so that the general requirements described previously are considered, which often means the EV will be at the furthest point from the residence entry into the garage.

The installation of the EVSE at the front of the vehicle may be acceptable unless the cord becomes a tripping hazard. Often the EVSE will be placed on an exterior wall to shorten the distance from the electrical box and at the same time positioning the EVSE out of the way.

If the EVSE is to be installed after the EV has been purchased, the location of the EV inlet will play a part in the location of the EVSE. It is best to keep the EVSE as close to the inlet as possible to minimize how much the cord is spread out on the floor. If the branch circuit is installed prior to the EV purchase, the garage junction box should be on the wall closest to the utility service connection, consistent with the general requirements for EVSE. Typical locations are shown in Figure 4-1.

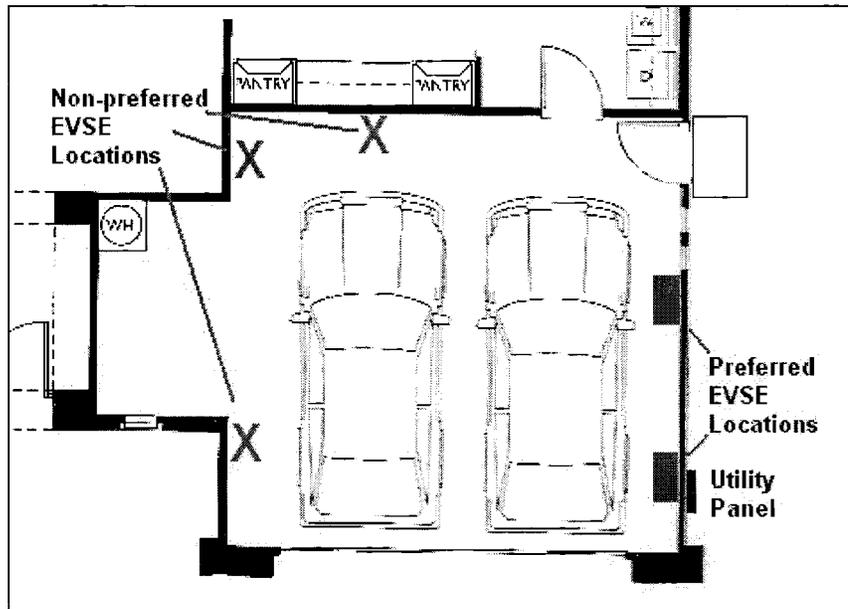


Figure 4-1 Double Garage Location for EVSE

In the above figure, the best location would be the EV on the right. The non-preferred EVSE locations are in typical walking areas and could present a tripping hazard. In addition, these locations are further away from the utility panel. If the EV owner wishes to place the EVSE in these locations, one option would be to install an overhead support for the charge cable and connector. If the EV inlet is on the left side of the vehicle, the owner could consider backing into the garage.

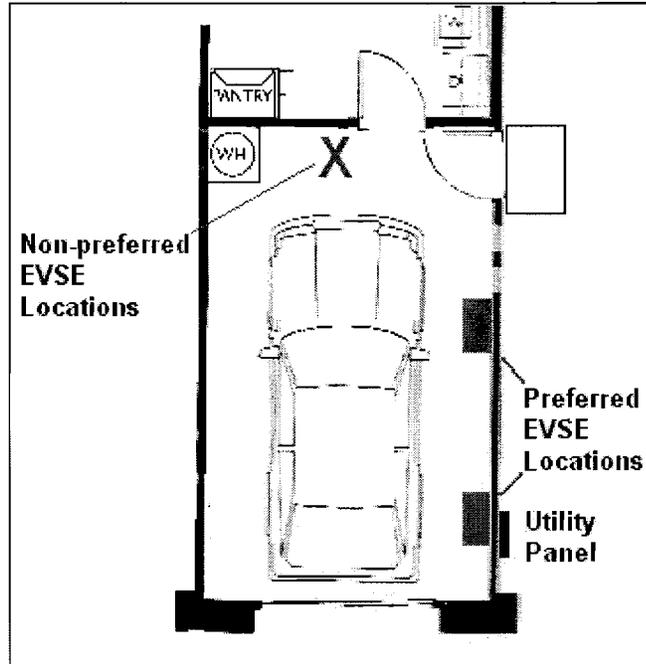


Figure 4-2 Typical Single Garage Location for EVSE

In the single garage environment, most locations will be acceptable for placing the EVSE, except perhaps at the head of the vehicle because of tripping concerns. The preferred locations were selected due to proximity to the utility panel. Again, the option of using overhead support for the EVSE cable would allow EVSE installation where the owner prefers.

The National Electrical Code provides additional requirements should the EVSE be located in a hazardous area. Any other materials stored in the garage also should be considered when placing the EVSE, particularly if they are hazardous.

Detached garages will add additional considerations when routing the electrical supply to the garage. Landscaping will be disrupted during the installation process, which may be of great significance to the owner and should be planned thoroughly in advance.

Installation Process

Installing an EVSE in a residential garage typically consists of installing a dedicated branch circuit from an existing house distribution panel to an EV outlet receptacle (125 VAC, 15/20 A) in the case of Level 1 charging or an EVSE (operating at 240 VAC, 40 A) for Level 2 charging. If the garage is built with the conduit or raceway already installed from the panel to the garage, the task is greatly simplified.

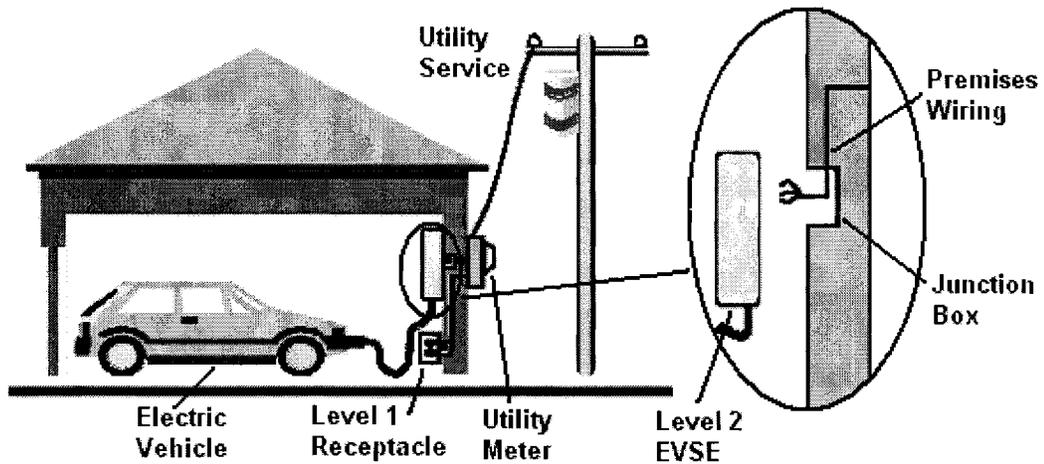


Figure 4-3 Typical Level 1 and Level 2 Installations for a Residential Garage

The specific steps involved in this process are shown in the flowchart in Figure 4-4. In general, they include:

- Consultation with the EV dealer to determine whether Level 1 or Level 2 EVSE is required, whether ventilation will be required, and which EVSE to purchase
- Consultation with the electric utility to determine rate structure, as well as any requirements for a special or second meter
- Consultation with a licensed electrical contractor to plan the installation effort, including location of the EVSE, routing the raceway from the utility service panel to the EVSE, Level 1 or Level 2 requirements, ventilation requirements, adequacy of current utility service, and preparing an installation quote
- Submission of required permitting documents and plans
- Completion of EVSE installation and utility service components, if required
- Inspection of final installation

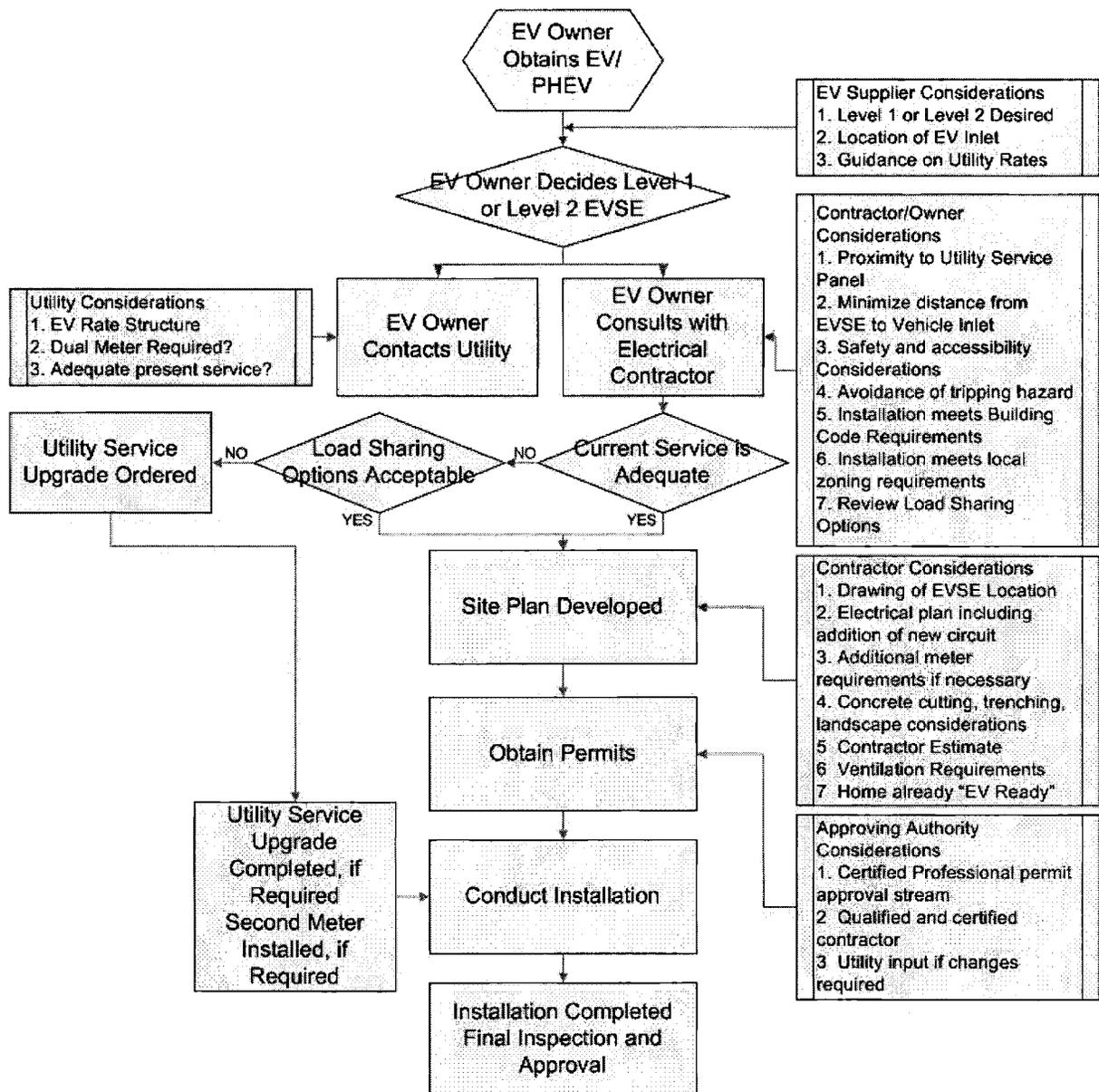


Figure 4-4 Installation Process for a Residential Garage/Carport

If the garage has a pre-existing raceway, a 120 VAC, 15/20 amp circuit or a 240 VAC, 40 amp circuit can be installed. Some homes may not have sufficient utility electrical service to install this circuit. In that case, either a new service must be added, as previously noted, or installation of an approved load control device may allow the homeowner to avoid a major panel upgrade and allow the utility to avoid upgrading the electrical service to the homeowner.

Although a new home may already have the raceway installed, a permit for the service is required. Increasingly, standards are directing that a raceway

for an electric vehicle will be included in new home construction. The conductors may or may not be included. If included, consideration should be given to sizing the conductors for the 240 VAC, 40 amp circuit required for Level 2 charging, but installing the 125 VAC, 20 amp Level 1 breaker and receptacle. The homeowner would have a functional circuit that could be upgraded easily to Level 2, if desired.

Contact a local electrical contractor to evaluate the options of adding a new service vs. upgrading the existing service, as utility fees may apply.

B. Carport

Power Requirements

Power requirements are the same as garage scenario above.

Cost Estimates

\$2,000 - \$2,500 for a generic installation. Costs will vary based on the length of the circuit run, electrical panel upgrades, and other factors.

Siting Requirements

The siting requirements for the carport will include those identified for the garage. Some owners may elect to place the EVSE in the garage, but charge a vehicle outdoors. This scenario is similar to the carport requirements. A carport is considered an outdoor area, so the EVSE should be properly designed for exterior use. Consideration must be given to precipitation and temperature extremes. In geographic areas that experience high precipitation, pooling of water in the carport or driveway may be a concern. While the EVSE is safe, owners may have a concern about standing in pooled water while connecting the EVSE. Consultation with the owner will be required when locating the EVSE.

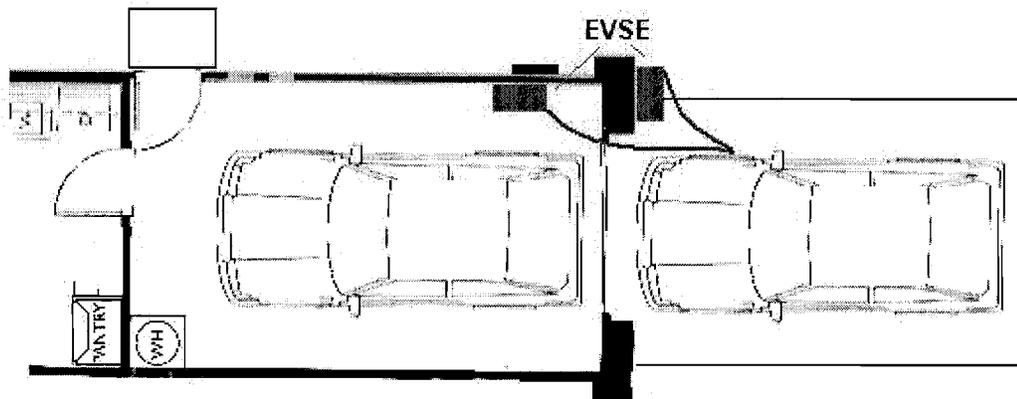


Figure 4-5 Installation Considerations for Outdoor Parking

Freezing temperatures can cause cords to freeze to the parking surface, so cord support should be considered in a cold environment. Adequate lighting is an additional consideration, along with mitigating efforts to prevent vandalism, as noted in Section 5. The carport installation process is similar to the garage process previously outlined.

Consultation with Landlord or HOA

An installation in a multi-family location may involve a more lengthy approval process for zoning considerations. The local zoning requirements may require a public hearing or pre-approval by a Design Review Committee.

C. Multi-Family Dwellings

Power Requirements

Power requirements are the same as the garage scenario.

Cost Estimates

Costs will vary based on length of the circuit run, trenching, electrical panel upgrades, and other factors.

Siting Requirements

Multi-family dwellings will have additional considerations, because the apartment or condominium owner also must be involved in any siting decisions. The EV owner will prefer a site close to the owner's dwelling, but this may not be in the best interest of the apartment owner. Special flooding or drainage conditions may apply. Lighting and vandalism concerns will exist. Payment methods for the electrical usage will need to be identified. There may be insurance and liability questions. All concerns should be discussed with the property owner prior to the EV purchase.

Should the EV owner later relocate, the electrical installation raceway and panel upgrades, if any, will be retained at the multi-family location. Ownership of the EVSE needs to be identified clearly. If the EV owner takes the EVSE, site restoration may be required. Circuit removal or de-energizing methods should be settled. Discussion with the utility also is required, since there may be metering questions or issues to be resolved. In condominiums, the Homeowners' Association (HOA) may be involved to approve EV additions.

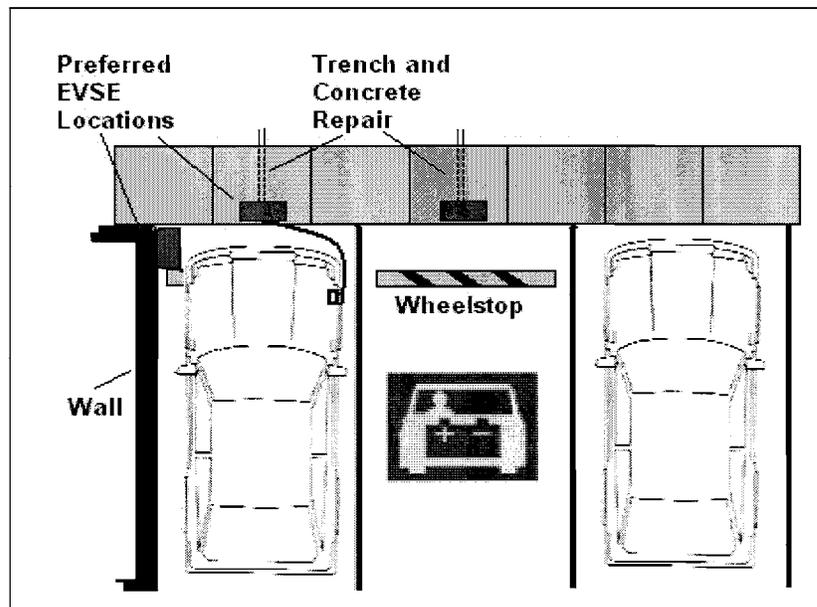


Figure 4-6 Typical EVSE Installation in Multi-Family Lot

In general, unless the location is well protected from the environment, the EVSE will need to be outdoor rated. The installation of the EVSE at the front of the vehicle may be the only choice unless an adjacent wall is available. If located at the front of the parking stall, the EVSE should be located on the vehicle side of any walkway to minimize the cord becoming a tripping hazard. The walkway for pedestrians would be on the back side of the EVSE. Because a wheel stop will be installed, consideration should also be given to make sure the EV parking is not in an area of normal pedestrian traffic in order to avoid pedestrians tripping over the wheel stop when no vehicle is present.

Trenching and concrete work and repairs are likely. Consideration must be given to maintaining a safe and secure area around the parking stall to avoid tripping hazards or EVSE interference with other operations.

Installation Process

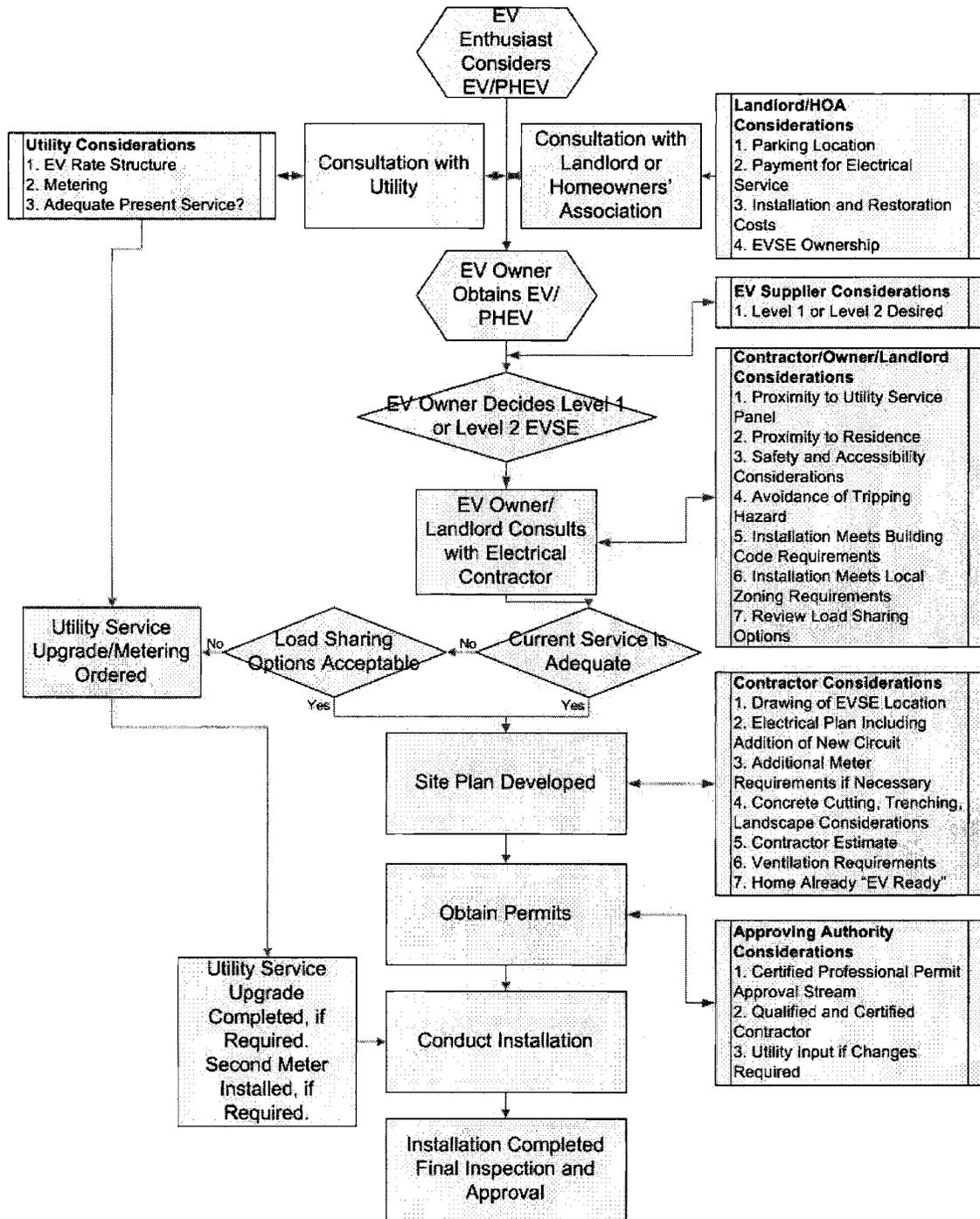


Figure 4-7 Installation Process for Multi-Family

If the parking area has a pre-existing raceway, the wishes of the EV owner and property owner can determine whether this will be a 120 VAC, 15/20 amp circuit or a 240 VAC, 40 amp circuit. This also would require review by an electrical contractor to make sure the service panel is sufficient to support the choice. Although a raceway may have been installed previously, a permit for the service will be required.

Multiple Parking Stall Installation

In a new construction or retrofit situation, broad charging infrastructure installation in a multi-residential building will require the services of an electrical consultant to determine the best approach. For example, the proponent may consider a load control strategy to manage the charging load within the capacity of the electrical service to the building, rather than upgrading the service size to accommodate increased building load from electric vehicle charging.

D. Commercial Fleets

Power Requirements

Dedicated branch circuits hardwired to permanently-mounted EVSE with the following specifications: 208VAC or 240VAC / Single-Phase, 4-wire (2 Hot, GND, Neutral), 40Amp Breaker.

Commercial fleet charge stations generally will include multiple charging station locations, and therefore with new construction, these additional locations will need to be allowed for when sizing the main service entrance section (SES). Since it is likely that most of the charging will occur during working hours, for existing buildings, the additional load may require an upgrade or a new SES and/or utility supply.

Because of the potentially large electrical load, it is recommended that a network connection is provided in close proximity to the charge stations. This connection may be required for interface with the building energy management system or to implement local utility load control strategies.

Cost Estimates

\$40,000 - \$50,000 for a generic installation of ten EVSE stations. Costs will vary based on length of the circuit run, trenching, electrical panel upgrades, and other factors.

Siting Requirements

Presently, commercial fleets make up the highest population of EVs. Utilities, governmental agencies, and other private fleets have been encouraged and are encouraging the private adoption of EVs. A significant amount of planning is required to correctly size an EV parking and charging area. Consideration must be given to current requirements, as well as anticipated future requirements. Electrical service requirements will be much higher than residential or multi-family installations, and can have a significant impact on electrical usage and the utility. For that reason, electrical utility planners need to be involved early on in the fleet planning process.

The individual homeowner will be interested in charging his/her vehicle off-peak. That interest will be greater for the fleet manager.

Flood-prone area restrictions must be considered, as well as issues of standing water. Often large parking lots have low spots where water accumulates. Although a Level 2 EVSE contains the proper protection device for this issue, employees may not be comfortable operating the EVSE in standing water.

Installation of an EVSE unit in a commercial facility typically consists of installing new dedicated branch circuits from the central meter distribution panel to a Level 2 EVSE. In a commercial fleet, there are typically many such EVSE units in adjacent parking stalls. Proximity to the electrical service is an important factor in locating this parking area. The length of the circuit run and the number of units will have a significant impact on the cost.

Because these EVSE units are in a designated area, the potential for pedestrian traffic is less and more consideration can be given to the most economical installation methods. In addition, the commercial nature of the site will allow greater overall security, such as fences and gates, so the threat of vandalism is minimized.

Fleet managers must also be aware of other equipment that will be stored in the vicinity of the EVSE. It is important that a hazardous environment does not already exist in the area planned.

Fleet manager interests and priorities can also stimulate the development of DC Fast Charging. The higher recharge rate means a shorter turnaround for each vehicle and maximizes on-road time. The 480/600 VAC required for DC Fast Charging is generally available in commercial facilities.



Figure 4-8 Level 2 Commercial EV Charging Location

Installation Process

The installation process is similar to the processes described previously, except that much more detailed planning is involved prior to the owner making the final decision and obtaining permits.

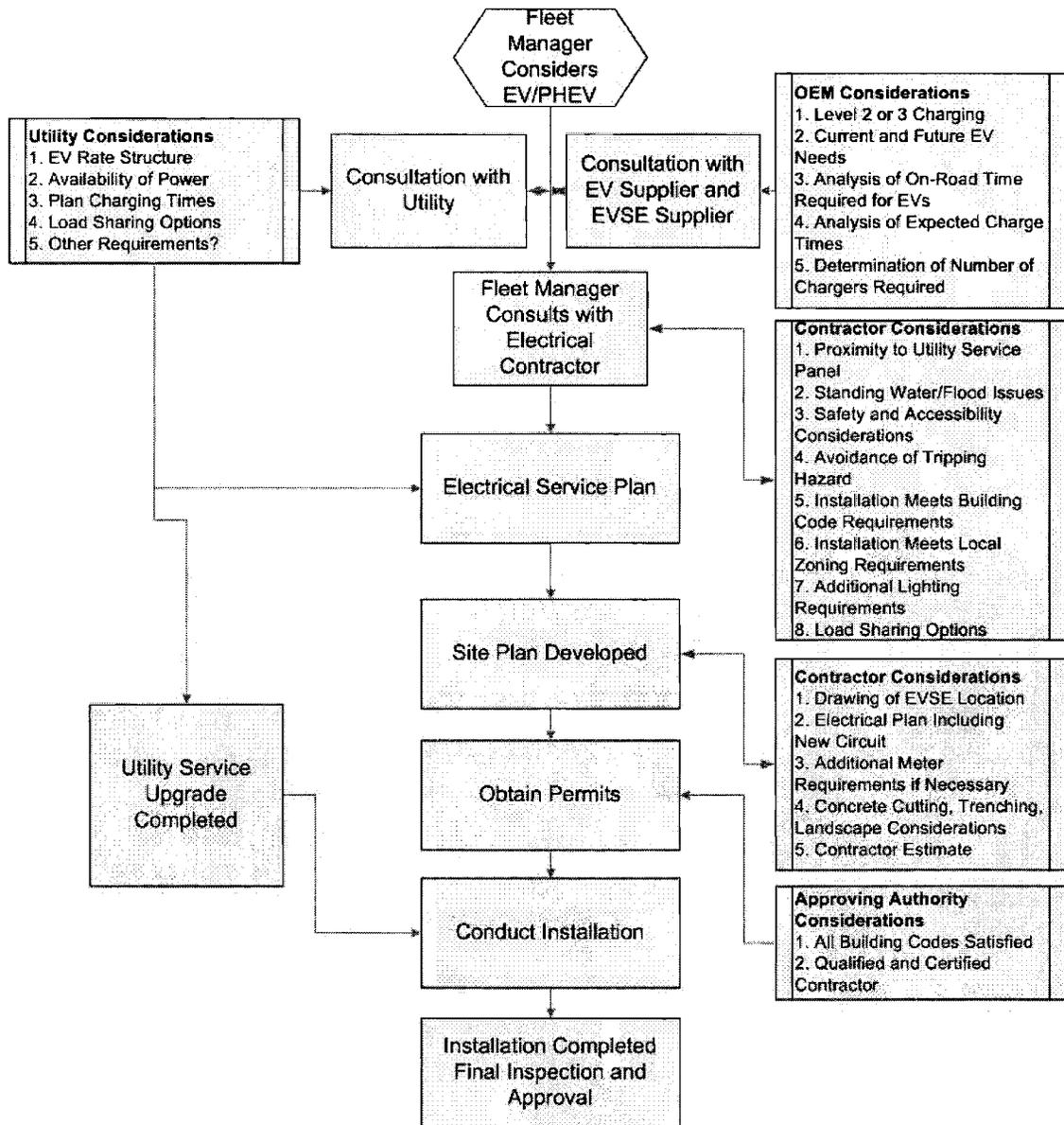


Figure 4-9 Installation Process for Commercial Fleet Operations

E. Publicly Available Charging Stations

A significant factor in the consumer adoption of EVs will be the ability to extend the range of battery-only power. This can be accomplished by the thoughtful installation of publicly available charging locations. The EV Micro-Climate program focuses on this important area.

Publicly available charging may employ a mix of Level 1, 2, and 3 (DC Fast) charging stations; however, the charge return generated by a dedicated Level 1 charging station will be minimal for a BEV, and its use is neither recommended nor included in the EV Micro-Climate. The recommended configuration for a publicly available Level 2 charging station is one equipped with a J1772

connector. This will accommodate all vehicles equipped with a J1772 inlet, including PHEVs and other EVs that require lower kW charging than a BEV.

Publicly available charging may be served by either public or commercial charging stations. Public charging stations are those EVSE installed on public-owned property, such as city or county property. Curbside chargers are a typical example. Commercial charging stations are those EVSE stations installed on private or commercial property, such as retail locations.

The determination of publicly available Level 2 EVSE charging sites should focus on locations where the EV owner will be parked for a significant period of time, i.e., 1 – 3 hours. An appreciable recharge can occur during this time period. Locations where owners can be expected to park for this length of time include restaurants, theaters, shopping malls, governmental facilities, hotels, amusement parks, public parks, sports venues, arts productions, museums, libraries, outlet malls, airport visitor lots, and major retail outlets, among many other choices.

Businesses, such as electric utilities or those that wish to promote EV usage, will install public charging near their building entrance in highly visible areas, even though EV owner stay times may be shorter than 1 – 3 hours. As noted above, these stations should be Level 2.

The determination of publicly available DC Fast Charging EVSE charging sites should focus on locations where the EV owner will be parked for a relatively short period of time, e.g., 15 minutes, where an appreciable recharge can occur during this time period. Locations where owners can be expected to park for this time include convenience stores, coffee houses, service stations, drug stores, and fast food restaurants, among many other choices. For DC Fast Charging, the availability of 480/600 VAC will be a consideration.

Publicly available charge stations will vary greatly in design and requirements. They also include a number of other requirements not found in residential and fleet applications, such as signage and point-of-sale systems, as described in Section 5.

LEED Building Certification

A driving force in the design, construction, and operation of facilities is the Leadership in Energy and Environmental Design (LEED) Green Building Rating System. It was developed by the U.S. Green Building Council and it provides standards for environmentally sustainable construction and facility operation. It requires a study of the CO₂ emissions by company personnel and encourages, through monetary incentives or preferred parking, the use of alternative fuel vehicles. It provides credits for installing EV charging stations and suggests certain percentages of parking be devoted to alternative fuel vehicles. These locations will apply to employees, as well as visitors using the facility. Companies interested in being LEED-certified are excellent sites for publicly available charging stations.

Power Requirements

Level 2: Dedicated branch circuits hardwired to permanently-mounted EVSE with the following specifications: 208VAC or 240VAC / Single-Phase, 4-wire (2 Hot, GND, Neutral), 40Amp Breaker.

DC Fast Charging: Dedicated branch circuit hardwired to permanently-mounted charger supplied with the circuit, as specified in the installation manual. DC Fast Charging chargers rated up to 30kW may require either 208AVC/3-Phase or 480VAC/3-Phase. DC Fast Charging chargers greater than 30kW probably will require 480VAC/3Phase.

Example Sizes

1. For 30kW Output Power, typical input power requirements are:

208VAC/3-Phase, 4-wire (3-Hot, GND), 125 Amp Breaker, -or-

480VAC/3-Phase, 4-wire (3-Hot, GND), 60 Amp Breaker

2. For 60kW Output Power, typical input power requirement is:

480VAC/3-Phase, 4-wire (3-Hot, GND), 125 Amp Breaker

Communication generally will be preferable for any publicly available charge stations, but it is not necessarily required. Wireless methods most likely will be utilized, but if a hardwired internet connection is available, it is generally preferable to wireless.

Siting Requirements

Siting requirements for publicly available charging are similar to other scenarios previously discussed, but involve many additional considerations. Questions such as ownership, vandalism, payment for use, maintenance, and data collection are addressed in following sections.

Flood-prone area restrictions must be considered, as well as issues of standing water or high precipitation. As previously noted, despite the safety of the device, users may not be comfortable operating the EVSE in standing water. Unlike fleet use, an area designated for public use should be in a preferred parking area. Also unlike fleet use, the area will be public, and therefore the threat for vandalism will be greater. Public chargers likely will be in a high pedestrian traffic area, so considerations around placing the charger to best avoid making the charge cord or the wheel stop into tripping hazards are very important.

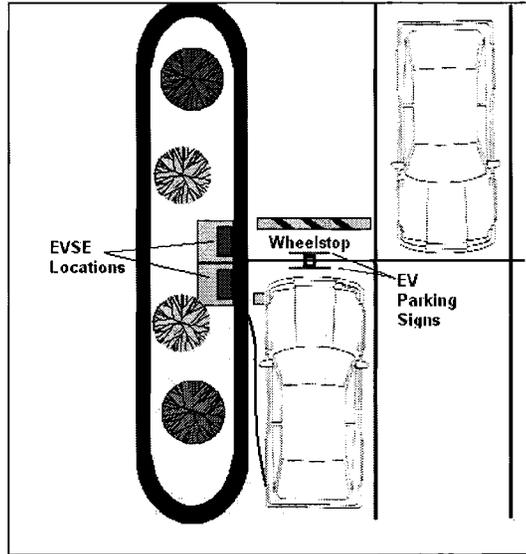


Figure 4-10 Publicly Available Charging Layout Example

There are several ways to address the protection of the equipment, shelter, signage, and pedestrian safety. The following pictures provide examples.

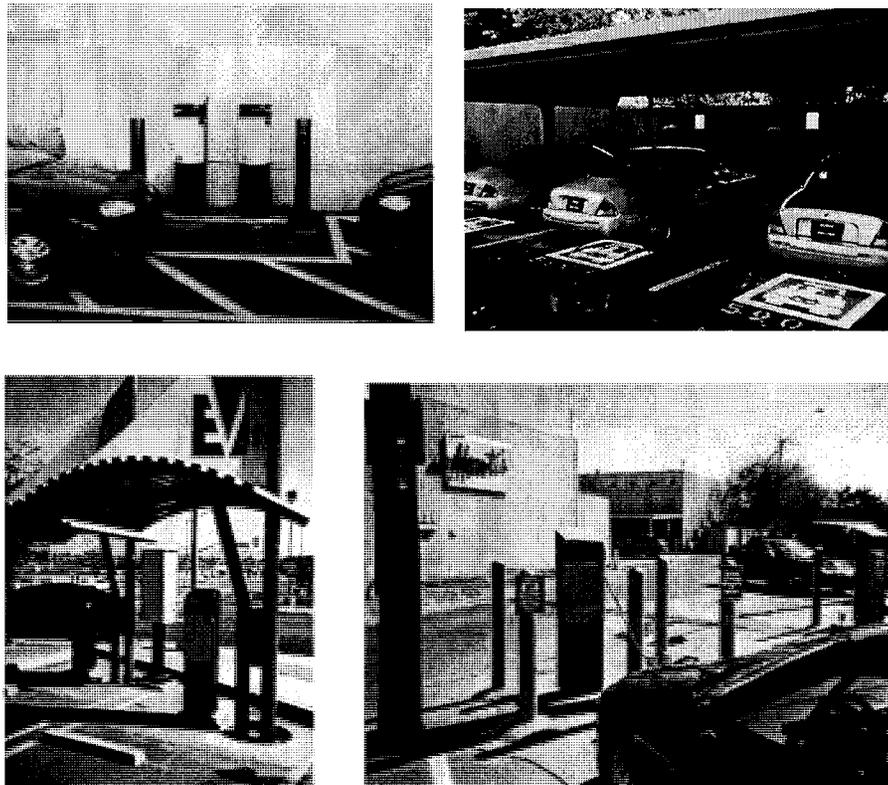


Figure 4-11 Publicly Available Charging Examples

Some publicly available charging will be advanced by commercial businesses interested in promoting electric vehicle use through personal preference or as part of LEED certification. Commercial businesses may decide on their own to purchase and install systems or to share in these costs. Other business owners will be receptive to placement of chargers in their parking lots once approached with incentives. Other public, private, and governmental agencies will install EVSE out of support for EVs. Mapping these selected locations will provide input to an overall municipal plan identifying the ideal sites to ensure wide coverage of publicly available charging.

Publicly available sites also will need to conform to accessibility requirements, as well as requirements for the number of parking stalls with EVSE that are accessible. This issue is discussed further in Section 5.

Lighting and shelter are extremely important in public sites. The EV owner must feel safe when parking at night. In addition, the EV owner must be able to read directions and properly locate the EV connector and insert it into the EV inlet. An indoor stall in a parking structure or a sheltered stall in the outdoor parking lot provides additional convenience for the EV owner (see Figure 5-3).

Installation of the EVSE in a public area typically consists of installing new dedicated branch circuits from the central meter distribution panel to a Level 2 EVSE. There likely will be many such EVSE units in adjacent parking stalls. Proximity to the electrical service is an important factor in locating this parking area. The length of the circuit run and the number of units will have a significant impact on the cost.

The cost of providing power to the EV parking location must be balanced with the convenience of the parking location to the facilities being visited by the EV owner. It may be more convenient for the EV owner if a large shopping mall has two or three EV parking areas rather than one large area, although the cost for three areas will be greater than the cost for one.

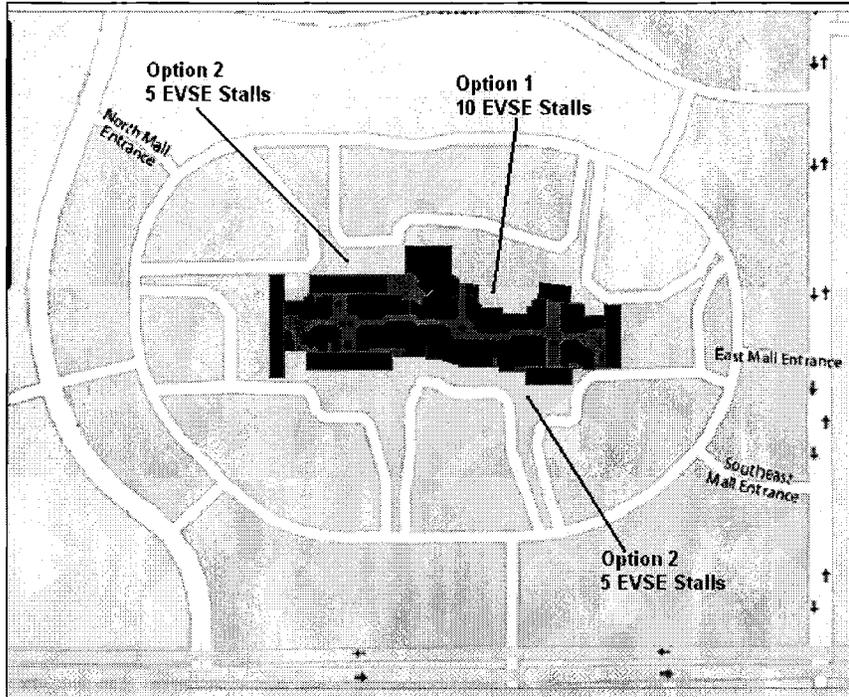


Figure 4-12 Shopping Mall EVSE Parking Example

Local area aesthetics also are important, and may require the installation of landscaping or screening walls to shield the electrical transformer, panel, or other equipment from the public eye.

Trouble reporting can be very important in public charging areas. Each publicly available charging area should be equipped with a method whereby the EV user can notify the equipment owner of trouble found with the equipment. Public satisfaction will suffer if stations are f out of service or not kept in an appealing condition. The trouble-reporting solution may be a normal business call number or a service call number that monitors many publicly available charging locations. This will require a communications line. At a minimum, a sign may be posted at the EVSE location directing comments to a particular office or store location.



Figure 4-13 Indoor Charging

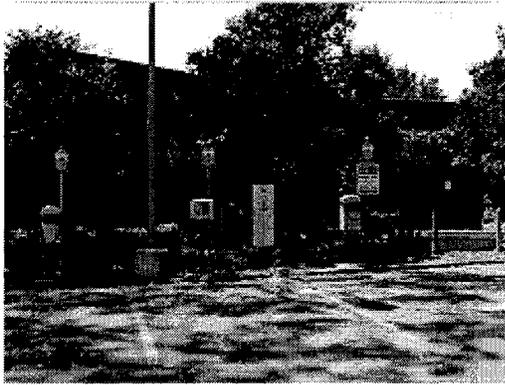


Figure 4-14 Outdoor Charging

Installation Process

The installation process is similar to the processes shown previously, but more detailed planning is required before submitting plans to obtain permits.

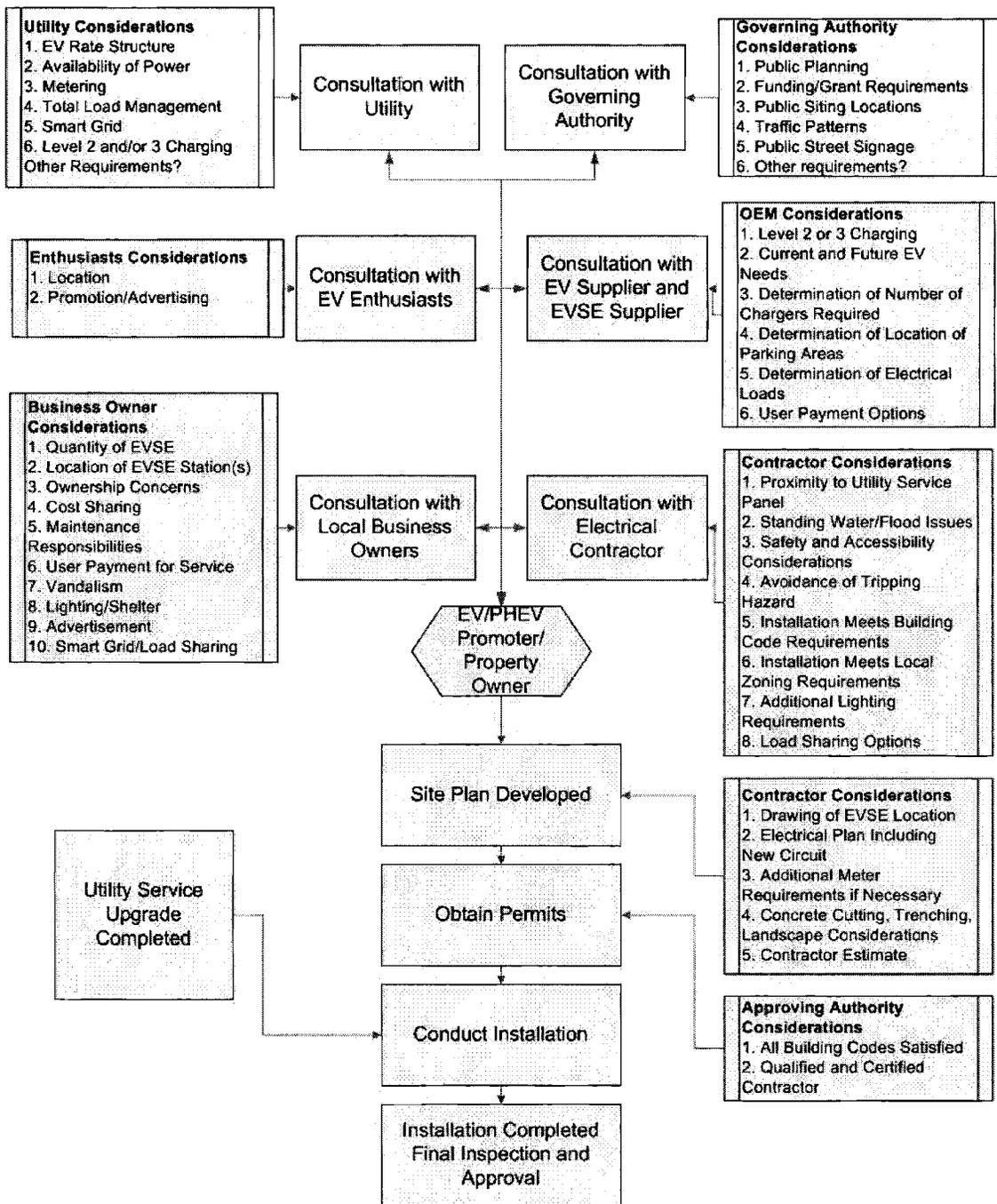


Figure 4-15 Installation Flowchart for Public Charging

The quality of the advance planning will determine the quality of the final installation and, ultimately, the EV owner's acceptance and satisfaction.

Curbside Charging

Curbside charging is not necessarily associated with a commercial business. Generally speaking, these areas are owned by the municipality rather than private interests. Many of the same considerations previously noted apply.



Figure 4-16 Curbside Charging

5. Additional Charging Considerations

A. Signage

In addition to the signs and warnings required by NEC that are described in Section 6, information signage is recommended for publicly available charging stations. Signage has two purposes: keeping non-EV vehicles from parking in charging station stalls, and helping EV drivers find charging stations.

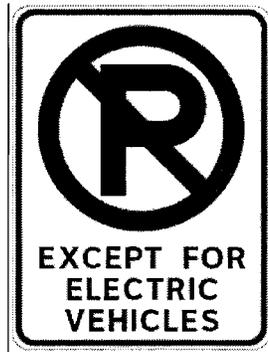


Figure 5-1 No Parking Except for Electric Vehicles Sign

Previous experience has shown that signs that follow the red on white standards for *No Parking* work best to keep non-EV drivers from occupying charging station stalls. The *Manual on Uniform Traffic Control (MUTCD)* defines the standards used by road managers nationwide to install and maintain traffic control devices on all public streets, highways, and private roads open to the public. The example in Figure 5-1 follows MUTCD standards. The general public does not recognize friendly green or blue *EV Parking* or *EV Parking Only* signs. If the signage is blue, it can be mistaken for an accessible location; green signs often are mistaken for short-term parking signs.

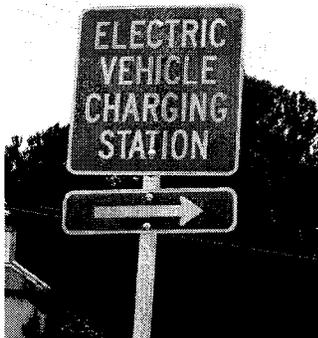


Figure 5-2 Wayfinding Sign

Widespread adoption of EVs will include maps or websites identifying charging locations. It is helpful to post EV parking area signs on adjacent streets and access points directing EV drivers to the charging locations. A wide variety of symbols for charging station wayfinding were developed in the mid-1990s. A number of designs have been suggested to update these symbols. Stakeholders have identified several criteria, including being able to symbolize the next generation of EVs that do not use lead-acid batteries, as well as modern charging stations that do not have a two-prong plug extending from the vehicle or charging station. Ideally, a common design to indicate charging station locations will be used on federal and state highways and local streets.

B. Lighting and Shelter

For commercial, apartment, condominium, and fleet charging stations, adequate lighting is recommended for safety and convenience. Shelter is not typically required for outdoor-rated equipment. For geographic locations that have significant rainfall or snow, providing shelter over the charging equipment will provide added convenience for potential EV users. Locations within parking garages or private garages that are well protected from the environment may utilize EVSE that is not specifically outdoor rated.

Lighting should be sufficient to easily read associated signs, instructions, or controls on the EVSE and provide sufficient lighting around the vehicle for all possible EV Inlet locations.

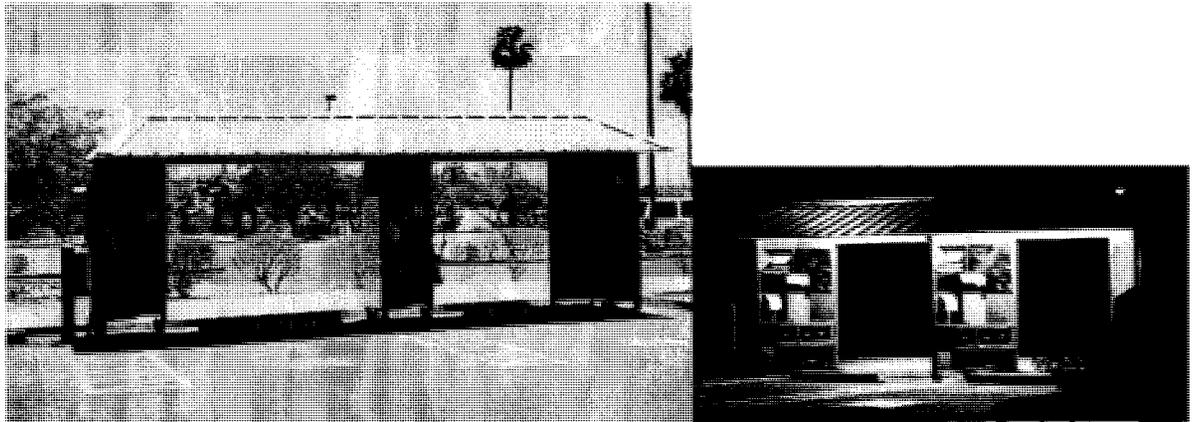


Figure 5-3 Public Charging with Shelter and Lighting

In residential garages or carports, lighting is also important so pedestrians can avoid tripping over extended charge cords while the EV is charging.

C. Accessibility Recommendations

Current state and federal regulations do not provide design criteria that specifically address EV parking and charging; however, certain design requirements were added to the NEC for accessible EVSE, and some

municipalities provide guidance for accessible EV parking locations. New standards may be developed; therefore, recommendations herein constitute the best guidance to date.

There are two possible scenarios to consider when establishing charging stations and accommodating persons with disabilities: where the primary purpose is EV charging, and where the primary purpose is accessible parking.

EV Charging is the Primary Purpose

When EV charging stations are provided at a site in addition to regular parking, EV charging is considered the primary purpose. Parking spaces with accessible EV charging stations are not reserved exclusively for the use of persons with disabilities and a disabled parking pass would not be required.

To enable persons with disabilities to have access to a charging station, EV connectors should be stored or located within accessible reach ranges. In addition, the charging station should be on a route that is accessible both between the charging station and the vehicle and all around the vehicle.

Accessible EV charging stations should be provided according to Table 5-1.

Table 5-1 Accessible Charging Station Recommendations

EV Charging Stations	Accessible EV Charging Stations
1 – 50	1
51 – 100	2

The accessible EV charging stations should be located in close proximity to major buildings and site facilities; however, these charging stations need not be located immediately adjacent to the buildings and other facilities like traditional ADA parking, since EV charging, not parking, is considered the primary purpose.

Accessible Parking is the Primary Purpose

If a charging station is placed in an existing accessible parking space, then the primary use of that space must be accessible parking; that is, a disabled parking pass would be required to park in this EV charging space.

The federal Americans with Disabilities Act, Revised Code of Arizona, and Arizona Administrative Code identify requirements for location, design, and number of parking spaces for persons with disabilities.

Note that it is important that the placement of the charging station in an existing accessible parking space should allow adequate space (minimum of 36 inches) for a wheelchair to pass the vehicle wheel stop.

D. Safety Issues Related to Indoor Charging

The possibility of invoking the ventilation requirements or hazardous environment requirements of the NEC exists when installing indoor charging. When the EVSE connector makes contact with the EV inlet, the pilot signal from the vehicle will identify whether the battery requires ventilation. While most BEV and PHEV batteries do not require ventilation systems, some batteries, such as lead acid or

zinc air batteries, emit hydrogen gas when charged. Most vehicle manufacturers will identify clearly that their batteries do or do not require ventilation. Without adequate ventilation, the hydrogen gas concentration may increase to an explosive condition. The Lower Flammability Limit of hydrogen in air is a 4% mixture by volume. Locations are hazardous when 25% of that limit is reached, which is a 1% mixture by volume. The EVSE contains controls to turn on the ventilation system when required, and also to stop charging should that ventilation system fail.

Recognizing that hydrogen is lighter than the air mixture, higher concentrations would accumulate near the ceiling. The ventilation system should take this into account by exhausting high and replenishing lower.

Indoor charging also can provide a challenge with respect to lighting, tight access, and storage of other material. Often areas in an enclosed garage can be poorly lighted, and when this is combined with tight access around the vehicle and other equipment stored in and around the vehicle parking stall, the possibility of personal injury from tripping increases.

E. Installations Located in Flood Zones

Permits for constructing facilities, including EV charging stations, include reviews to determine whether the site is located in a flood-prone area. The Code of Federal Regulations, *Title 44 Emergency Management and Assistance, Part 60 Criteria for Land Management and Use*, includes the following requirement:

“If a proposed building site is in a flood-prone area, all new construction and substantial improvements shall (i) be designed (or modified) and adequately anchored to prevent flotation, collapse, or lateral movement of the structure resulting from hydrodynamic and hydrostatic loads, including the effects of buoyancy, (ii) be constructed with materials resistant to flood damage, (iii) be constructed by methods and practices that minimize flood damages, and (iv) be constructed with electrical heating, ventilation, plumbing, and air conditioning equipment and other service facilities that are designed and/or located so as to prevent water from entering or accumulating within the components during conditions of flooding.”⁸

For EVSE components, elevation and component protection are the two primary methods for minimizing flood damage, preventing water from entering or accumulating, and resisting flood damages. These measures are required by the National Flood Insurance Program (NFIP).

The primary protection for EVSE is elevation. *Elevation* refers to the location of a component above the Design Flood Elevation (DFE). All locations approved for EVSE installation should be above the DFE. This may mean that the EVSE is located outside a garage if inside would be below the DFE. This may mean that certain areas of a condominium parking lot would not contain any EVSE if that elevation is not achievable. This may require the installation of EVSE charging stations on the third level of a parking garage rather than the first.

⁸ 44CFR60.3(a)(3)

Component protection refers to the implementation of design techniques that protect a component from flood damage when they are located below the DFE.

Wet flood proofing refers to the elimination or minimization of the potential of flood damage by implementing waterproofing techniques designed to keep floodwaters away from utility equipment. In this case, the rest of the structure may receive damage, but the EVSE is protected by barriers or other methods.

Dry flood proofing refers to the elimination or minimization of the potential for flood damage by implementing a combination of waterproofing features designed to keep floodwaters completely outside of a structure.⁹ If the entire building is protected from flood water, the EVSE is also protected.

F. Point of Sale Options

During the Early Adoption stage of EV ownership, most owners of publicly available charging stations will absorb the cost of the electricity used, since this actual cost is low per use. However, as the public acceptance and ownership of EVs grow, more EV owners will favor having the option for point of sale. In most areas, only electric utilities can actually sell electricity, so a fee for convenience/service probably will be the strategy. Often a credit card transaction fee will well exceed the electricity cost of charging an EV. However, the availability and convenience of charging will be a service the public will desire and purchase. A fee for service can help the EVSE owner recover the costs for equipment, installation, service, and maintenance. Several options for point of sale options are available.

Card Readers

Several types of card readers are available that may be incorporated with the EVSE. Credit/debit card readers would be simple to use and are already widely accepted by the public. The credit/debit card would record a fee each time publicly available charging is accessed and base the fee on the number of times accessed rather than the length of time on charge.

A smartcard is a card that is embedded with a microprocessor or memory chip, so it can securely store more detailed information than a credit/debit card. A smartcard could be sold with a monthly subscription for charger use and be embedded with additional user information. That information could be captured in each transaction and used for data recording, as noted in Section G. The smartcard could be used for a pre-set number of charge opportunities or to bill a credit card number for each use.

Both cases will require a communication system from the reader to a terminal for off-site approval and data recording. Upon approval, power will be supplied to the EVSE. The cost of this system and its integration into the EVSE will be a design consideration.

⁹ FEMA Publication 348, *Principles and Practices for the Design and Construction of Flood Resistant Building Utility Systems*, November 1999.

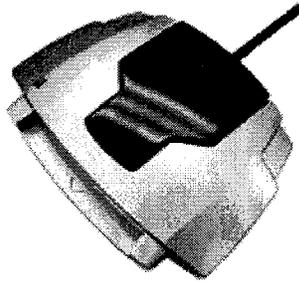


Figure 5-4 Smartcard Reader¹⁰

Parking Area Meters

Drivers are very familiar with the parking meters used in public parking. A simple coin-operated meter is an option for EV parking areas, and can be installed at the head of each EVSE parking stall. Another method in common use at public pay parking lots is to provide a central kiosk for credit card purchases. The parking stall number is identified at the kiosk and a parking receipt issued that can be displayed in the vehicle. There is little cost for the meter, and a single kiosk reduces the point of service cost for the whole parking lot. This system will require an attendant to periodically monitor the area for violations. Penalties for violators will need to be determined. Note that a coin-operated meter also may invite vandalism.

Radio-Frequency Identification (RFID) Subscription Service

Like the smartcard, an RFID fob can be programmed with user information. The RFID reader collects the information from the fob to activate the EVSE station. A monthly subscription for the user keeps the fob active and the monthly fee can be based upon number of actual uses or a set fee. The reader is programmed for the accepted RFID.

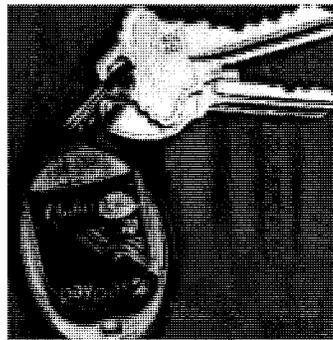


Figure 5-5 RFID Fob¹¹

¹⁰ ACR-38 Smart Card Reader by Advanced Card Systems.

¹¹ Texas Instruments RFID.

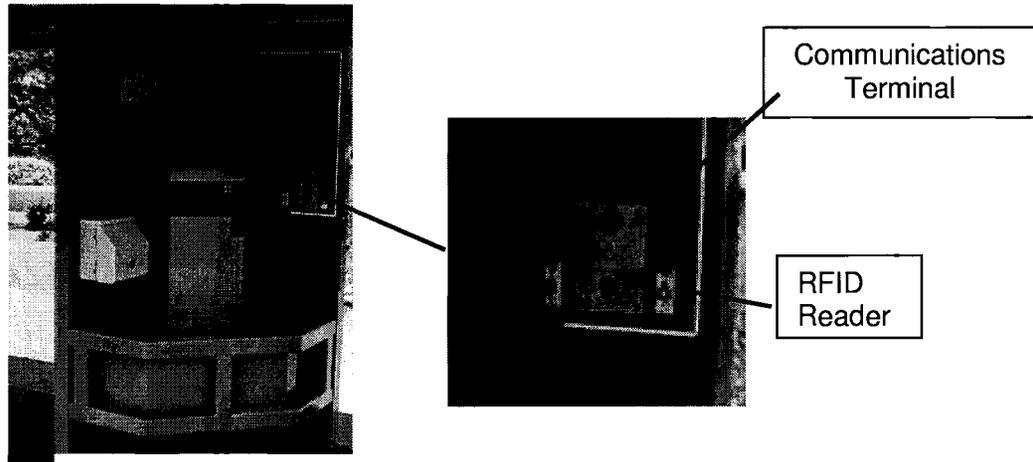


Figure 5-6 RFID Reader and Communications Terminal

G. Data Collection

More than simply recording payment for service, the use of a smartcard or RFID can substantially increase the amount of information available at each publicly available charging station. Data collection systems can track usage at each of the stations and provide feedback on actual EV usage. It may be found that usage at some venues is lighter than expected, whereas others may have heavier use. This information could be helpful in expanding publicly available charging locations. In addition, data on the time of day usage may show peak usage at unexpected times, which may impact power utilization. Some EVSE may include features that allow a wide range of data to be collected.

H. Vandalism

Publicly available charging carries the possibility of vandalism and theft. Destruction of property through purposeful defacing of equipment is a possibility; however, such destruction actually proved to be very minor during EV usage in the mid-1990s. Still, as public acceptance and the number of publicly available charging sites continue to grow, steps should be taken to minimize the possibility of vandalism.

Most EVSE can be constructed of materials that will clean easily and can have graffiti removed. Careful planning for site locations to include sufficient lighting and equipment protection will discourage damage and theft. Motion sensor activated lighting may benefit users and deter abusers. EVSE with cable retractors or locking compartments for the EVSE cord and connector may be designed. Placing the EVSE in security-patrolled areas or within sight of manned centers will discourage vandalism.

EVSE owners in condominiums and apartments may wish to protect the equipment with a lockable, secure cabinet to prevent unauthorized use and for protection from vandalism.

I. Station Ownership

Ownership of the individual charging station may not be entirely clear. A business owner may wish to host publicly available charging, but may not have the legal right to the parking lot or for making improvements. Charging stations constructed with public grants or other financing may have split ownership - one entity may own the charger and another may own the infrastructure. The sale of a business may include the EVSE or the sale of the property may include both. EVSE may be rented or leased equipment. Before planning any installation, it is important to identify the entities that have legal rights with respect to the equipment and its installation. Whose approvals are required to obtain the permits and whose approvals are required to remove the equipment later?

For individual EV owners, the ownership of the EVSE should reside with the owner. The ownership of the installation should reside with the property owner. However, both may share legal responsibilities and liabilities for the equipment and both should be protected by insurance.

For publicly available charging, there may be a combination of owners. Utilities may wish to own and manage the public charging infrastructure in order to manage power requirements. In a successful EV market penetration, ownership of new public charging may shift to private ownership. Several businesses may join together to promote EV usage and may share in the EVSE ownership. However, there should be *one* individual business entity tasked with the responsibility of ownership, along with the proper contact information to be shared with the local utility.

J. Maintenance

The EVSE typically will not require routine maintenance. However, all usable parts can wear, and periodic inspections should be conducted to ensure that all parts remain in good working order. Periodic cleaning may be required, depending on local conditions. Testing of communications systems and lighting should be conducted periodically. Repair of accidental damage or purposeful vandalism also may be required. Unless otherwise agreed, these responsibilities generally fall to the owner identified in Section I above.

6. Codes and Standards

During the initial introduction of EVs in the early 1990s, stakeholders representing the automotive companies, electric utilities, component suppliers, electric vehicle enthusiasts, equipment manufacturers, and standards and national testing organizations worked to obtain a consensus on methods and requirements for EV charging. This resulted in revisions to building codes, electric codes, first responder training, and general site design and acceptance documentation. These requirements are designed to protect the public and make EVSE accessible for use.

Equipment is designed to EVSE standards set by organizations, such as the Society of Automotive Engineers, and is tested through nationally-recognized testing laboratories, such as Underwriters Laboratories. This testing certifies that the equipment is suitable for its designed purpose. The equipment installation is required to follow the rules of the National Electric Code and Building Codes. Both of these codes can be augmented by state or local governing bodies. Frequently, the codes also affect the standards provided - as is the case for Electric Vehicles.

Nothing within these Guidelines should be construed to allow any detail of the EV charging installations to deviate from the adopted building codes and planning ordinances of each jurisdiction in which they are installed. Our intent is to develop standard plans for each jurisdiction and to have those plans approved prior to requesting permits or inspection approvals from that jurisdiction. We understand that those standard plans may vary slightly from jurisdiction to jurisdiction based on their specific adopted building codes and planning ordinances.

In order to protect the public health and conform to safety regulations, regulatory agencies are responsible for monitoring the installation process to ensure that the proper codes and standards are being implemented.

A. Regulatory Agencies

The federal government, as well as state, county, and city governments, each have model building codes established that provide minimum requirements for safe construction and installation processes.

The City of Phoenix, for example, currently recognizes, among others, the International Building Code and Arizona Revised Statutes. These model codes, as well as national codes such as the National Electric Code, are updated on a regular basis, based on industry performance and technical advances.

B. National Electric Code

The National Electric Code (NEC) is part of the National Fire Code series established by the National Fire Protection Association (NFPA) as NFPA 70. The NEC codifies the requirements for safe electrical installations into a single, standardized source. This code is adopted by state and local jurisdictions and may be augmented by those jurisdictions to be applied as the local practice. When identifying the electrical requirements for EVSE installation, it is important to work with the local jurisdiction to identify any local requirements in addition to the national code standard. The NEC is updated every three years. Although the current published, adopted edition is 2008,

not all jurisdictions have approved this edition, and care should be taken to follow the electrical code currently in place for each jurisdiction. Section 625 of the NEC specifically addresses electric vehicles.

C. SAE and UL

Currently, the Society of Automotive Engineers (SAE) has determined that there will be a single conductive coupler design. The J1772 “SAE Electric Vehicle Conductive Charge Coupler” is the standard that is being used by automotive suppliers in the United States. While J1773, the Inductive Charge Coupler, is still active, none of the automakers are using this method.

Applicable SAE Standards include:

- SAE J1772
- SAE J2293
- SAE J2847
- SAE J2836
- SAE J2894
- SAE J551

SAE J2293 establishes requirements for EVs and the off-board EVSE used to transfer electrical energy to an EV from a utility source. This document defines, either directly or by reference, all characteristics of the total EV Energy Transfer System (EV-ETS) necessary to ensure the functional interoperability of an EV and EVSE of the same physical system architecture. The ETS, regardless of architecture, is responsible for the conversion of AC electrical energy into DC electrical energy that can be used to charge an EV’s storage battery. J2847 provides specifics on digital communications; J2836 provides a case for the use of digital communications between vehicle and EVSE; J2894 addresses on-board charger power quality; and J551 provides standards for electromagnetic compatibility.

Underwriters Laboratories (UL) provides testing and certification that equipment complies with relevant standards, especially in areas involving public safety. The following UL standards form a basis for certifying EVSE.

- UL 2202 *Electric Vehicle (EV) Charging System Equipment*
- UL 2231-1 *Personnel Protection Systems for Electric Vehicle (EV) Supply Circuits: General Requirements*
- UL 2231-2 *Personnel Protection Systems for Electric Vehicle (EV) Supply Circuits: Particular Requirements for Protection Devices for Use in Charging Systems*
- UL 2251 *Plugs, Receptacles, and Couplers for Electric Vehicles*

Equipment that successfully completes the testing is “certified”, “approved”, or “listed” as meeting the standard. In general, the SAE and UL requirements are more restrictive and are expected to be incorporated in harmonized standards.

D. Occupational Safety and Health

Under the Occupational Safety and Health Act (OSHA) of 1970, OSHA's role is to assure safe and healthful working conditions for working men and women by authorizing enforcement of the standards developed under the Act; assisting and encouraging the states in their efforts to assure safe and healthful working conditions; and providing for research, information, education, and training in the field of occupational safety and health.¹²

The Arizona State Department of Labor and Industries publishes the *Safety and Health Core Rules*, which are the basic safety and health rules required by most employers in the state of Arizona.

E. Engineering, Permitting & Construction

The process flowcharts shown in Figure 4-4, 4-7, 4-9, and 4-15 all require permitting of the work. A typical permit application includes the name of the owner or agent; the physical address where the work will be conducted; the property's parcel number; the voltage and amperage of the system; the name, address, and license number of the qualified contractor performing the installation; whether additional trades will be involved; and other information required in that jurisdiction.

Service load calculations may be required. The electrical contractor will review the existing current service loading and consider the rating of the EVSE unit(s) to be installed. The contractor then will develop a new loading calculation to determine whether the existing service panel is adequate or new service will be required.

It is recommended that local methods be considered to streamline the permitting process for residential EVSE installations. For BEV purchasers, the Level 1 Cord Set provided with the vehicle will require a significant charge period, so in general, an EV owner will prefer a Level 2 EVSE. Keeping the time span from EV purchase to fully functional and inspected EVSE installation as short as possible will be important for customer satisfaction.

Installation drawing requirements may vary by jurisdiction, ranging from layouts for residential installations to a full set of plans for public charging. In general, an electrical contractor from eTec's approved certified contractor network can complete the requirements for residential garage circuits.

For fleet and public charging, an engineering company is recommended to prepare the detailed site plans for installation. Several trades may be involved, including general contracting, electrical, landscaping, paving, concrete, masonry, and communications systems. As noted above, careful planning is required to coordinate this effort, and an engineering company can provide the detailed set of drawings that will be required. In addition, there may be several permitting offices involved with the approval of these plans.

¹² OSHA website www.osha.gov

7. Utility Integration

A. Background

Electric utilities are under significant pressure to maintain a dependable, clean, low-cost electrical supply to their customer base. In order to achieve these goals, utilities are evaluating, and in some cases implementing, Smart-Grid technologies that allow utilities to control various electrical loads on their systems. Through these Smart-Grid technologies, utilities can minimize new power plant and electrical distribution and transmission investment by shifting and controlling load while minimizing the impact to the customer.

Advanced Metering Infrastructure (AMI) or Smart Meters are being deployed by utilities to provide remote meter reading. Smart Meters also have the ability to control various customer loads.

Electric vehicles are one of the better loads to control for the utilities through Smart Meters, because EVs have an on-board storage system, which means that delaying the charge of the battery has no noticeable impact on the customer, unlike turning off a lighting or air-conditioning load, which can have an immediate impact on the customer. Additionally, a neighborhood transformer may not be sized such that every EV-owning customer in an area can be charging at the same time. The ability to schedule the EV charging systems connected to a neighborhood transformer could significantly extend the life of that transformer, or delay or eliminate the need to replace the transformer with a larger size.

As the adoption of EVs increases, load control strategies for multi-family dwellings may allow the utility to control charge times to maximize the effectiveness and utilization of existing transformers.

During residential EVSE installations, the electrical contractor will evaluate the electrical service capabilities of the existing system. If inadequate power is available at the service entrance, an additional service panel or other upgrade may be required.

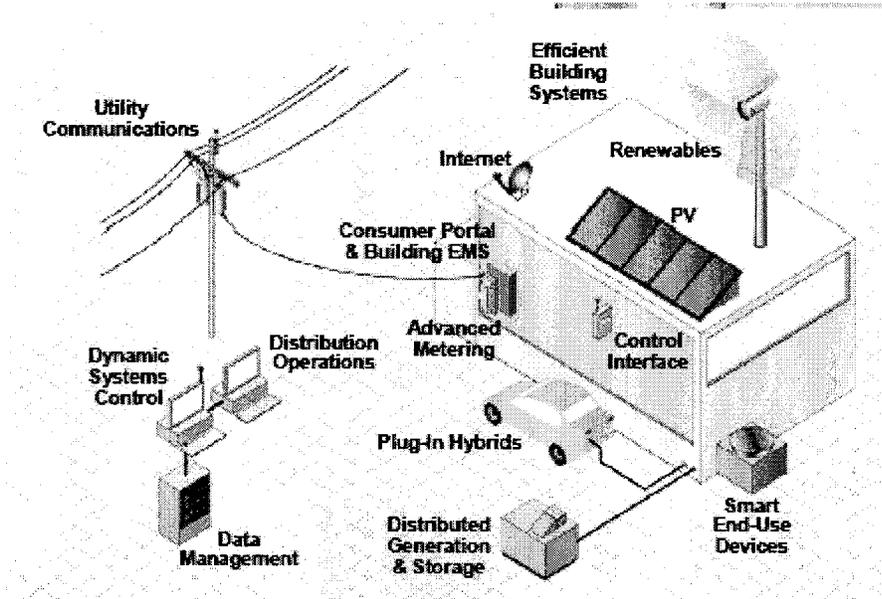


Figure 7-1 Smart Grid Infrastructure¹³

Figure 7-1 incorporates many design features of a Smart Grid/distributed energy storage system. Home use of photovoltaic or wind energy can supplement the utility power. A home area network (HAN) communicating with the Advanced Meter can control lighting, heating, cooling, and other major appliances. Given the right incentives, a home owner may elect to have the utility control total home consumption or delivery power back to the utility through the storage capability of the EV.

There are various mechanisms for utilities to control EV load including:

Time-of-Use (TOU)

TOU is an incentive-based electrical rate that allows the EV owner to save money by charging during a designated “off-peak” time frame established by the utility. Typically, these off-peak times are in the late evenings through early mornings and/or weekends, during a timeframe when demand on the utility electrical grid is at its lowest point. TOU is now being implemented by some utilities, but currently there is not a common approach. Discussion with the local utility prior to installation of the charge station is recommended.

¹³ Transportation Systems (EVP) in Electric - Successful Integration of Plug, EPRI, Plug-In 2009 Canada, September 2009.

Dual Metering

Some utilities will provide a special rate for EV charging and will require the installation of a second meter specifically for this purpose. This will require additional installation time, since the utility must install the meter before the EVSE is available for use. The use of a “revenue-grade” meter in the EVSE and a communications path to allow the utility control may obviate the need for the second meter.

Demand Response

Demand response is a voluntary program that allows a utility to send out a signal to customers (typically large commercial customers) to cut back on loads during times the utility is experiencing a high peak on their utility grid. These customers are compensated when they participate in this program. As deployment of Smart Meters becomes more prevalent, EV owners may participate in such programs. Utilities may enter into contracts with EV owners to allow the utility to maintain more control over EV charging.

Real-Time Pricing (RTP)

RTP is a concept that could be implemented in the future for EVs. In this model, pricing signals are sent to a customer through a number of communication mediums that allow the customer to charge their EV during the most cost-effective period. For example, the EVSE installed in the EV owner’s garage could be pre-programmed to ensure the car is fully charged by 6:00 am, at the lowest cost possible. RTP signals from the utility would allow this to occur without customer intervention. In order to implement RTP, smart meters would need to be in place at the charging location and the technology built in to the EVSE. These programs are under development at the time of this writing.

Vehicle-to-Grid (V2G)

V2G is a concept that allows the energy storage in electric vehicles to be used to support the electrical grid during peak electrical loads, in times of emergency such as grid voltage support, or based on pricing economics. V2G could also support vehicle-to-home, where the energy stored in the vehicle battery could supplement the home’s electrical requirements. V2G requires that the on-board vehicle charger be bi-directional (energy is able to flow both in and out of the system). The EVSE at the premises must also be bi-directional and able to accommodate all of the utility requirements related to flowing energy back into the electrical grid. Although there are various development efforts in V2G, for on-road EVs, this concept probably is several years away from implementation in any commercial sense.

B. Interconnection Requirements

Although vehicle-to-grid (V2G) connections may be in the future for most applications, some infrastructure will incorporate EVSE with solar parking structures or other renewable resources. Because these systems will connect to the local grid, it will be necessary to contact the local utility to determine whether there are any interconnection requirements. These requirements are in place to protect personnel and property while feeding electricity back into the utility grid. Most utility requirements typically are already in place for solar photovoltaic and wind systems that are grid-tied to the utility.

C. Commercial Electrical Supply/Metering

There are typically two scenarios for connection to a commercial electrical supply. The first is utilizing the existing main service entrance section (SES) or an otherwise adequate supply panel at the commercial establishment, and the second is to obtain a new service drop from the local electric utility.

The decision on which approach to take depends on a number of factors, including the ability to obtain permission from the property owner and/or tenant of the commercial business, and the location of the existing SES or adequate electrical supply from the proposed electric vehicle charge station site. If permission is granted by the property owner and/or tenant (as required), then a fairly simple analysis can be performed to compare the cost of utilizing an existing supply vs. a new service drop to determine the best approach.

A new utility service drop typically requires the establishment of a new customer account, which may include a credit evaluation of the entity applying for the meter, and a monthly meter charge in addition to the energy and demand charges. The local utility also may require an analysis of the anticipated energy consumption.

MARICOPA ASSOCIATION OF GOVERNMENTS

INFORMATION SUMMARY... for your review

DATE:

June 23, 2010

SUBJECT:

FY 2011 MAG Regional Plan to End Domestic Violence

SUMMARY:

During FY 2010, the MAG Regional Domestic Violence Council, together with more than 150 stakeholders, developed the new Regional Plan to End Domestic Violence in response to the changing dynamics created by the recession. This is not the first plan to address domestic violence using a regional approach. The first MAG Regional Plan to End Domestic Violence was approved by the MAG Regional Council in 1999. The new plan includes fifteen strategies in the areas of funding, training and education, coordination and collaboration, and services developed to maximize impact with limited resources in today's community.

PUBLIC INPUT:

The Regional Plan to End Domestic Violence was developed on the basis of broad community input of more than 150 stakeholders beginning October 2009 through May 2010. The community input included elected officials, service providers, municipal staff and the private sector including survivors of domestic violence. Opportunities for public input on the Regional Plan were provided at the May 6, 2010, Regional Domestic Violence Council meeting and the June 9, 2010, MAG Management Committee, but no public input was given.

PROS & CONS:

PROS: The Plan promotes a coordinated community response to domestic violence, maximizes impact with limited resources, helps to avoid duplication of efforts, and engages the community in efforts to maintain or enhance funding for domestic violence programs.

CONS: There are no anticipated cons from developing the report.

TECHNICAL & POLICY IMPLICATIONS:

TECHNICAL: The plan was developed using a strength-based approach. The techniques of Appreciative Inquiry (AI) and Open Space Technology were utilized to engage community partners in identifying strengths and how best to capitalize on prior successes to continue making the most impact. Nearly 90 AI interviews were completed with community stakeholders as well as with survivors of domestic violence. Responses to the interviews were analyzed for trends, which provided a starting point for discussion at the MAG Regional Plan to End Domestic Violence Summit in February 2010. Nearly 100 community stakeholders, including those who participated in the AI interview process, helped to develop strategies during the Summit. The Regional Plan to End Domestic Violence will be implemented in collaboration with community partners with regular progress reports provided to the MAG Regional Domestic Violence Council.

POLICY: The MAG Regional Domestic Violence Council created the next Regional Plan to End Domestic Violence to ensure continuation of a coordinated community response to domestic violence. The new plan takes into account advances in addressing this issue as well as the changing dynamics caused by the recession. The Regional Plan to End Domestic Violence provides a roadmap for maximizing impact with limited resources through increased coordination and collaboration throughout the region.

ACTION NEEDED:

Approval of the FY 2011 MAG Regional Plan to End Domestic Violence.

PRIOR COMMITTEE ACTIONS:

MAG Management Committee: The committee recommended the Regional Plan for approval at the June 9, 2010, meeting:

MEMBERS ATTENDING

- Mark Pentz, Chandler, Chair
- Carl Swenson, Peoria, Vice Chair
- # Matt Busby for George Hoffman, Apache Junction
- Charlie McClendon, Avondale
- Stephen Cleveland, Buckeye
- Gary Neiss, Carefree
- * Usama Abujbarah, Cave Creek
- Spencer Isom for B.J. Cornwall, El Mirage
- * Phil Dorchester, Fort McDowell Yavapai Nation
- Rick Davis, Fountain Hills
- Rick Buss, Gila Bend
- * David White, Gila River Indian Community
- Michelle Gramley for Collin DeWitt, Gilbert
- Brent Stoddard for Ed Beasley, Glendale
- # Mark Gaillard for John Fischbach, Goodyear
- Bill Hernandez, Guadalupe

- Darryl Crossman, Litchfield Park
- Christopher Brady, Mesa
- Jim Bacon, Paradise Valley
- David Cavazos, Phoenix
- # John Kross, Queen Creek
- * Bryan Meyers, Salt River Pima-Maricopa Indian Community
- * Dave Richert, Scottsdale
- # Michael Celaya for Mark Coronado, Surprise
- Charlie Meyer, Tempe
- * Reyes Medrano, Tolleson
- Gary Edwards, Wickenburg
- Lloyce Robinson, Youngtown
- Robert Samour for John Halikowski, ADOT
- Mike Sabatini for David Smith, Maricopa County
- Bryan Jungwirth for David Boggs, Valley Metro/RPTA

* Those members neither present nor represented by proxy.

Participated by telephone conference call. + Participated by videoconference call.

MAG Regional Domestic Violence Council: The committee recommended the Regional Plan for approval at the May 6, 2010, meeting.

MEMBERS ATTENDING:

- * Cmdr. Kim Humphrey, Phoenix Police Dept., Chair
- Celeste Adams, Save the Family
- Christina Avila, Avondale
- Lt. Robert Bates, Phoenix Police Dept.
- Kristen Scharlau for Kathy Berzins, Tempe
- Larry Grubbs for John A. Blackburn, Jr., AZ Criminal Justice Commission
- Allie Bones, Arizona Coalition Against Domestic Violence
- Rick Buss, Gila Bend
- * Chris Christy, Salt River Pima-Maricopa Indian Community
- JoAnn Del-Colle, Phoenix Family Advocacy Center
- Councilmember Roy Delgado, El Mirage
- * President Diane Enos, Salt River Pima-Maricopa Indian Community
- Will Gonzalez, Phoenix Prosecutor's Office
- Laura Guild, Arizona Dept. of Economic Security

- * Dan Hughes, City of Surprise
- Lynette Jelinek, Glendale Fire Dept.
- Alice Ghareib for Mary Lynn Kasunic, Area Agency on Aging
- * Patricia Klahr, Chrysalis Shelter Inc.
- Councilmember Suzanne Klapp, Scottsdale
- * Councilmember Sheri Lauritano, Goodyear
- Councilmember Phil Lieberman, Glendale
- * Jodi Beckley Liggett, Arizona Foundation for Women
- Barbara Marshall, Maricopa County Attorney's Office, Vice Chair
- Jeremy Arp for Maria-Elena Ochoa, Governor's Office for Children, Youth, and Families
- Dottie O'Connell, Chicanos Por la Causa
- Connie Phillips, Sojourner Center
- Lynn Potts for John Pombier, Mesa
- * Kerry Ramella, Phoenix Fire Dept.
- Sarah Youngblood, Community Legal Services

*Those members neither present nor represented by proxy.

CONTACT PERSON:

Renae Tenney, Human Services Planner, 602-254-6300



Regional Plan to End Domestic Violence

Regional Domestic Violence Council
May 6, 2010





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Executive Summary

The Maricopa Association of Governments (MAG) Regional Domestic Violence Council leads the coordinated community response for ending domestic violence. Established in 1999, the Council strives to reduce the incidence of and trauma associated with domestic violence. The Council was formed to implement recommendations laid out in the first regional domestic violence plan. At the time, the MAG region was one of only six regions to utilize the strength of a regional approach to addressing domestic violence. The work of the Council continues today with the guidance of a new regional plan. The *MAG Regional Plan to End Domestic Violence* will impact the region by increasing survivor safety, holding abusers accountable, and trimming costs through streamlined processes.

Thanks to Saint Luke's Health Initiatives, the new regional plan was developed through the guidance of domestic violence survivors, advocates, and community partners. Participation



Domestic Violence Council Chairman Commander Kim Humphrey (left foreground) attends the Regional Plan to End Domestic Violence 2010 Summit.

in the strength-based planning process brought together many devoted to making changes for survivors of domestic violence. Their diverse perspectives and innovative ideas are reflected in the plan's strategies. The outcome of the plan will be addressing sustainability of funding,

raising awareness and education, enhancing multi-disciplinary coordination, and strengthening long-term supports.

The plan is built on four areas of focus:

- Funding
- Training and Education
- Coordination and Collaboration
- Services



Fifteen strategies provide the framework:

1. Communicate the need for sustainable funding for existing programs and services.
2. Develop avenues for raising awareness and educating the public.
3. Increase social capital through grassroots efforts focused on the prevention of domestic violence.
4. Develop standardized, multi-disciplinary curriculum for providing domestic violence education to criminal justice system and first responders.
5. Develop multi-disciplinary training for victims about the criminal justice process, law enforcement procedures, and realistic expectations of these systems.
6. Develop cross-training between law enforcement, criminal justice system and advocates.
7. Increase coordination and collaboration between shelters and services.
8. Increase access to information on available resources.
9. Coordinate multi-disciplinary effort for reviewing standard protocols and practices for responding to domestic violence.
10. Connect all critical resources for people experiencing domestic violence and homelessness through a coordinated community response.
11. Create an ideal model for culturally competent prevention and intervention services.
12. Enhance the process for appropriately meeting survivors' housing needs.
13. Develop support groups for teens who have experienced or witnessed domestic violence.
14. Develop more transportation options for those residing in shelter.
15. Create long-term supports for helping survivors maintain their safety.

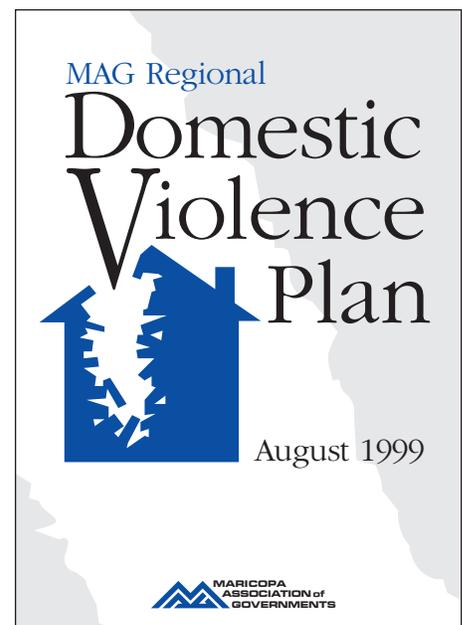
Forward movement is vital to the success of the *MAG Regional Plan to End Domestic Violence*. The Council is indebted to community partners that champion this work. Progress will be monitored on a quarterly basis through reports to the MAG Regional Domestic Violence Council.

An annual report will show the impacts achieved by implementing the plan's strategies. The new *MAG Regional Plan to End Domestic Violence* will prove to enhance processes for increasing victim safety, holding abusers accountable, and

leveraging limited resources to make the most impact in ending domestic violence throughout the region.

To become involved in these efforts, please contact Renae Tenney at rtenney@mag.maricopa.gov or by calling (602) 254-6300.

All materials may be accessed at www.mag.maricopa.gov.



The MAG Domestic Violence Council was formed to implement recommendations laid out in the first regional domestic violence plan which was developed in 1999.



Introduction

The Maricopa Association of Governments (MAG) *Regional Plan to End Domestic Violence* provides a blueprint for continuing to make a difference in the lives of individuals and families seeking to escape violence. The landscape of domestic violence has changed dramatically over the past 10 years, especially in the wake of the recession. Funding for domestic violence providers has been drastically reduced while case managers are reporting their clients are experiencing an increase in intensity and complexity of abuse. Job loss and home foreclosures are increasing the severity of financial strains experienced by many families throughout the region. This new environment provides plenty of uncertainty, but it also presents a unique opportunity to reassess how to work better together as a region to address domestic violence.

The MAG Regional Domestic Violence Council was developed to reduce the incidence of and trauma associated with domestic violence in the MAG region. The



This three-dimensional timeline highlights ten years of accomplishments achieved through a coordinated community response to domestic violence.

Council coordinates initiatives with community partners and municipalities in accordance with the regional plan. Survivors of domestic violence, advocates, and community partners guide the development of the plan, and ultimately, the Council's work. This document serves as the next regional plan to end domestic violence. The outcome of the plan will be addressing sustainability of funding, raising awareness and education, enhancing multi-disciplinary

coordination, and strengthening long-term supports to increase survivor safety, hold abusers accountable, and trim costs through streamlined processes. The Council's work has positively impacted the region's ability to respond to domestic violence.

Additional shelter beds are now available to help meet the demand for safety. Employers are more active in addressing domestic violence in the workplace. Teens are connected to

an online resource about dating violence. First responders and healthcare professionals receive training about identifying potential domestic violence victims. Efforts are underway to increase survivors' access to legal help. Countless lives have been saved by the work of the MAG Regional Domestic Violence Council in partnership with dedicated community partners.

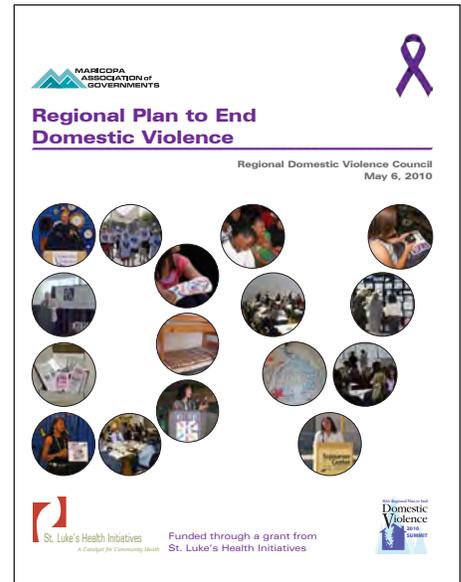
The *MAG Regional Plan to End Domestic Violence* provides a roadmap for continuing an impactful regional response to domestic violence. First, an overview of the Council's background and key accomplishments are provided. Next, the plan describes the strength-based planning process utilizing *Appreciative Inquiry* interviews and facilitation techniques of World Café and Open Space Technology.

Fifteen strategies were developed around the need to embrace the changing environment and discover creative ways to leverage severely limited resources. These strategies address the following areas:

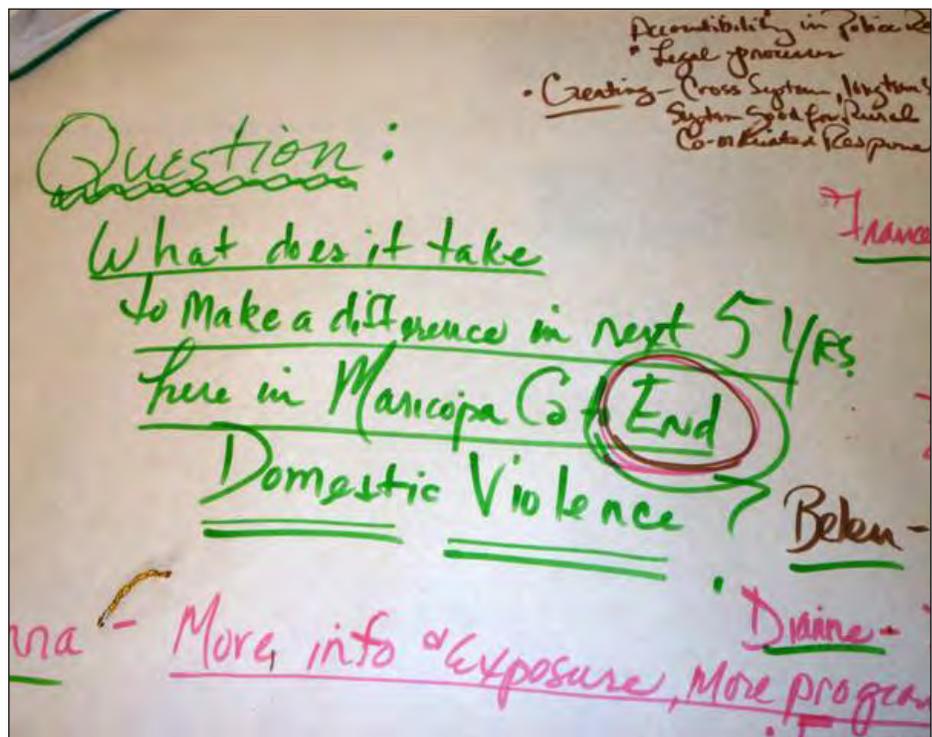
- Communicate funding concerns;
- Develop multi-disciplinary trainings for law enforcement, prosecution, and victim advocates;
- Assess protocols for arresting and prosecuting domestic violence offenders; and

- Develop additional long-term supports for survivors.

Many dedicated community partners participated in the plan development process. More than 150 individuals took part in *Appreciative Inquiry* interviews and attended the planning summit. Their innovative ideas for continuing meaningful work are captured in the plan's strategies and action plans. The strength of this plan lies in the diverse perspectives of those who participated in this process. The *MAG Regional Plan to End Domestic Violence* represents the collaborative work of many advocates, professionals, and survivors devoted to ending domestic violence in the region.



This new plan will provide a roadmap for continuing a regional response to domestic violence.



Collaborative brainstorming techniques were used during the summit to find ways to end domestic violence in the region.



Domestic Violence 101

Domestic violence is a pattern of behavior that includes the use or threat of violence and intimidation for the purpose of gaining power and control over another person. Such violence is characterized by physical abuse, sexual abuse, economic abuse, isolation, emotional abuse, control, or verbal abuse. Legally, domestic violence is defined through 19 other crimes such as assault, homicide, and harassment. In Arizona, domestic violence was recognized legally only between adults within specific relationship parameters until recent legislative action. In July 2009, the state's definition of domestic violence was expanded to recognize dating violence amongst adults and teens.

Research vividly documents the negative impact domestic violence has on the health of an individual, as well as the community. Abused teens are more likely to experience lower self-esteem; be abused as an adult; and have higher rates of teen pregnancy, substance abuse, depression, eating disorders and delinquency. Adult

survivors of domestic violence are more likely to experience behavioral health issues, struggle with substance abuse, and suffer from physical injuries that may or may not be treated. The health of the one being abused suffers gravely, as well as witnesses to the abuse, such as children.

Domestic violence is a serious health condition that has a devastating impact on individuals, families, and communities. Every year, domestic violence takes away a significant number of lives and lost opportunities. Local research indicators point to the vast prevalence of domestic violence in this region. Respondents to a survey commissioned by MAG in 2005 revealed that 40 percent of adults personally knew someone who had been involved with domestic violence. Teens echo this sentiment consistently in focus groups conducted by MAG. Since 2006, more than 300 teens have participated in focus groups. On average, at least 50 percent reported they knew someone who had been a victim of teen dating violence. In some groups, 90 per-

cent had such an experience. Lives can be saved through regional planning and coordination.

The downturn of the economy is intensifying domestic violence. More victims are remaining with their abusers longer due to the uncertainty of their jobs. Shelters and service providers are struggling to maintain appropriate service levels with unstable or reduced funding. In March 2010, state budget reductions included \$1.6 million in cuts to domestic violence services. An estimated 1,600 victims are projected to be turned away from shelter due to a lack of funding. Regional planning dollars supporting the MAG Regional Domestic Violence Council have been eliminated. These changes emphasize the importance of working better together to make the most impact with limited resources.

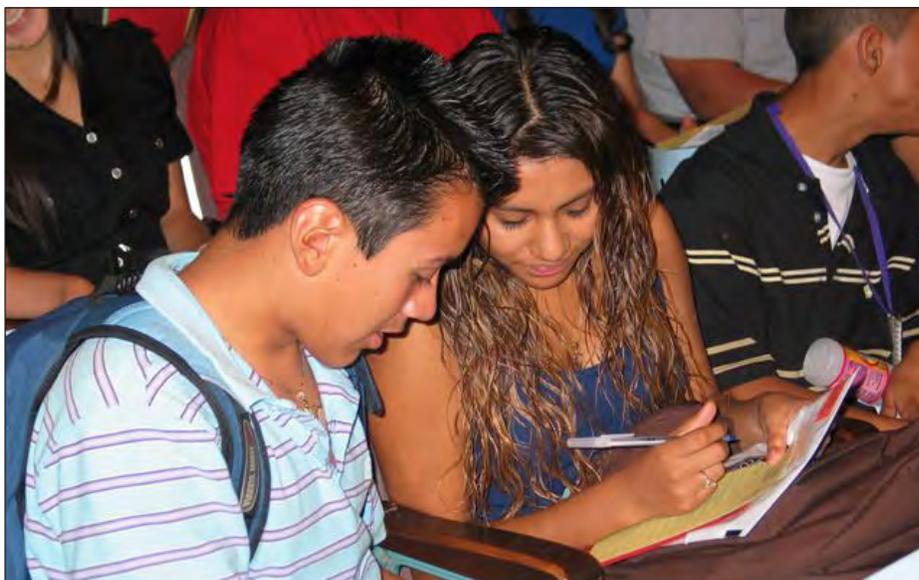




Background

Eleven years ago, the region awoke to the brutal reality of domestic violence when a mother was killed by her husband in front of their children. She had tried to escape, but returned home when none of the shelters she called had any room. Her story shocked the region into action. In 1999, the community came together to develop the *MAG Regional Domestic Violence Plan*, the region's first regional plan to address domestic violence. The plan was a major component in mobilizing the community around the issue of domestic violence.

Within the year, the MAG Regional Domestic Violence Council was formed to coordinate implementation of the plan's recommendations. The Council engaged the community in collaborative projects in prevention, early intervention, crisis intervention, systems coordination and evaluation, and long-term responses to domestic violence. The results included trainings for health care professionals, online resources for teens experiencing



Teens participating in the 2008 "Date Smart" Event at North High School discuss potential warning signs in dating relationships that could lead to abuse.

dating violence, and the *Walk to End Domestic Violence*. This coordinated community response profoundly and positively impacted the region while earning national acclaim as a best practice model.

The *MAG Regional Domestic Violence Plan* successfully guided initiatives to address domestic violence in the region for 10 years. These initiatives, and the recent downturn of the economy, have changed the landscape of

domestic violence in the region dramatically. In 2009, the MAG Regional Domestic Violence Council decided to refocus its efforts through development of a new regional plan. The Council spent the next year engaging the community in a strength-based process looking at what had been done well, and what successes could be capitalized upon to continue making a difference. The results of this process are captured in the new *MAG Regional Plan to End Domestic Violence*.



Highlights of Accomplishments

In 2009, the MAG Regional Domestic Violence Council celebrated ten years of progress toward ending domestic violence in the region. Great strides have been made in providing safety to those deciding to leave a violent relationship.

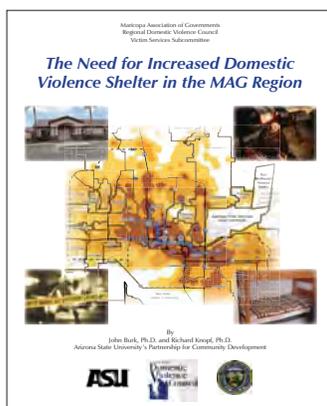
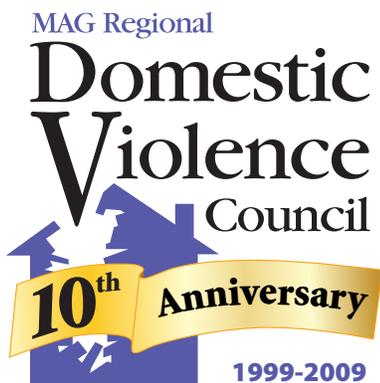
From increasing shelter beds, to implementing workplace policies, to educating teens about healthy dating relationships, the Council has established the groundwork for intervening and preventing domestic violence in the region. A timeline of the Council's projects throughout the last ten years is provided as *Appendix A*. The following are highlights from the timeline of accomplishments.

Increased Domestic Violence Shelter Beds

The MAG Regional Domestic Violence Council conducted a study in 2006 on the need for additional domestic violence shelter beds in the region. *The Need for Increased Domestic Violence Shelter in the MAG Region* report was developed in partnership with Arizona State University's Partnership for Community Development and the Governor's Office Division for Women. The report offered statistically conclusive evidence that at least 325 more beds were needed to meet the existing demand for shelter. The study's findings led to the addition of 319 shelter beds, which ultimately decreased the turn away rate of families seeking shelter by more than 40 percent.

Empowered Employers to Respond to Domestic Violence

In 2000, business leaders on the MAG Regional Domestic Violence Council established Employers Against Domestic Violence (EADV) to form a united response to domestic violence in the work place. This group conducted trainings as well as biannual forums on the importance of corporate policies to help victims and maintain workplace safety. EADV partnered with the Men's Anti-Violence Network to develop domestic violence awareness packets for distribution to hundreds of employers, and collaborated with The Arizona Republic for the first *Annual Walk to End Domestic Violence*,





Many MAG staff members and their families participated in the first Walk to End Domestic Violence event in 2003.

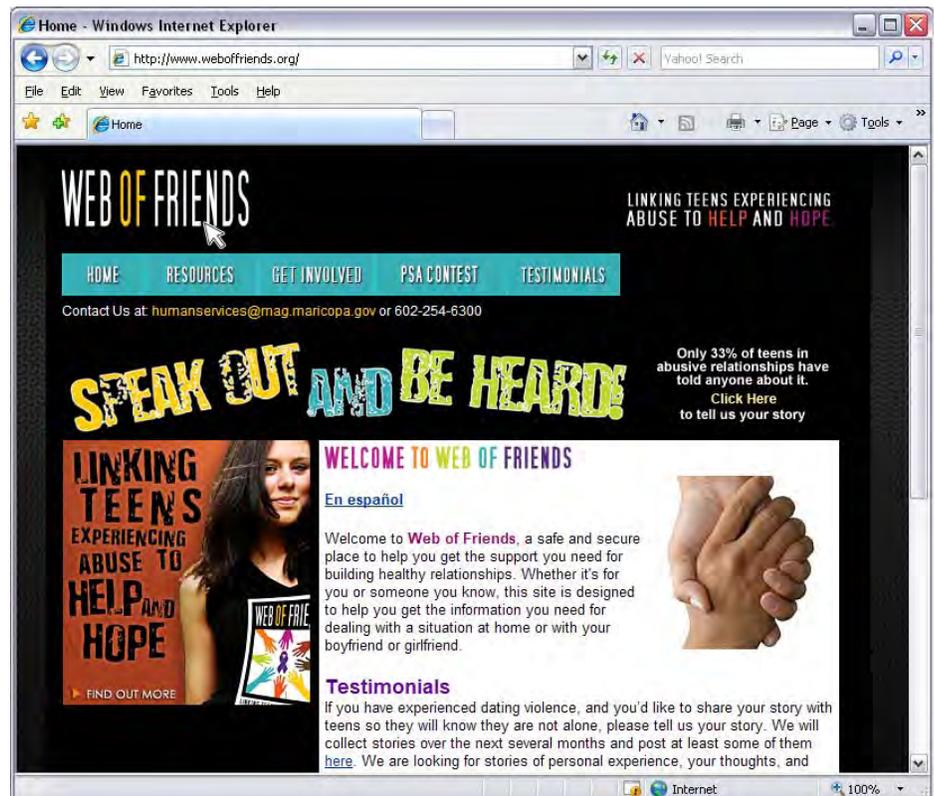
the most successful first-time walk in Arizona history. More than 3,000 people participated in the first year of the walk raising more than \$130,000 to benefit nine domestic violence service providers.

Connected Teens to Safety

The MAG Youth Empowerment Project was developed to put resources about dating violence directly in the hands of teens. In 2006, the project was created after feedback from teens indicated they didn't feel safe in their relationships. More than 50 percent of the teens surveyed indicated they, or someone they

knew, had been involved in an abusive dating relationship. They

said the best way to reach teens with information about this issue was to talk about it and create an anonymous, on-line resource. Hundreds of teens throughout the region were engaged in discussions about the importance of building healthy dating relationships, and involved in developing a series of public service announcements. Teens' feedback also led to the creation of www.WebofFriends.org, a Web site specifically for teens. In 2009, visits to the project's Web site increased by 158 percent! Teens have contacted the site from as far away as Georgia, Norway, and India. The MAG Youth Empowerment Project continues to help teens experiencing abusive relationships throughout the nation and across the globe.



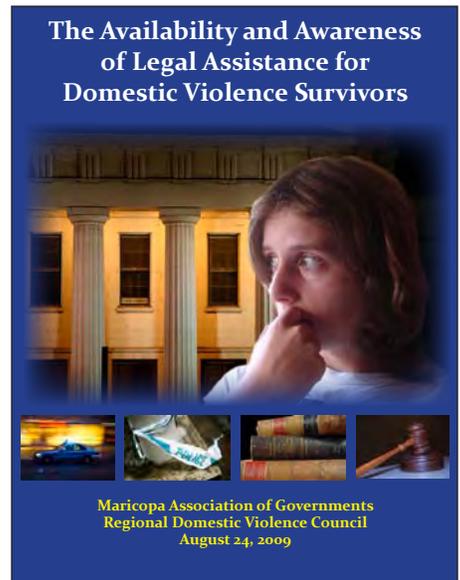
Trained Healthcare Professionals to Screen for Domestic Violence

The MAG Regional Domestic Violence Council has partnered with the healthcare community to help victims to access assistance. In 2001, the Council collaborated with more than 20 municipalities and community organizations to provide first responders with training on how to work with child witnesses of domestic violence. The following year, an evidence-based domestic violence screening question was developed to help emergency room personnel identify potential domestic violence victims. Healthcare professionals received training on how to photograph domestic violence injuries and maintain photo documentation in patient's medical files. Interactive trainings for pediatricians were developed to help them determine when either the child or parent was experi-

encing domestic violence. This extensive collaborative project provided healthcare professionals with information about how to identify and respond to victims of domestic violence, and ultimately, provided victims with more avenues for accessing safety.

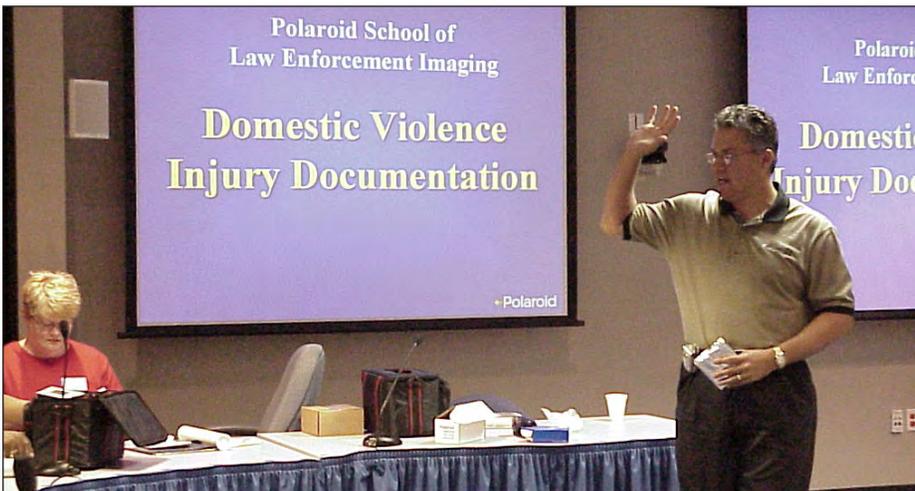
Increased Legal Help for Survivors

The Availability and Awareness of Legal Assistance for Domestic Violence Survivors, a 2009 report released by the MAG Regional Domestic Violence Council, focused on the legal needs of domestic violence survivors navigating the civil court system. Domestic violence survivors as well as legal and domestic violence professionals participated in the study. All reported survivors have a difficult time finding affordable legal services. Nearly one quarter of survivors wanting legal



The study found that nearly one quarter of all survivors wanting legal assistance did not receive it.

assistance reported they did not receive it. The study found survivors may not know how to access affordable legal help. Professionals cited awareness as the biggest barrier to survivors securing legal representation. Recommendations were made to develop a legal assistance fund, reach out for pro bono legal services, develop educational materials about the civil court process, and raise awareness of the benefits of legal services. The Arizona Coalition Against Domestic Violence's Legal Committee is moving forward with several recommendations including coordinating efforts for accessing pro bono legal services, providing educational materials about the civil court process, and raising awareness of the benefits of accessing legal services.



MAG hosted a training session in 2003 on how to use photography to document domestic violence injuries.



Methodology to Develop the Plan

The development of a new regional plan to end domestic violence was identified as a primary goal of the MAG Regional Domestic Violence Council for FY 2010. The Council described the goal of the plan as highlighting what has been done well to address domestic violence in the region and what successes may be capitalized upon to make the most impact moving forward. The Council decided to use a strength-based, three-phase approach utilizing *Appreciative Inquiry* interviews, a community forum, and strategy refinement. Funding was secured from Saint Luke's Health Initiatives to support meaningful community engagement and plan development.

Appreciative Inquiry Interviews

The first phase of plan development was an *Appreciative Inquiry* interview process. In October 2009, members of the MAG Regional Domestic Violence Council and community partners were invited to participate in a planning



Nearly 90 community partners and domestic violence survivors participated in Appreciative Inquiry interviews for development of the plan.

workgroup. The workgroup met twice to identify key areas of focus and create *Appreciative Inquiry* interview guides. Participants decided to create two sets of interview guides. One guide was designed for domestic violence professionals and another guide created for survivors of domestic violence. Workgroup members agreed to conduct and participate in interviews. Key community members were identified for requesting their participation in the interview process. The in-

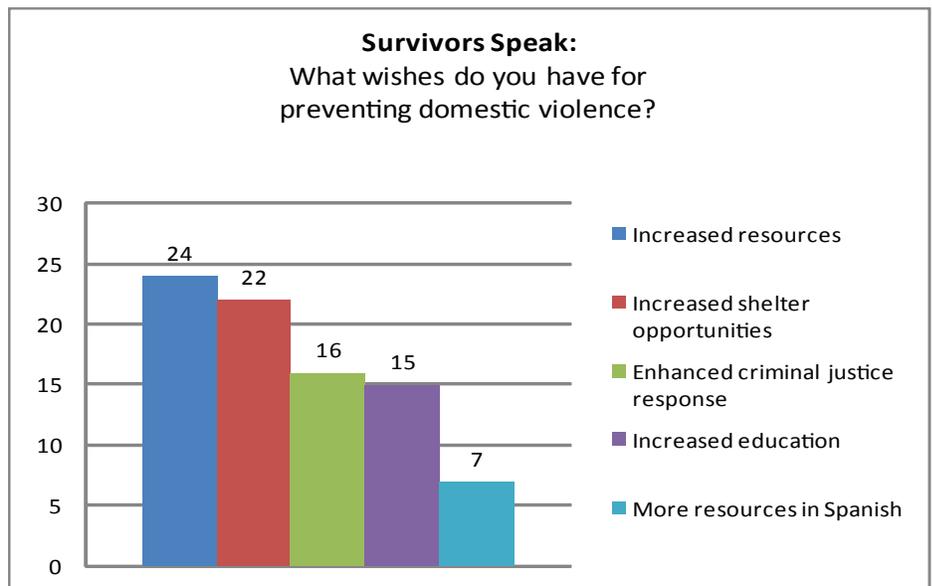
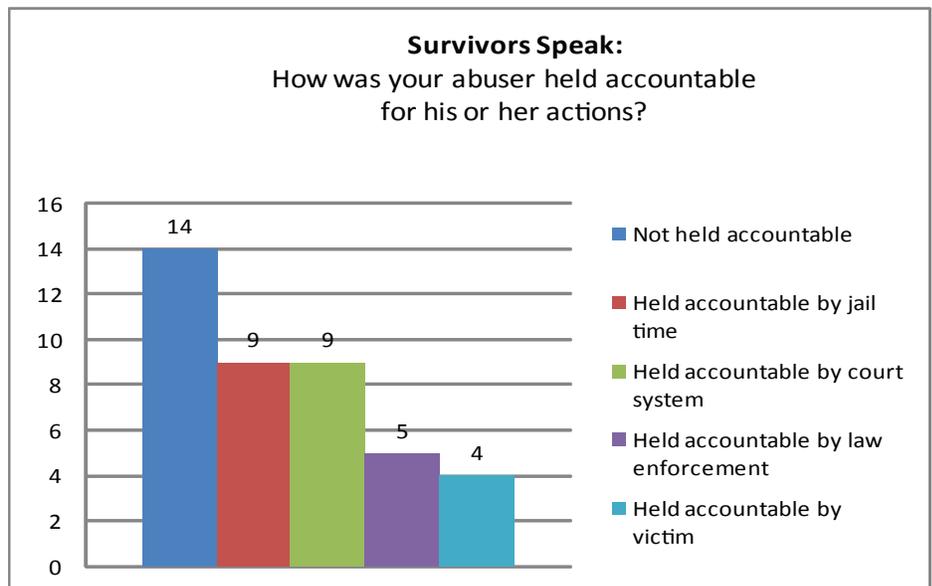
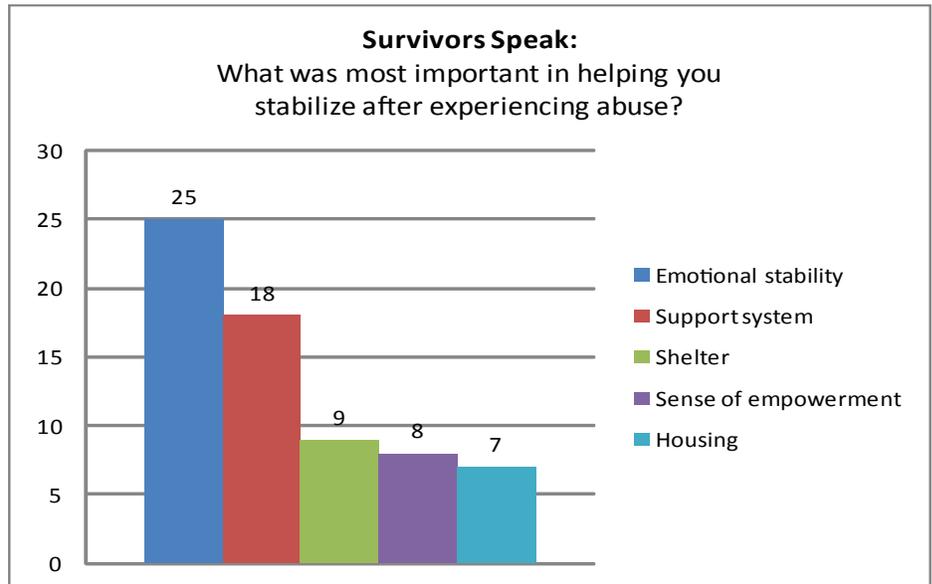
terview guides are provided in *Appendices B and C*.

Interviews were conducted between November 2009 and January 2010. Nearly 90 community partners and survivors of domestic violence participated in this process. Interview data was analyzed for emerging trends and notable gems. Interviews were conducted with 44 professionals and 43 survivors of domestic violence. An overview of the findings is provided below. A full

set of the results is provided in *Appendices D and E*.

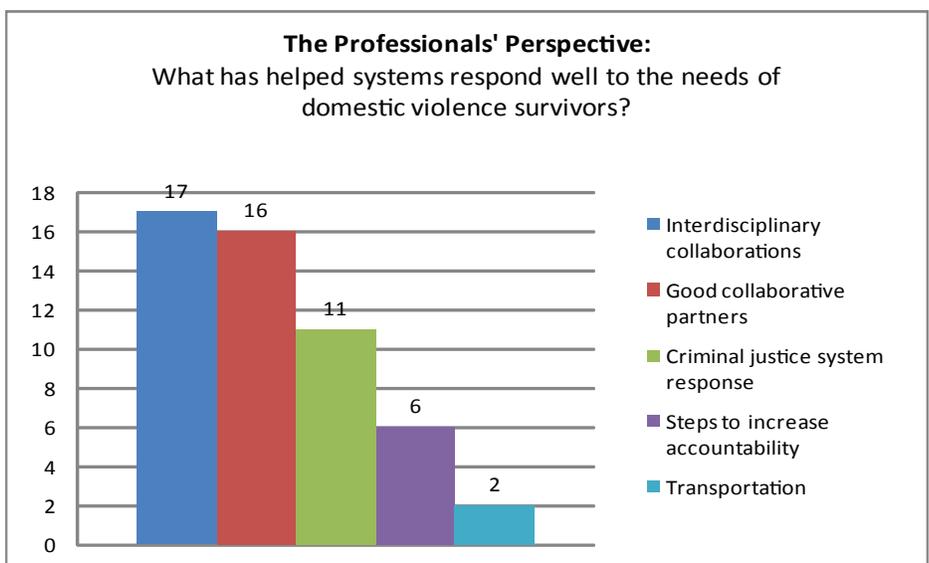
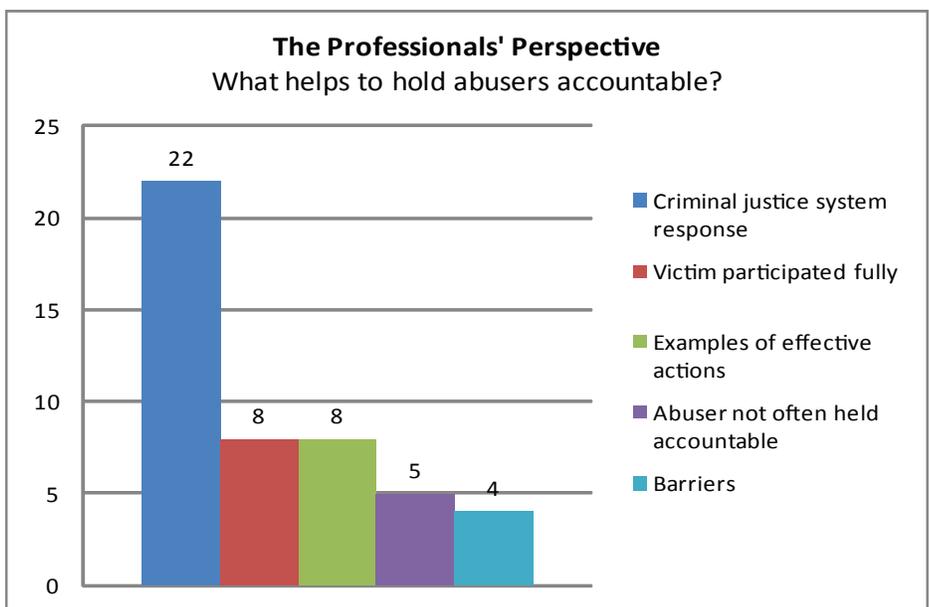
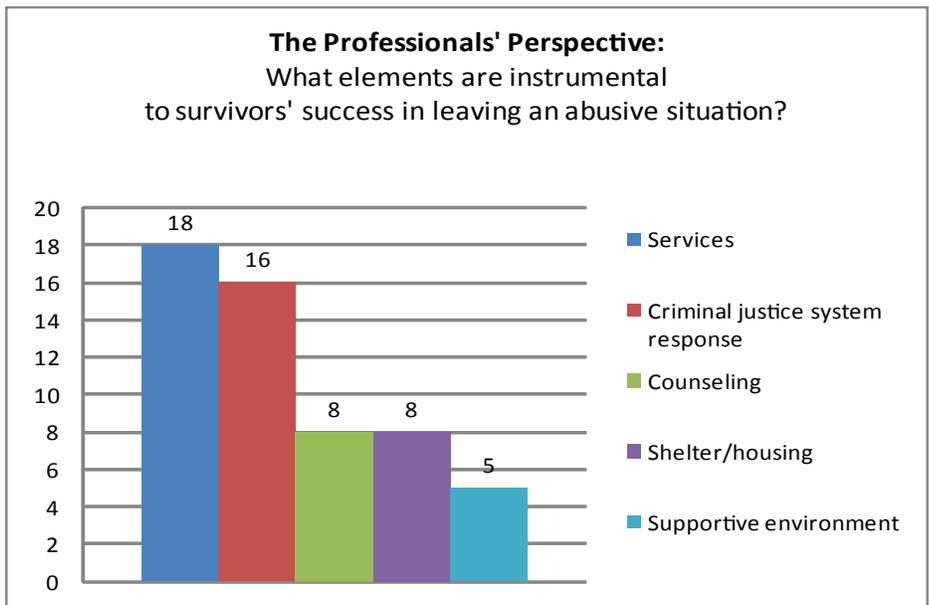
Professionals shared their enthusiasm for the positive changes that have occurred to address domestic violence in the region. From providing services and shelter to survivors, raising public awareness, and establishing interdisciplinary collaborations, they believe the region has come a long way. However, they also indicated the need to continue work in these areas to refine the great efforts already taking place. Professionals cited the need to continue services and shelter options in spite of drastic state budget reductions, provide healthy relationships education to youth, and strengthen partnerships with first responders and players in the criminal justice system. Overall, professionals shared they are truly inspired by survivors of domestic violence and dedicated to improving efforts to end domestic violence in the region.

Survivors of domestic violence shared the triumphs and tribulations of their personal stories in the hopes of helping others trying to escape from abuse. Many survivors spoke of the importance of a support system, including their children, friends, extended family, victim advocates, shelter staff, and other survivors. Since most of the survivors were accessed through



services and shelters, it was no surprise that many cited these resources made a huge difference in their ability to leave. Survivors wished for increasing resources to assist more survivors, providing additional avenues for accessing housing, increasing the public's awareness and education about domestic violence, and enhancing law enforcement and the criminal justice system's response to domestic violence. Overall, survivors were thankful for the help they have received in rebuilding their lives and the opportunity to enhance these resources for others struggling to leave a violent relationship.

Several common themes were found throughout interviews with both the professionals and survivors. The information gleaned from the interview process provided themes for discussion. These themes included the need to continue providing emergency services and shelter, enhancing public awareness and education efforts, and strengthening interdisciplinary collaborations between domestic violence providers, law enforcement, and the criminal justice system. These themes provided a foundation for the work of community partners at the MAG Regional Domestic Violence Summit, a community engagement forum.



MAG Regional Plan to End Domestic Violence Summit

On February 24, 2010, nearly 100 community partners attended the *MAG Regional Plan to End Domestic Violence Summit*, the second phase of plan development. Participants included representatives from law enforcement, prosecution, education, domestic violence shelters and programs, victim advocates, and survivors of domestic violence. The Summit was facilitated by Sarah Griffiths and Cassandra O'Neill of Wholonomy Consulting, Inc.

Appreciative Inquiry interview data provided a starting point for strategy development at the Summit. An overview of the nine themes found during analysis of the interview data was presented

to participants. One theme was assigned per table. Participants were encouraged to join the tables hosting the topics of interest to them and their work. These table discussions were organized using the World Café facilitation technique. World Café is used to create collaborative discussions, share knowledge, and build on the ideas of others to discover new opportunities.

World Café Table Topics:

- Increasing Community Awareness
- Increasing and Maintaining Access to Resources
- Creating a Coordinated Response
- Increasing Knowledge About Resources
- Increasing Accountability Throughout the Legal Process
- Creating Long-Term Supports

MAG Regional Plan to End Domestic Violence



On February 24, 2010, nearly 100 community partners attended the MAG Regional Plan to End Domestic Violence 2010 Summit.

- Creating Cross-System Opportunities
- Increasing Accountability in Police Response
- Creating Systems Responsive to Rural and Native American Communities

Participants at each table responded to the question: “Over the next five years, what would it take for the region to become known nationally for its work to end domestic violence?” The groups began their discussion based on this question and their table topic.

The World Café activity inspired rich conversations and ignited new ideas for developing strategies for the new regional plan.

Ideas from the World Café activity were brought forward



for the next step in the planning process, an Open Space Technology session. This technique is used to encourage creativity, generate enthusiasm, and help people self-organize to discuss what is most important to them and generate action plans to get results. Participants were seated in a large circle and encouraged to bring their ideas to the larger group. They were asked to frame their ideas within the context of the question: *“What strategies could be rolled out to connect and strengthen the regional work that seeks to end domestic violence?”* Participants generating ideas were designated as table hosts and took the lead in facilitating table discussions on their topics. Nine topic areas emerged during the Open Space planning session.

Open Space Table Topics:

- Sustaining Funding
- Awareness, Prevention, and Education
- Teen Help Programs
- Culturally Competent Services
- Housing
- Criminal Justice Accountability and Expectations
- Education of Criminal Justice System and First Responders
- Coordinated Community Response
- Shelters and Services Linkages



Strategy Refinement

The MAG Regional Domestic Violence Council distilled the work from the Appreciative Inquiry interview process and the Summit in the third phase of plan development. In March 2010, the nine topic areas identified during the Open Space planning session were developed into strategies. Community partners were engaged throughout strategy development. Fifteen strategies were offered to the MAG Regional Domestic Violence Council on April 1, 2010. The Council provided feedback on the strategies. The strategies and action plans are reflected in the next section of this report.





Strategies and Action Steps

Community partners provided valuable input and innovative ideas throughout the planning process. Fifteen strategies emerged from the facilitated activities at the MAG Regional Plan to End Domestic Violence Summit. These strategies aligned with input received during the Appreciative Inquiry Interview process. The strategies were organized into the four main categories of Funding, Training and Education, Coordination and Collaboration, and Services.



Fifteen strategies provide the framework:

1. Communicate the need for sustainable funding for existing programs and services.
2. Develop avenues for raising awareness and educating the public.
3. Increase social capital through grassroots efforts focused on the prevention of domestic violence.
4. Develop standardized, multi-disciplinary curriculum for providing domestic violence education to criminal justice system and first responders.
5. Develop multi-disciplinary training for victims about the criminal justice process, law enforcement procedures, and realistic expectations of these systems.
6. Develop cross-training between law enforcement, criminal justice system and advocates.
7. Increase coordination and collaboration between shelters and services.
8. Increase access to information on available resources.
9. Coordinate multi-disciplinary effort for reviewing standard protocols and practices for responding to domestic violence.
10. Connect all critical resources for people experiencing domestic violence and homelessness through a coordinated community response.
11. Create an ideal model for culturally competent prevention and intervention services.
12. Enhance the process for appropriately meeting survivors' housing needs.
13. Develop support groups for teens who have experienced or witnessed domestic violence.
14. Develop more transportation options for those residing in shelter.
15. Create long-term supports for helping survivors maintain their safety.

Category: Funding

Strategies	Short-Term	Long-Term	Resources	Primary Partners	Time-line
1. Communicate the need for sustainable funding for existing programs and services.	1) Conduct meeting with foundation leaders, 2) Advocate on impact of loss of funding.	Create an endowment for funding services.	Domestic violence providers, business community, media, public	AZ Coalition Against Domestic Violence, Arizona Foundation for Women	FY 2011-2012

Category: Training and Education

Strategies	Short-Term	Long-Term	Resources	Primary Partners	Time-line
2. Develop avenues for systems to raise awareness and educate the public about domestic violence.	1) Research best practices for raising awareness and providing education on a systems level, 2) Develop basic, consistent message, 3) Develop train-the-trainer modules for educating children and adults.	Create culturally competent, regional approach for systems to raise awareness and provide education on domestic violence; including awareness events, outreach, education in schools.	Schools, hospitals, doctors, WIC, county clinics, service providers, law enforcement, fire departments, behavioral health, government, community leaders, faith community	AZ Coalition Against Domestic Violence, Arizona Foundation for Women	FY 2011-2013
3. Increase social capital through grassroots efforts focused on the prevention of domestic violence.	1) Research strategies for leveraging social capital, 2) Recruit volunteers to assist in prevention and awareness activities.	Develop opportunities for community conversations to raise awareness and increase prevention of domestic violence; including awareness activities and outreach.	Community based leaders and groups, members of faith communities, neighbors, friends, relatives, and others engaged through personal connections	Purple Ribbon Council	FY 2011-2012
4. Develop standardized, multi-disciplinary curriculum for providing domestic violence education to criminal justice system and first responders.	1) Identify existing trainings for the criminal justice system, first responders, and law enforcement; 2) Determine how to expand or improve existing training curriculum.	1) Develop collaboration among cities to share lessons learned including best practices for working with rural and Native American communities, 2) Establish a train-the-trainer curriculum for the criminal justice system, first responders, and law enforcement.	AZ POST, AZ Prosecuting Attorneys' Advisory Council, Regional Training Advisory Council, judges, mayors, decision makers, attorneys, police officers, advocates, survivors	AZ Coalition Against Domestic Violence, AZ Supreme Court Administrative Offices of the Court, Governor's Division for Women	FY 2011-2013
5. Develop multi-disciplinary training for victims about the criminal justice process, law enforcement procedures, and realistic expectations of these systems.	1) Determine key areas to highlight in training, 2) Develop multi-disciplinary training.	Deliver multi-disciplinary training.	AZ POST, AZ Prosecuting Attorneys' Advisory Council, family advocacy centers, judges, clergy, defense attorneys, survivors, police officers, advocates	Governor's Division for Women, Phoenix Family Advocacy Center	FY 2011-2012

Category: Training and Education (continued)

Strategies	Short-Term	Long-Term	Resources	Primary Partners	Time-line
6. Develop cross-training between law enforcement, criminal justice system, and advocates.	Research national models for successful collaboration in best interest of survivors.	Develop regional process for ongoing collaboration in best interest of survivors.	AZ Prosecuting Attorneys' Advisory Council, Phoenix Family Advocacy Center, police officers, judges, attorneys, advocates, survivors,	AZ POST, MAG	FY 2012-2013

Category: Coordination and Collaboration

Strategies	Short-Term	Long-Term	Resources	Primary Partners	Time-line
7. Increase coordination and collaboration between shelters and services.	Increase communication between shelters, programs, and transitional housing programs.	Develop environment of collaboration instead of competition among shelters and programs.	Shelter directors, program directors, victim services, transitional housing programs	AZ Coalition Against Domestic Violence	FY 2011-2012
8. Increase access to information on available resources.	1) Identify types and locations of existing resources, 2) Determine where resources are needed.	Develop a centralized database or Website listing resources.	Shelter directors, program directors, victim services, advocates, survivors, housing programs, attorneys	AZ Coalition Against Domestic Violence, AZ Department of Economic Security	FY 2011-2012
9. Coordinate multi-disciplinary effort for reviewing standard protocols and practices for responding to domestic violence.	Review existing protocols and practices of law enforcement, prosecution, and advocates for responding to domestic violence.	Develop multi-disciplinary process for seamless response to domestic violence.	City of Phoenix Police Department, Phoenix Prosecutor's Office, Mesa Prosecutor's Office, Avondale Police Department, Southwest Family Advocacy Center, El Mirage Police Department, Tolleson Police Department Scottsdale Prosecutor's Office Victim Services Program, AZ Criminal Justice Commission, Sojourner Center, AZ Coalition Against Domestic Violence	MAG, Governor's Division for Women	FY 2011-2012
10. Connect all critical resources for people experiencing domestic violence and homelessness through a coordinated community response.	1) Survey domestic violence and homeless providers to identify who they currently engage for response, 2) Research best practices for public health and emergency response models, 3) Educate faith leaders on how to address domestic violence.	Develop a coordinated community response in the region.	Domestic violence shelter and providers, homeless shelter and providers, healthcare community, first responders, faith communities, community leaders, city leaders	MAG, Governor's Division for Women	Ongoing

Category: Services

Strategies	Short-Term	Long-Term	Resources	Primary Partners	Time-line
11. Create an ideal model for culturally competent prevention and intervention services.	Develop culturally diverse trainings designed for indepth learning.	Organize discussions on coordinating services to address cultural, age, disability, and substance barriers without denying services.	Variety of cultures (i.e., refugees, LG-BTQ, disabilities, aging), Department of Economic Security, Department of Health Services	AZ Coalition Against Domestic Violence in partnership with culturally specific organizations, such as Southwest Indigenous Women's Coalition, AZ South Asians for Safe Families, Chicanos por la Causa, Governor's Division for Women	FY 2011-2013
12. Enhance the process for appropriately meeting survivors' housing needs.	Create more opportunities for networking and sharing information about existing services.	Create a "big picture" approach to planning services without sacrificing attention to crisis needs.	Shelters, transitional housing, affordable housing, MAG, AZ Department of Economic Security, Valley of the Sun United Way, Local Initiatives Support Corporation	AZ Coalition Against Domestic Violence, Arizona Housing Alliance	FY 2012-2014
13. Develop support groups for teens who have experienced or witnessed domestic violence.	Develop network of domestic violence counselors (both male and female) to help teens.	Provide support group, counseling services, and prevention education to teens.	AZ Coalition Against Domestic Violence, AZ Department of Economic Security, survivors, teen survivors, counseling programs	Governor's Division for Women, Court Appointed Special Advocates (CASA)	FY 2011-2012
14. Develop more transportation options for those residing in shelter.	Research existing transportation options and barriers.	Coordinate new or expanded transportation options with community partners.	Shelters, program providers, transportation programs, AZ Department of Economic Security	MAG	FY 2011-2012
15. Create long-term supports for helping survivors maintain their safety.	Identify types of long-term supports that increase survivors' success in remaining safe.	Create long-term support services for survivors to access after leaving abuse.	Shelters, program providers, survivors, AZ Department of Economic Security	AZ Coalition Against Domestic Violence, Governor's Division for Women	FY 2012-2014



Next Steps

The new regional plan provides strategies for moving forward efforts to end domestic violence. These strategies will only result in meaningful work if they are implemented, which seems daunting in the current economic environment. While resources remain thin, the relationships and reliance on community partners becomes even more important. Together, we can take the next steps in providing safety to victims and holding offenders accountable.

Several projects that will move the plan's strategies forward are already taking shape. These projects highlight the immense need for collaborative efforts for increasing training and education, enhancing coordination, and improving services. The momentum in these areas demonstrates the community's desire to make the necessary changes to better serve those impacted by domestic violence. A few of the projects are mentioned below.



Fourteen community partners collaborated to secure STOP grant funding for the MAG Protocol Evaluation Project which will assess protocols for arresting and prosecuting domestic violence offenders.

Training and Education

Thanks to STOP Grant funding through the Governor's Division for Women, the MAG Protocol Evaluation Project began in May 2010. The purpose of this project is to assess the protocols used to arrest and prosecute domestic violence misdemeanor offenders. The project includes an evaluation of existing protocols used by law enforcement, prosecutors, and victim advocates to address domestic violence cases; assessment of national

and local promising practices; and the development of training and public awareness tools. This project strives to enhance communication and continuity across multiple disciplines to the betterment of survivor outcomes.

The Arizona Supreme Court Administrative Office of the Courts is leading a collaborative project to enhance domestic violence education for members of the criminal justice system. An education specialist is developing

computer-based trainings specific to the unique nature of domestic violence cases. Additional trainings are being developed for inclusion in upcoming conferences for judges. The group has applied for grant funding to support this effort.

Coordination and Collaboration

Increasing coordination and collaboration are key components of work being done by the Arizona Coalition Against Domestic Violence and A New Leaf. In partnership with MAG, the Coalition began hosting a quarterly meeting of the executive directors of domestic violence shelters throughout the region. Attendees have found these meetings to be meaningful and are increasing their frequency to monthly.

The AzCADV Residential Programs Committee provides domestic violence service providers with a monthly opportunity to exchange information on available resources through their organizations. A New Leaf, in partnership with Valley of the Sun United Way, facilitates a bi-monthly meeting of the Domestic Violence Collaborators. These meetings provide shelter and program personnel the chance to share promising practices, build collaborative efforts, and discuss trends impacting shelters.

Services

The MAG Domestic Violence and Homeless Transportation Project will assess the transportation needs of domestic violence survivors. In partnership with the Arizona State University CARE Program, an intern will conduct a series of focus groups with domestic violence shelter staff and survivors. These groups will help identify transportation needs of survivors in shelter, especially related to their ability to gain and maintain employment. Participating agencies include Chicanos por la Causa, the Area Agency on Aging DOVES Program, Sojourner Center, and Chrysalis. This project will lead to increased transportation options for those residing in domestic violence shelters throughout the region.

Summary

Forward movement is vital to the success of the *MAG Regional Plan to End Domestic Violence*. Community partners will champion this work. Progress will be monitored on a quarterly basis through reports offered to the MAG Regional Domestic Violence Council. Feedback from the Council will include any needed modifications. In addition, an annual report will be developed to indicate progress made, areas needing improvement, and actions to stimulate implementation efforts. This report will be prepared for the MAG Regional Domestic Violence Council and distributed to the public.



The Domestic Violence and Homeless Transportation Project will assess transportation needs for domestic violence survivors.



Conclusion

During the past ten years, great strides have been made in addressing domestic violence. More shelter beds reduced the number of individuals and families turned away from a safe place to sleep. Resources empowered employers to reach out to personnel experiencing abuse. Screening questions helped healthcare professionals identify potential victims of abuse. These important steps created more opportunities for those experiencing abuse to reach out and access help.



Shelter beds, like this one at the Sojourner Center, are at risk of closing due to recent budget cuts. These beds provide an important safety net to ensure individuals and families have a safe place to go when leaving an abusive situation.

Difficult decisions are being made about domestic violence services. Budget cuts are challenging providers to continue their important work. Service providers are seeking resourceful options for continuing to help those seeking safety from violence. Devoted community partners are pulling together in creative ways to

maintain services that have saved countless lives throughout the region.

Ending domestic violence will require increased coordination and communication. This plan lays out strategies focusing on the importance of knowing what re-

sources exist and how to leverage severely limited resources. The way of doing business will evolve to ensure services remain in place to provide safety. The role of the coordinated community response is vital to continuing to make the most impact for victims and survivors of domestic violence.

See the Signs.



the Abuse.



Acknowledgements

The development of the MAG Regional Plan to End Domestic Violence was made possible by the many dedicated community partners and survivors who gave their time and expertise to the planning process. Community partners participated in the Appreciative Inquiry interview process, attended the Summit, and assisted in gathering input from survivors of domestic violence. This plan is a reflection of their dedication to ending domestic violence in the region.



MAG Regional Domestic Violence Council

- Commander Kim Humphrey, *Phoenix Police Department, Chair*
- Barbara Marshall, *Maricopa County Attorney's Office, Vice Chair*
- Celeste Adams, *Save the Family*
- Christina Avila, *City of Avondale*
- Lieutenant Robert Bates, *Phoenix Police Department*
- Kathy Berzins, *City of Tempe*
- John A. Blackburn Jr., *Arizona Criminal Justice Commission*
- Allie Bones, *Arizona Coalition Against Domestic Violence*
- Chris Christy, *Salt River Pima-Maricopa Indian Community*
- JoAnn Del-Colle, *Phoenix Family Advocacy Center*
- Diane Enos, *President, Salt River Pima-Maricopa Indian Community*
- Will Gonzalez, *City of Phoenix Prosecutor's Office*
- Laura Guild, *Arizona Department of Economic Security*
- Dan Hughes, *City of Surprise*
- Lynette Jelinek, *Glendale Fire Department*
- Mary Lynn Kasunic, *Area Agency on Aging*
- Patricia Klahr, *Chrysalis Shelter Inc.*
- Suzanne Klapp, *Councilmember, City of Scottsdale*
- Phil Lieberman, *Councilmember, City of Glendale*
- Jodi Beckley Liggett, *Arizona Foundation for Women*
- Maria-Elena Ochoa, *Governor's Office for Children, Youth and Families*
- Dottie O'Connell, *Chicanos por la Causa*
- Stephanie Olohan, *City of Goodyear*
- Connie Phillips, *Sojourner Center*
- John M. Pombier, *City of Mesa*
- Kerry Ramella, *Phoenix Fire Department*
- Sarah Youngblood, *Community Legal Services*

Regional Domestic Violence Plan Workgroup

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- Allie Bones, *Arizona Coalition Against Domestic Violence*
- Maribel Castro, *Chicanos por la Causa – De Colores Shelter*
- Dottie O’Connell, *Chicanos por la Causa – De Colores Shelter*
- JoAnn Del-Colle, *Phoenix Family Advocacy Center*
- Debbie Driscoll, *City of Buckeye*
- Will Gonzalez, *City of Phoenix Prosecutor’s Office*
- Alice Ghareib, *Area Agency on Aging*
- Laura Guild, *Arizona Department of Economic Security*
- Katie Hobbs, *Sojourner Center*
- Terri Leveton, *Phoenix Rescue Mission*
- Mary Alice McKone, *Elim House*
- Sandy Robertson, *A New Leaf*
- Linda Scott, *Jewish Family & Children’s Service*

MAG Regional Plan to End Domestic Violence Summit and Appreciative Inquiry Interview Participants

- A New Leaf
- Aguila Youth Leadership Institute

- Ak-Chin Indian Community
- Area Agency on Aging
- Arizona Coalition Against Domestic Violence
- Arizona Department of Economic Security
- Arizona Prosecuting Attorneys’ Advisory Council
- Arizona State University Morrison Institute for Public Policy
- Arizona Supreme Court, Administrative Office of the Courts
- Avondale Young Families Program
- Catholic Charities Community Services
- Catholic Diocese of Phoenix
- Chicanos por la Causa, De Colores Shelter
- Child Protective Services
- Chrysalis Shelter, Inc.
- City of Glendale Fire Department
- City of Mesa Police Department
- City of Mesa Prosecutor’s Office
- City of Phoenix Fire Department
- City of Phoenix Police Department
- City of Phoenix Prosecutor’s Office
- City of Scottsdale Police Department
- City of Tempe, Care 7 Program
- DRA Consulting Services
- Glendale City Court
- Governor’s Office for Children, Youth, and Families
- Health Choice Arizona
- Homeward Bound
- Human Resource Essential
- Jewish Family & Children’s Service
- Maricopa County Attorney’s Office
- Mariposa: Wings to Safety
- Marley House
- Mountain Sky Junior High/ Washington Elementary School District
- National Organization for Women
- Nina Mason Pulliam Charitable Trust
- O’Connor House
- Peoria Unified School District
- Phoenix Family Advocacy Center
- Phoenix Rescue Mission
- Purple Ribbon Council to Cut Out Domestic Abuse
- Recovery Innovations of Arizona
- Remax Infinity
- Saint Luke’s Health Initiatives
- Salt River Pima-Maricopa Indian Community
- San Lucy District
- Save The Family
- Sojourner Center
- Southwest Human Development
- Tempe Community Council
- The Arizona Republic
- The Salvation Army Elim House Domestic Violence Shelter
- Wesley Community and Health Center
- Wholonomy Consulting, Inc.



Appendices

Appendix A: MAG Regional Domestic Violence Council Timeline

1999

- Developed the *MAG Regional Domestic Violence Plan*.
- Established the MAG Regional Domestic Violence Council.
- Published Domestic Violence Safety Planning Brochures.

2000

- Formed Employers Against Domestic Violence (EADV).
- Developed *Model Guide for Developing Local Coordinating Councils*.

2001

- Formed Health Cares About Family Violence initiative collaborating with hospitals to implement screening and follow up protocol for domestic violence.
- Developed *Domestic Violence Resource Guide for Faith Leaders* and sponsored the Religious Response to Domestic Violence forum.
- Published *Crisis Response Team Reference Guide*.
- Held first Employers Against Domestic Violence forum about employers' response

to domestic violence in the workplace.

2002

- Published *MAG Regional Domestic Violence Plan Update*.
- Developed Domestic Violence Response Kits, including domestic violence screening lanyards and shoe cards.

2003

- Partnered with The Arizona Republic for first Annual Walk to End Domestic Violence.
- Co-sponsored forum on the religious response to domestic violence.
- Coordinated health care provider training on photographing domestic violence injuries.

2004

- Partnered with Men's Anti-Violence Network (MAN) to develop domestic violence awareness packets for employers.
- Developed *Domestic Violence Plan Five-year Report Card*.
- Held first annual domestic

violence awareness press conference, "There's Not a Minute to Lose."

- Produced interactive training CDs helping pediatricians identify potential domestic violence cases.
- Partnered with The Arizona Republic for second Annual Walk to End Domestic Violence.
- Developed training curriculum for Crisis Response Teams working with children witnesses of Domestic Violence.

2005

- Conducted a Domestic Violence Survey through Behavior Research Center.
- Participated with The Arizona Republic for the third Annual Walk to End Domestic Violence.
- Conducted Employers Against Domestic Violence trainings.
- Held "Do You See It?" press conference highlighting the importance of identifying and stopping domestic violence.

2006

- Developed *The Need for Increased Domestic Violence Shelter in the MAG Region*, which assisted in the efforts to increase domestic violence shelter beds.
- Drafted the *Exploration of the Fiscal Impact of Domestic Violence on Local Criminal Justice Systems* in the MAG Region.
- Implemented Youth Empowerment Project (YEP) to raise awareness of dating violence among teens.
- Launched *WebofFriends.org* website with live chat during annual press conference.

2007

- Conducted region's first "Text-a-thon" with teens at annual press conference to spread the word about the importance of healthy relationships.
- Coordinated first YEP Public Service Announcement (PSA) Competition, resulting in a television PSA on teen dating violence.
- Coordinated first annual joint committee meeting of the MAG Regional Domestic Violence Council and MAG Continuum of Care Regional Committee on Homelessness.

2008

- Organized first Smart Dating event at the annual press conference to help teens See the Signs. Stop the Abuse.
- Conducted community outreach engaging youth in discussions about healthy dating relationships.
- Implemented second Annual YEP Public Service Announcement Competition, resulting in a radio advertisement.
- Identified collaborative goals at second annual joint committee meeting of the MAG Regional Domestic Violence Council and MAG Continuum of Care Regional Committee on Homelessness.
- Participated in the Annual Walk to End Domestic Violence

2009

- Held third Annual YEP Public Service Announcement Competition, resulting in development of print ad and Web banner.
- Developed YEP Best Practices Toolkit.
- Released *The Availability and Awareness of Legal Assistance for Domestic Violence Survivors* report.
- Developed regional screening principles for the domestic violence and homeless shelter system.

- Conducted a domestic violence and homeless shelter capacity study as a follow up to *The Need for Increased Domestic Violence Shelter in the MAG Region*.
- Launched a community involvement process to update the MAG Regional Plan on Domestic Violence.

2010

- Conducted the Regional Plan to End Domestic Violence 2010 Summit
- Created the *MAG Regional Plan to End Domestic Violence*.

Appendix B: Appreciative Inquiry Interview Guides – Survivors

Maricopa Association of Governments
Regional Domestic Violence Council
Appreciative Inquiry Interview Questions - DV Survivor

Name of Interviewer: _____

Name of Interviewee: _____

Date of Interview: _____

Thank you for participating in this interview process. As someone who has been personally touched by domestic violence, you can play an important role in identifying what is most helpful to those leaving a violent situation. Thank you for your willingness to share your experience so others may receive the help they need to be safe.

Although changes have been made in addressing domestic violence in our community, there is still a lot of work to be done. Information is being gathered from survivors of domestic violence as well as professionals helping those currently experiencing domestic violence to help identify what is already being done well and what should be done in the future. Thank you for participating in this effort.

Services for Domestic Violence

- 1) Tell me about a positive experience in which one or more systems you accessed for help were responsive to your needs. Examples of systems include criminal justice, law enforcement health care, community organizations, and/or faith communities. What happened in this experience? What made it positive for you?
 - What did the organization or agency do to make this positive outcome possible?
 - What did you do that helped achieve this positive outcome?
- 2) What would you say were the three most important things that helped you stabilize after the abuse?
- 3) Imagine it is four years from today and the three most important things that helped you are available to all women who experience domestic violence. What has changed?
- 4) Was it difficult for you to access shelter or services? What helped you to connect with these resources?
- 5) Can you describe a time when your abuser was held accountable for his/her actions? How was your abuser held accountable? What happened?
 - What did the organization or agency do to make this positive outcome possible?
 - What did you do that helped achieve this positive outcome?

Prevention of Domestic Violence

- 6) What is being done the best in our community to prevent domestic violence?
- 7) What three wishes do you have for the way domestic violence survivors are served in Arizona? What three wishes do you have for preventing domestic violence in Arizona?

Additional Comments

- 8) Is there anything else you would like to share?

Please contact Renae Tenney, MAG Human Services Planner I, with questions at rtenney@mag.maricopa.gov or 602-254-6300. Interview notes should be sent to Renae by January 29, 2010 via email, fax (602-254-6490), or mail (302 N. 1st Ave., Phoenix, AZ 85003).

Thank you!

Appendix C: Appreciative Inquiry Interview Guides – Professionals

Maricopa Association of Governments Regional Domestic Violence Council Professional Interviewer Guide

Name of Interviewer: _____

Name of Interviewee: _____

Date of Interview: _____

We have come a long way in addressing domestic violence in our region. As professionals in this work, we have implemented strategies to help identify domestic violence, developed brochures to help victims keep themselves safe, and expanded shelter and support services. Thank you for your shared passion and dedication in making a difference for those escaping violent relationships.

Now it is time to take a close look at how we can continue to make a difference for those experiencing domestic violence. We need your help in generating fresh approaches. We ask you to use “out of the box thinking” to identify concepts you have gleaned from your life and work experiences. Please join us as we continue to put an end to domestic violence in this region.

Preservation of Resources

- 1) Think of a time when you assisted a domestic violence survivor along their journey to self-sufficiency. What about this survivor’s story and experience was inspirational?
 - What would you say were the *top three elements* or services that made their journey possible?
 - What would you say can be done to maintain or enhance these elements or services?

(INTERVIEWER: If the interviewee needs prompting, mention these may be feelings, relationships, services, and/or resources they found helpful. Try to determine where their three answers originated. For example, if “hope” was a major element, where did this hope come from? What made them feel hopeful?)

- 2) Not all survivors may share the same access to services and shelter. What would you say is being done well to provide access to underserved domestic violence populations?

(INTERVIEWER: If the interviewee needs prompting, mention that “underserved populations” may include people from historically marginalized groups. Some examples may be refugees, immigrants, undocumented people, those with substance abuse issues, disabilities, mental health issues, and affluence.)

Offender Accountability

- 3) Describe a peak experience when an abuser(s) was held accountable for his/her actions.
 - What did the survivor do that helped achieve this positive outcome?
 - What did the organization or agency do differently that allowed this positive outcome to be possible?

Systems Accountability

- 4) Describe a positive experience in which one or more systems accessed by a domestic violence victim(s) were responsive to the victim's needs.
- What made this experience memorable for you?
 - What did the systems do differently that helped to achieve this positive outcome to be possible?

(INTERVIEWER: If the interviewee needs prompting, mention that "systems" may include outreach by community organizations and/or faith communities, law enforcement, health care, and courts. Try to determine how the systems were helpful. For example, if the interviewee says the police were helpful, what did they do that was helpful?)

Prevention

- 5) Describe a peak experience when you clearly saw the positive effects of domestic violence prevention efforts. What do you see as the keys to successful prevention efforts?

Collaboration/Leveraging

- 6) Tell about a time when you were energized about working closely with others in reaching a shared goal. What made the experience energizing for you?

Leadership Development

- 7) Recall a time when you were inspired by irresistible leadership. What was the situation and what made the leadership irresistible to you?
- In what ways has this experience influenced your leadership style?
 - What would you say can be done to develop new leaders and re-energize current leaders in the domestic violence community?

Additional Comments

- 8) Is there anything else you would like to share?

Please contact Renae Tenney, MAG Human Services Planner I, with questions at rtenney@mag.maricopa.gov or 602-254-6300. Interview notes should be sent to Renae by January 29, 2010, via email, fax (602-254-6490), or mail (302 N. 1st Ave., Phoenix, AZ 85003).

Thank you!

Appendix D: Appreciative Inquiry Interview Results – Survivors

Services for Domestic Violence #1		
<i>Part A: How were systems responsive to survivors' needs?</i>		
Result	#	Comments
Shelters	6	Available shelter space, a safe place, without shelter space we would be dead, saw past policy
First Responders (police, fire)	5	Treated as high priority, gave referrals, assisted with securing shelter and advocate, police helped with order of protection
Justice System Response	4	Abuser arrested and held, received order of protection, a judge finally believed in me, help with divorce for free
Faith Community	2	Provided support, helped with accessing shelter and services
Employer's Assistance	1	Helped with counseling and safety planning
Healthcare Professionals	1	
Food Bank	1	
<i>Part B: How did survivors help achieve a positive outcome?</i>		
Result	#	Comments
Gained sense of empowerment	10	Self-awareness, inspired myself by getting this far, peace and confidence, learned from my situation, taking care of my health, don't have to live with threats and violence, have learned to be a better person, now living a better, healthy, peaceful life, better with my children, have grown in many different ways, learned about laws, felt empowered through DV Walk, received assistance
Gained knowledge of services	9	Transportation, job training, counseling, housing, Community Information & Referral, TERROS, Sojourner, orders of protection, Jewish Family and Childrens' Services
Went into shelter	7	Shelter services, DeColores, protection for self and family,
Found support system	6	Shelter staff, other survivors in shelter, family, co-workers, girlfriends

Services for Domestic Violence #2		
<i>Three most important things that helped you stabilize after abuse?</i>		
Result	#	Comments
Emotional Stability	25	Time enough to be okay, counseling, talking with someone, crisis team, therapy, validation by therapist, mental health issues, feel better emotionally, emotional well-being (faith, hope, health), regained my health, love for self and children, feeling better, no more fear, no more abuse and insults, less stress, better health, family therapy, feel I have a future filled with triumph
Support System	18	Family, friends, other survivors, shelter staff, support groups, not feeling alone, having mentors or role models
Shelter	9	Shelter resources, resources, victim advocate, VAWA services and process, My Sister's Place, Save the Family, Community Bridges, shelter
Sense of Empowerment	8	Willpower, self-determination, discipline, self-sufficient, independent, hope, talking about my experience in court, sharing my knowledge of resources with another victim
Housing	7	Find housing, housing, some place to go, safety, find a place to live, a safe place to live where he can't hurt me
My Children	6	Safety for my children, my son
Financial Stability	4	Financial stability, employment, school, education
My Faith	4	Faith, spiritual growth, church, God, my faith
Leaving situation	3	Getting out, leaving situation, moving far away
Legal Help	2	Legal help, filed for divorce and child custody
Communication across systems	1	

Services for Domestic Violence #3

Four years from now, what has changed to help women experiencing domestic violence?

Result	#	Comments
Shelter and Services	13	Shelter, counselors, housing, medical, safe place, safety, classes, affordable services, referrals, English classes
Awareness	10	In schools and mainstream media, television commercials, domestic violence awareness expanded, prevention education, educating children, more education, less stigma, different environment
Empowerment	8	Willing to ask for help, women speaking out, not returning to abuser, feeling stronger and more self-sufficient after overcoming abuse, enhanced self-esteem, focus on self and children
Counseling	6	Provided a lot of help and advice
Legal Help	6	Orders of protection; divorce; understanding laws; legal help; court-mandated classes; laws in general because not only am I affected, my child is affected
Support System	4	Support system, unconditional support of a friend, support of other survivors, my children, my teachers
Coordination Between Systems	3	Relationship between victim advocate and probation; being able to relay on help from police, judge, shelter staff; centralized family services with judges, police, counselor, shelter, therapists, groups, legal advocates
Career and Educational Opportunities	2	Accessible education available to all women
DV taken seriously by police	2	Have specific number to call police about DV
Easier shelter screening	2	Process was long and questions were rude, finding shelter space
Transitional Housing	2	
Transportation	1	
CPS Intervention	1	

Services for Domestic Violence #4

Part A: Was accessing shelter or services difficult?

Result	#	Comments
Lack of Services	13	Unaware of help; did not know shelters existed; people are not aware of resources; limited awareness of shelters; information not available without shelter referrals; difficult to access services; unaware of shelters; need more information about DV at WIC offices, stores, and wherever victims might seek help, DES not helpful, long-term services to help victims from having to return to abusers, access to services in college
Shelter	10	Every time I called the shelter was full, difficult finding open space at shelter, called shelter but they would not take me in, taken to Watkins first then to De Colores four days later, Long process to get into shelter, screening questions were difficult and rude, prefer separate rooms in shelter
Eligibility Criteria	5	Not serving men, rich people, drug users, women with older children; youth programs difficult to find, transitional housing does not accept teens, had to be labeled Seriously Mentally Ill to be able to access affordable medication and therapy, ridiculous process for accessing food stamps requiring abuser to verify victim's income level
Law Enforcement	3	Talk down to victims, could do more, survivor charged with DV
Transportation	2	Limited public transportation

Part B: What helped connect you to them?

Result	#	Comments
Received referrals	8	Referred from hospital, asked for referrals, referred to resources, nurse was a great resource, teachers connected me to DeColores, received shelter list from church, crisis hotline, TV ads motivated me to keep trying
Not difficult	5	Helped by friend, CONTACTS helped, referred by Mexican Consulate, police provided a number to call and shelter was waiting with open arms and love

Services for Domestic Violence #5

Part A: How was your abuser held accountable?

Result	#	Comments
Was Not Held Accountable	14	Rural area, few services, strong church influence, given probation with recent girlfriend but statute of limitations with me, held accountable for hurting others but not me, never held accountable, deported, police informed me I could stay somewhere I felt safe if I feared what would happen after they left, no charges filed because he threatened to take my children, no charges filed
Held Accountable by Jail Time	9	Jailed for two days and sent to 26 weeks of DV classes, jailed for 24 hours then made bail of \$20,000, jailed for two months, husband went to jail while I was pregnant, given jail time but came out angrier, victim charged with abuse, imprisoned for one month
Held Accountable by Court System	9	Fined for delaying legal process, provided documentation aiding lawyer, court held him accountable, judge held abuser accountable, Order of Protection, child support enforcement, provide support
Held Accountable by Law Enforcement	5	Arrested on other charges and deported but came back worse, arrested and served 15 days, arrested when he put me in a coma and almost killed my son, police very helpful
Held Accountable by Victim	4	Victim left, survivor advocated and didn't give up, abuser lost his family, abuser lost his children
Was Not Held Accountable - Not Reported to Police	4	Did not contact police due to fear; he made threats; threatened to take my children, kill me, or report me and I would get deported
Sent to Rehab	1	

Prevention of Domestic Violence #6

Best prevention of domestic violence in our community?

Result	#	Comments
Awareness	17	Teen awareness; awareness outside of reservation; awareness and education such as television commercials, DV Walk, shoe cards, pamphlets in bathrooms; education on DV awareness, ads on preventing DV, word of mouth from others who have been in shelter, providing information on radio, internet, flyers; give more DV information, provide phone numbers, survivor's share experience
Resources	7	Services outside of reservation, resources like Web of Friends, availability of resources, more visible information about programs for victims, informing other victims of services, more information on how to overcome abuse
Shelters	5	Shelters, safe houses, housing in communities, safe place with a level of independence
Offender Accountability	5	Harsher sentencing, more severe consequences, stronger charges, tougher laws, law better enforced
Education	4	Educate children about domestic violence, educate on healthy relationships as young as possible, school programs
Outreach	4	Hospitals, doctors asking questions, teachers asking questions, churches
Nothing	2	Nothing being done
Communication	1	

Prevention of Domestic Violence #7

Three wishes for serving domestic violence survivors?

Result	#	Comments
Increased Resources	24	Easier access to documents, services for those moving out of DV situation, long-term support, classes, support groups, financial help for mothers with infants, affordable childcare, policy changes so financial assistance does not disappear as soon as employed, transportation to services, financial assistance, reliable foster system, hotline, availability of professional counseling, sliding scale system for services, knowledge of resources, life skills, employment, education, affordable services, accessible agencies, safe haven for children while parents work out their difficulties
Increased Shelter Opportunities	22	Easier access to shelter, more shelters, safe place, more shelters for Spanish/Mexican individuals, continue to have exceptional shelters, less discrimination, no eligibility limits on older boys, accept kids of all ages, extend 30 day limit in shelter, for families of two, for single female victims
Enhanced Criminal Justice Response	16	Stricter, tougher, stronger, more severe charges for abusers, harsher on abusers on first and second calls, jail time for all abusers, stronger child custody laws, judges are aware of the issue, charges are carried forward, DV court, compassionate lawyers, police involvement, more rights for victims
Increased Education	15	Educate on relationships, awareness and education, domestic violence education, equal DV awareness for providers from all systems, accessible information and education to low-income, provide more information on television and radio, inform the community, knowledge of domestic violence, more information geared towards abusers so they know what will happen to them, outreach, can happen to anyone, can cycle
Increased Resources in Spanish	7	More information in Spanish, increased services in Spanish, more Spanish-speaking representatives, translator services, more court services in Spanish
Increased Resources for Undocumented Women	9	Less discrimination, information and services in Spanish (especially victim's rights), treat everyone equally, undocumented hold back from calling for help due to discrimination and end up losing their lives, dedicate more time and attention to making changes to help victims obtain residency without all the barriers and requirements, continue helping find work and opportunities, give legal status to DV victims
Children Taught Healthy Relationship Skills	5	Domestic violence curriculum in schools, prevention through education of children, parents teach children respect for others, teach children how to be members of a family without domestic violence
Sense of Personal Empowerment	4	Confidence, let survivors know they are not alone, collaborate with survivors, stand up against abuse
Seriousness Validated by Responders	3	Responders are more kind toward victims, advocates for children arrive with first responders, DV taken seriously
Affordable Housing	2	Places to live

Other #8

Anything else to share?

Result	#	Comments
Importance of Support System	8	Sharing my story with others, support of friends, co-workers, get information on support groups, working with child care provider to better child's life, my sister inspired me to leave
Importance of Helping Others Through Their Experiences	7	Information used to help others, take action on information gathered, hope more is done so no one else experiences DV, do something to end DV, help DV victims put a stop to DV, increase services available for women who are abused, hope information helps so no else suffers or dies from DV

Appendix E: Appreciative Inquiry Interview Results – Professionals

Preservation of Resources #1		
Part A: what was inspirational about survivor's journey?		
Result	#	Comments
Interviewees were by far most inspired by survivors of domestic violence themselves.	19	Personal spirit, courage, resiliency, support of others, take control, right to be safe, no stigma, kids' safety, self-esteem, following through, values, empowered, determined, success, survivor's determination to survive
Some affected deeply by system.	2	Fear of the system, inspired to act by failure of system
Part B: what three top elements were instrumental to success?		
Result	#	Comments
Services	18	Central point of access, coordinated, accessible, Spanish, quick, seamless, integrated in community, confidential, examples: Phoenix Family Advocacy Center (FAC), Fresh Start, Temporary Assistance for Needy Families (TANF) (2), Section 8, WIC
Criminal justice system response	16	Good police training, police sensitive communication, Orders of Protection (3), police identify signs, court kept abuser in jail until trial, victim advocates (4), police unsupportive, advocate very attentive, hospital-police-prosecutor followed protocols
Counseling	8	Free, unlimited, children's behavioral health
Shelter/housing	8	Emergency shelter (6), transitional housing, given alternative place to stay during trial when couldn't go to shelter
Supportive environment	5	Tell story and be understood, emotional support, encouragement, from surviving to thriving
Connected to family	4	Reunited with family, daughter helped mother to safety, family support
Nogales Circles of Peace	1	Not all survivors want to leave relationship
Transportation	1	
Medical care	1	
Part C: what needs to be done to maintain or enhance services?		
Result	#	Comments
Address funding	6	Keep it consistent, provide more funding for services (2), provide more funding for housing, maintain funding, survivors can be reluctant to prosecute because they need abuser's income
Raise awareness	5	Put face on DV, educate public, Laura Munoz raised awareness about DV, educate survivors about cycle
Provide housing (not shelter)	4	Need a place to go that is not shelter
Support interdisciplinary work	3	Networking important, facilitate more interdisciplinary meetings, CPS called by school and helped family
Provide training	2	1st responder training needed, more training
View DV as one comprehensive system	1	
Need Court Watch program	1	
Advocate	1	
Intervene against patriarchal institutions	1	
Lower caseloads for prosecutors	1	

Preservation of Resources #2

What is being done well to provide access to underserved populations?

Result	#	Comments
Serving Spanish-speaking survivors	13	Bilingual outreach (3), monolingual services (3), bilingual services (2), Spanish behavioral health services (2), services for undocumented people have improved, clients get Visas, De Colores cultural competency programs
Examples of needed services	13	Behavioral health services, legal assistance, culturally specific programs, transportation (2), taxi services, victim advocates (2), discipline focused training, diversity and sensitivity training, educate underserved clients, advocates need list of resources and open beds
Examples of effective programs	11	Sojourner serves clients with substance abuse issues, community centers, South East Asians for Safe Families, Moma's House, Echo magazine, Doves, De Colores (2), Violence Against Women legislation, Family Advocacy Center (2), SEEDS program, Phoenix, Scottsdale, CONTACTS
Steps to provide better access	10	Increase media awareness, listen and advocate, increase number of safe houses, network well, employ case workers who reflect diversity of clients, reimburse shelters for capacity not occupancy, provide funding to keep services available, shelters cooperate with police, utilize beds fully so more beds won't be needed, have staff provide personal support
Examples of underserved populations still in need	8	Boys older than 12 years, seriously mentally ill, substance abuse, refugees, LGBT, mid to high income survivors, substance abuse, men victimized as children
Tribal concerns	4	Number of shelters on tribal lands has increased, Amnesty International- Violence Against Native Women, tribal laws need to improve, need more services for tribal women
Emergency shelter	4	Emergency shelter serve survivors with low incomes well (2), lose clients in transitional housing, survivor insecure about entering shelter but found great support
Recognition	2	Recognition of underserved populations has improved, acknowledging need more
Underserved is overemphasized	1	
Good referrals	1	
Hotline attitudes have changed	1	

Offender Accountability #3		
<i>Describe a peak experience when abuser was held accountable.</i>		
Result	#	Comments
Criminal justice system response	22	Treated DV as public safety issue, swift consequences (2), judges trained in DV, police free to determine arrests, collaboration between 911-prosecution-jail-advocate, positive relationship between police and shelter, police knowledgeable about coercive control, new detectives trained, need more victim advocates, assign same victim advocate to police officer, legal advocate assisted and present in court (3), Family Court researched and recreated documentation, police and Maricopa County Attorney's Office brought abuser to trial, good laws like 3 strikes means aggravated assault, court system involved, need better education about orders of protection (OOP), OOP used effectively (2), prosecutor subpoenaed survivor to testify and abuser convicted, good investigation
Victim participated fully	8	Survivor well educated about process, empowered, gave good statements for police, participation is key, attended trial, cooperated because friend had just been killed by her abuser and she realized that could have been her, testifying positive experience for survivor and helps to convict abuser, survivor didn't minimize abuse
Abuser not often held accountable	5	Not held accountable (3), not held accountable until felony charge, rarely adjudicated
Examples of effective actions	8	Men holding men accountable, modified Duluth model, responsive approach, all stay engaged through process, no personal agendas, unified voice, compliance specialist, photos of injuries
Barriers	4	Need more accurate reporting, need to change thinking behind abuse, reverse dual arrest policies, Crawford ruling hurt accountability prosecution (excited utterance)
Treatment	2	Offender treatment, highly skilled therapists
Abuser wanted to return to jail	1	Offended again and realized needed more services
Not survivor's job to hold abuser accountable	1	

Systems Accountability #4

Describe a positive experience when one or more systems accessed for benefit of survivor.

Result	#	Comments
Interdisciplinary collaborations	17	Teams secured services for survivor, school and shelter partnered, shelters collaborate with each other and share overhead costs, joint meetings with police-CPS-FAC-shelter, shelters collaborated with criminal justice system, social service agencies collaborated with courts (3), FAC collaborates with criminal justice system, Verizon phone, immigrant attorney, church, legal aide, municipalities sharing OOP data, survivor video conference with judge, probation officer met halfway (2), Com Trans, survivor's family, State
Good collaborative partners	16	Case manager coordinated services, child support enforcement, DES Community Conversations, DV Walk in Native communities, shelters going above and beyond, LARC, CPS coordinated housing and courts, Magellan direct care clinic, Scottsdale DV Center, Catholic Church, Pinal DV Court (2), O'Connor House, ASU School of Social Work, CASA, FAC, healthcare system, survivors benefit from working together (2)
Criminal justice system response	11	Judge removed from case, video phone helps judges, police made extra effort to arrest, courts, collaboration between police and cell phone company, police cared and followed through, quick follow-up and consequence, victim advocate worked with police to get OOP, advocate secured food and shelter for survivor, release revoked to keep survivor safe
Steps to increase accountability	6	Adjust approach to survivor's religious and cultural needs, move beyond anger, need tools and freedom to make best choices, create a database of DV convictions, stimulate economic development for families, bring Mentors in Violence Prevention Program to region, advocate going to court with survivor to obtain OOP
Transportation	2	Officers provided transportation, more transportation needed
Barriers	2	Most lesbian survivors not in shelter, survivors need more permanent housing and not shelter
Healthcare		
Inspired by seeing survivor smile at end of trial		

Prevention #5		
<i>Describe a peak experience when effects of prevention clearly seen.</i>		
Result	#	Comments
Examples of effective steps	20	Survivors gaining self-sufficiency (2), survivors building self-esteem, healthcare involved, survivors more aware of resources (2), community holds abusers accountable, prioritize cases, courts involved, change beliefs, realize it's cheaper to prevent than intervene, be tenacious about justice, ask people if they are okay (3), release from prison revoked (2), communication between agencies key, speaker at teen program
Education	14	Community outreach, education about signs of abuse (2), school-based education, community-based education, start in elementary school, education for kids, parenting classes, education about cycle, training
Awareness	6	Spanish-speaking public service announcements, community awareness, mainstream media, send a consistent message that DV is wrong, make messages about DV mainstream like anti-smoking ads
Examples of good prevention resources	6	Fresh Start Resource Center, Men's Anti-Violence Network, Scottsdale crisis teams, Violence Anonymous 12-step program, Purple Ribbon Council, Healthy Relationships session at Hispanic Women's Conference
Services	5	Intake center services, ongoing services, counseling, holistic services, accessible services
Family	4	Serve entire family within one system for true picture, recognize importance of offender accountability to whole family, engage survivor through children, daughter helped mother
Places of worship	2	Church stood up to abuser, Church of Latter Day Saints and tribe collaboration

Collaboration/Leveraging #6		
<i>Describe a time when energized by working closely with others toward shared goal.</i>		
Result	#	Comments
Examples of positive experiences	19	Relationships between police, advocates, and FAC; ASU Lado Telethon, research and advocacy groups, Victim Assistance Academy, legal clinic, 6 court and police collaboration to develop defendant info sheet, coercive control program with Phoenix police (2), FAC, Arizona Coalition Against Domestic Violence (AZCADV) Legislative Committee, National College of District Attorneys, MAG Regional Domestic Violence Council, Men's Anti-Violence Network, Governor's Commission, impactful community services (2), community volunteers, prosecutors used to be assigned to precincts and communication improved, detective very involved, Pendergast School District, Kids at Hope Training, collaborating with law enforcement-doctors-prosecutors to address shocking rates of child abuse
Necessary elements	13	Pride in work (2), short-term goals with long-term focus, share resources, focus on safety for survivors, innovative solutions, common ground, excited about activities, honesty (2), diversity (3)
Inspired by survivors	9	Survivors participated in DV Walk, survivor success (3), survivor educated and employed, empathy, CPS and advocates focused on children, contact with survivors personalizes work
Group dynamics	8	Decision people/right people in the room (3), everything comes together because everyone is necessary to the process, right leadership, inspired by colleague success and knowledge (2), group member roles respected
Examples of steps needing to be taken	3	Institutionalize bullying, prioritize cases, see DV as function of poverty

Leadership Development #7

Describe what makes leadership irresistible.

Result	#	Comments
Personal characteristics	13	Honest about flaws (2), dynamic, enthusiastic (5), stands by decisions, visionary, confident, no personal agenda, truly believes in work
Passionate	10	Worked in trenches and still passionate, passionate about work
Interactions with others	8	Brings diverse groups together, ability to motivate through example (2), challenges others, creates sense of ownership (2), protects and understands survivors (2)
Communication	7	Gives honest feedback, spokesperson people can relate to, good communicator, consistent outspoken message, advocates, asks the hard questions, willing to listen
Impact on others	7	All are equal, empowers others, personally invests in others, everyone matters, all are valued, ordinary people make a difference, inspiring
Innovative	6	Fosters different perspectives, open to change, open to new approaches, creative problem solving, new ideas, flexible
No ego	5	Selflessness, no ego
Examples of irresistible leadership	5	Sojourner's proactive position on budget reductions, Clothesline Project's enthusiastic outreach, Justice O'Connor's multi-disciplinary approach, MAG Regional Domestic Violence Council's and ACADV's open dialogue, Kids at Hope's encouragement to find positive attributes even in negative situations
Examples of steps needing to be taken	5	Focus on solutions, use an informed strength-based approach, develop redundancies in system, offer true life stories to inspire and show steps taken
Knowledgeable	5	Hands-on knowledge, not just theory, informed and involved, focuses on teaching moments, knowledgeable

Other #8

Is there anything else to share?

Result	#	Comments
Criminal justice issues	12	Make policies specific to DV because DV is unlike other crimes, police and detective workloads are too high with 90 cases a month, assign the same victim advocate to an officer each time, need more legal assistance (2), prosecute more people, Kaity's Law helps teen dating survivors, changes in law and police methods helping, criminal justice response has improved, need for more courts to address the whole family, hundreds of cases not prosecuted for very one that is successful because survivor recants, focus more on police investigations and successful prosecution
Next steps	9	Tear down barriers and work together, recognize staff burn-out as a critical issue, do more group projects, raise awareness about FAC, need to challenge the system, need really good facilitators, address funding issues and medical/DV services being cut, ensure survivors and children can access ongoing counseling, more shelters and services in West Valley
Survivor issues	7	Investigate the family relationships, fear of immigration issues, survivors speak Mexican/Indian dialect and are learning to read and write, women sometimes victimize each other in shelter, don't put survivors down, use an empowerment model, fear of system
Training	3	Joint training between shelters and police, education is key, cross train
Offenders	3	Hold terrorists accountable, need to address offenders more, stop creating offenders
Hard to share only positive	1	

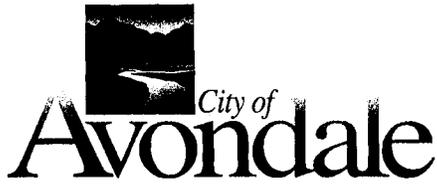
**Regional Plan to
End Domestic Violence**



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www.mag.maricopa.gov

Appointment of Members and Officers for the Transportation Policy Committee (TPC) - June 2010

Central City	Councilmember Peggy Neely
Seven Largest Cities	
Mesa	Mayor Scott Smith
Glendale	Mayor Elaine Scruggs
Scottsdale	Mayor Jim Lane
Chandler	Councilmember Jack Sellers
Gilbert	Vice Mayor Les Presmyk
Peoria	Mayor Bob Barrett
Tempe	Vice Mayor Shana Ellis
Five Cities/Towns Elected Officials	
(Three to achieve geographic balance, selected from and by the under represented geographic area)	
Avondale	Mayor Marie Lopez Rogers
Goodyear	Mayor James Cavanaugh
Surprise	Mayor Lyn Truitt
(Two At-Large geographically balanced, selected by the Regional Council)	
Buckeye	Mayor Jackie Meck
Cave Creek	Councilmember Dick Esser
Maricopa County Supervisor	Supervisor Max Wilson
Native American Indian Community	Phillip K. Matthews Salt River Pima-Maricopa Indian Community
State Transportation Board	Victor Flores
Chair, Citizen's Transportation Oversight Committee	F. Rockne Arnett
Officer Positions (Names submitted for consideration)	
Chair	Mayor Scott Smith, Mesa
Vice Chair	Councilmember Peggy Neely, Phoenix



May 12, 2010

Maricopa Association of Governments
Chairman Neely
302 North 1st Avenue, Suite 300
Arizona 85003

Dear Chairman Neely:

Please accept this letter as my interest in continuing to serve on the Maricopa Association of Governments (MAG) **Transportation Policy Committee** to full fill one of the three West Valley Geographic Balance members.

As you know, I am the outgoing Chair of the Transportation Policy Committee and have a record of dedication and long-term involvement in various leadership roles within MAG which makes me ideal candidate for remaining on the Committee.

Thank you for your consideration. If you have any questions, please contact me at 623-333-1911 or mlrogers@avondale.org.

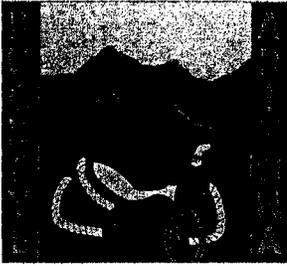
Sincerely,

A handwritten signature in black ink that reads "Marie Lopez Rogers". The signature is written in a cursive, flowing style.

Marie Lopez Rogers
Mayor

Office of the Mayor

11465 W. Civic Center Drive • Avondale, AZ 85323
Phone: (623) 333-1900 • Fax: (623) 333-0120 • TDD: (623) 333-0012
www.avondale.org



Town of Buckeye

Office of the Mayor

May 18, 2010

Councilmember Peggy Neely
Chair, Regional Council
Maricopa Association of Governments
302 North 1st Avenue, Suite 300
Phoenix, Arizona 85003

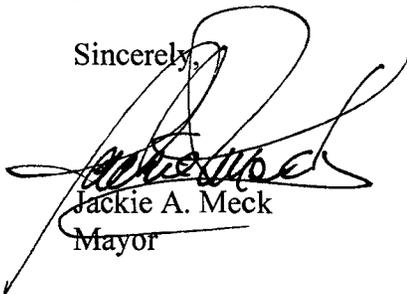
Re: Transportation Policy Committee Nominations

Dear Councilmember Neely:

Please accept this letter as formal expression of my interest in continuing to serve on the Transportation Policy Committee in the At-Large, geographically balanced position for the West Valley. My interest in continued service arises from Buckeye's unique role in the future of transportation development in the region.

The Town of Buckeye Municipal Planning Area (MPA) comprises a significant portion of the area of the Interstate 10-Hassayampa Valley Transportation Framework Study which represents a key element of the next phase of transportation infrastructure development for the region. Likewise, much of the future commuter rail corridor identified in MAG's Yuma West Commuter Rail Corridor Development Plan is located in Buckeye. Our Town is also at the heart of the fastest growing portion of the Valley with an expected population of 1.5 million at full-build-out. Finally, Buckeye is nearing completion of its first multimodal transportation master plan which will serve as a guide for transportation development in the Town's MPA for the next twenty-five years. Buckeye's role in all of these efforts enables me to bring essential expertise and a unique perspective to the Transportation Policy Committee.

Sincerely,



Jackie A. Meck
Mayor



Chandler · Arizona
Where Values Make The Difference

Boyd W. Dunn
Mayor

Office of the Mayor

Telephone
(480) 782-2200

Fax
(480) 782-2233

E-mail
boyd.dunn@chandleraz.gov

Mailing Address
Mail Stop 603
PO Box 4008
Chandler, Arizona 85244-4008

Location
Suite 301
55 North Arizona Place
Chandler, Arizona 85225

May 24, 2010

The Honorable Peggy Neely
Chairman, Transportation Policy Committee
Maricopa Association of Governments
302 N. 1st Avenue Suite 300
Phoenix, AZ 85003

Dear Councilmember Neely:

I am respectfully submitting the name of Councilmember Jack Sellers to serve as Chandler representative on the MAG Transportation Policy Committee.

I look forward to Chandler's presence on this critical board as we work regionally on the transportation issues of the area. Thank you again for the opportunity. And thank you, Councilmember Neely, for your leadership on the Committee.

Sincerely,

Boyd W. Dunn
Mayor

cc: Dennis Smith

RICHARD K. ESSER
5423 NEW RIVER ROAD
CAVE CREEK, ARIZONA 85331

June 7, 2010

Maricopa Association of Governments
Chairperson Peggy Neely
c/o Dennis Smith, Executive Director
302 North First Avenue, Suite 300
Phoenix Arizona 85003

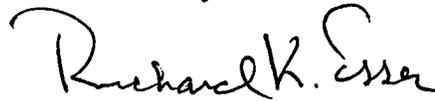
Dear Chairperson Neely:

Please accept this letter to confirm my interest in continuing to serve on the MAG Transportation Policy Committee.

I am presently serving my third term as a Cave Creek Council member and have a special interest in this committee, based on my background and experience with transportation matters.

I welcome the opportunity to serve the region in this capacity.

Sincerely,



RICHARD K. ESSER
Cave Creek Council Member

cc: Marie Lopez Rogers, Mayor, City of Avondale



Town of Gilbert, Arizona
A Community of Excellence
Municipal Center
50 East Civic Center Drive
Gilbert, Arizona 85296

From the Office of
Mayor
John W. Lewis

"Most Livable City"
U.S. Conf. of Mayors

June 9, 2010

Dennis Smith, Executive Director
Maricopa Association of Governments
302 North First Avenue
Phoenix, AZ 85003

Dear Mr. Smith:

Please accept this letter as a formal request to appoint Councilman Les Presmyk to serve as the designated member for the MAG Transportation Policy Committee for the Town of Gilbert.

Thank you for your continued efforts.

Sincerely,

John W. Lewis
Mayor

cc: Councilman Les Presmyk



ELAINE M. SCRUGGS
Mayor

June 3, 2010

The Honorable Peggy Neely
Chairman, Maricopa Association of Governments
302 North 1st Avenue, Suite 300
Phoenix, Arizona 85003

Dear Councilmember Neely:

I am writing to express my interest in being reappointed as the Glendale representative on the Transportation Policy Committee at the Maricopa Association of Governments. I look forward to continuing to serve with you and the other members of the committee.

Sincerely,

Elaine M. Scruggs
Mayor

James M. Cavanaugh
Mayor

June 4, 2010

Councilmember Peggy Neely
Chair, MAG Regional Council
c/o Maricopa Association of Governments
302 N 1st Ave, Ste 300
Phoenix, AZ 85003

Councilmember Neely;

Please let this letter serve as my interest for appointment to the Maricopa Association of Governments
Regional Council Transportation Policy Committee.

Sincerely,


James M. Cavanaugh
Mayor

Office of the Mayor

190 N. Litchfield Road, Goodyear, AZ 85338

623-882-7776 • Fax 623-932-4249 • TDD 623-932-6500 • www.goodyearaz.gov

Goodyear

★★★★
All-America

2008

City
Livability
Award
2008



Maricopa County

DON STAPLEY, CHAIRMAN
Board of Supervisors, District 2

301 West Jefferson Street
10th Floor
Phoenix, AZ 85003-2143
Phone: 602-506-7431
Fax: 602-506-6362
www.maricopa.gov

June 11, 2010

Councilmember Peggy Neely
Chair, MAG Regional Council
City of Phoenix
200 West Washington Street, 11th Floor
Phoenix, Arizona 85003

RE: Appointment of Supervisor Max Wilson to the MAG Transportation policy
Committee (TPC)

Dear Councilmember Neely:

As Chairman of the Maricopa County Board of Supervisors, I am appointing Supervisor Max Wilson to represent the County on the MAG Transportation Policy Committee. Supervisor Wilson has been serving in that capacity for several years, and I am sure he will continue to be a cooperative and helpful member of the committee.

Thank you for serving our community, and we look forward to working with you in the future.

Sincerely,

A handwritten signature in black ink that reads "Don Stapley".

Don Stapley, Chairman
Maricopa County Board of Supervisors
District 2



20 E Main St Suite 750
PO Box 1466
Mesa, Arizona 85211-1466

mesaaz.gov

May 24, 2010

Councilmember Peggy Neely
MAG Regional Council Chair
c/o Dennis Smith
Maricopa Association of Governments
302 North 1st Ave., Suite 300
Phoenix, AZ 85003

Dear Councilmember Neely:

Please accept this letter as my interest to continue as a member of the MAG Transportation Policy Committee (TPC). In addition, I would like the opportunity to be considered for the position of Chair of TPC. It has been an honor to participate on the committee and I would welcome the opportunity to serve another term.

Sincerely,

A handwritten signature in black ink, appearing to be "Scott Smith". The signature is fluid and cursive, with a large loop at the beginning and a long horizontal stroke at the end.

Scott Smith
Mayor



City of Phoenix

OFFICE OF THE CITY COUNCIL
Councilwoman Peggy Neely
District 2

June 9, 2010

Mr. Dennis Smith
Executive Director
Maricopa Association of Governments
302 North 1st Avenue
Phoenix, Arizona 85003

Re: Transportation Policy Committee Vice Chair Position

Dear Mr. Smith:

Please accept this letter as my expression of interest to remain the Phoenix representative on the Transportation Policy Committee. Additionally, I am interested in being considered for the Vice Chair position.

If you have any questions or require any information about me for this process, please contact our Office of Government Relations at 602-256-4257.

Sincerely,

A handwritten signature in cursive script that reads "Peggy Neely".

Peggy Neely
Councilwoman
District 2

cc: Karen Peters, City of Phoenix



Honorable Bob Barrett
Mayor

June 10, 2010

Councilmember Peggy Neely
Chair, Regional Council
Maricopa Association of Governments
302 North 1st Avenue, Suite 300
Phoenix, AZ 85003

Dear Councilmember Neely:

Please accept this letter as my request to serve as the City of Peoria's representative on the Transportation Policy Committee.

I look forward to serving in this capacity and assisting in developing regional transportation policy.

Sincerely,

A handwritten signature in black ink that reads "Bob Barrett". The signature is written in a cursive style with a long horizontal stroke at the end.

Bob Barrett
Mayor



SALT RIVER
PIMA-MARICOPA INDIAN COMMUNITY
10005 East Osborn Road / Scottsdale, Arizona 85256-9722 / Phone (480) 362-7465 / Fax (480) 278-7188

June 10, 2010

The Honorable Peggy Neely
Chairperson
Maricopa Association of Governments
302 North 1st Avenue, Suite 300
Phoenix, Arizona 85003

Dear Chairperson Neely:

On behalf of the Salt River Pima-Maricopa Indian Community ("Community"), I'm writing to submit the name of Mr. Phillip K. Matthews as our nominee to serve on the Maricopa Association of Governments Transportation Policy Committee ("MAG TPC"). Attached you will find a copy of the resume for your review.

Mr. Matthews has a Bachelor of Science in Civil Engineering and he is currently employed as Assistant Director for Engineering and Construction Services. He is directly involved with transportation projects, which include partnerships with Maricopa Association of Governments, City of Scottsdale, City of Mesa, Bureau of Indian Affairs, and Maricopa County Highway Department.

In addition, Mr. Matthews has worked for various private firms dealing with transportation and related engineering projects. As you can see, Mr. Matthews has invaluable experience to assist MAG TPC in developing regional transportation policy positions for Regional Council consideration. I strongly urge your serious deliberation of our request. Thank you.

Sincerely,

A handwritten signature in black ink, appearing to read "Diane Enos".

Diane Enos
President

PHILLIP K. MATTHEWS

EDUCATION: B.S. in Civil Engineering
University of Idaho 1973

REGISTRATION: Civil Engineer, State of Arizona #12396

SALT RIVER PIMA – MARICOPA INDIAN COMMUNITY – March 1999 – Present

2000 – Present Assistant Director Engineering and Construction Services with over site management responsibilities for the Water Resource Division, Community Construction Division, and development engineering assistance. Projects include Pima Road a joint project with the Community and City of Scottsdale, Dobson Road Design Concept Report with the Maricopa County Highway Department and the City of Mesa, BIA and ARRA funded community road projects, MAG funded road projects, infrastructure master planning for Section 12, and assisting the Community Development Department with implementing processes and standards for development and management of the commercial corridor.

1999 – 2000 Design Division Manager overseeing the design for residential housing, utilities, streets, and right-of-way surveying. Projects included residential home designs, residential home remodeling, site utilities, and right-of –way acquisition.

CLARK ENGINEERS SW, INC. - February 1997 – March 1999

Director of Civil/Structural Engineering Department with over site management of site engineering for commercial and residential property.

PRIVATE CONSULTANT - September 1994 - February 1997

Private Consultant performing engineering services in the areas of subdivision designs, site development designs, environmental site assessments, site surveys, and other engineering designs for private clients and consulting engineering firms.

LANDMARK CONSULTANTS INC. - January 1994 - September 1994:

Principal in charge of Phase I and II site assessments, asbestos inspections, and air monitoring services for private clients and Government Agencies.

JAYKIM ENGINEERS, INC. - October 1986 - January 1994:

Vice President/Office Manager for the Arizona operations of Jaykim Engineers, with responsibility for acquiring and directing statewide and nationwide engineering services. The office was heavily involved in site & route surveying, residential subdivisions commercial site development designs, environmental site assessments, asbestos inspections, and Phase II \ III remediation services.

SVERDRUP CORPORATION - January 1986 - October 1986

Director of Field Services, overseeing consultant services in the areas of surveying, mapping, geotechnical investigations, and right-of-way plan preparation as the Arizona Statewide Management Consultant to the Arizona Department of Transportation. Duties included overseeing the production of

over twenty miles of aerial topographic mapping for freeway and highway designs plus reviewing consultant plan submittals.

DMJM/ADAM, HAMLIN, ANDERSON - April 1983 - January 1986

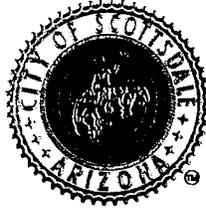
Director of Programs with responsibilities for organizing marketing efforts, new client contacts, preparing proposals, and supervising special projects.

ADAM HAMLIN ANDERSON CONSULTING ENGINEERS, INC. - November 1978 - March 1983

Starting as Project Manager, achieving the status of Director of the Civil Engineering Division, supervising from five to fifteen personnel and ten projects at a time.

TUDOR ENGINEERING COMPANY - October 1971 - October 1978

Resident Construction Engineer, responsible for fiscal management records, regulatory agency liaison, and construction inspection. Contract City Engineer for Kuna, Idaho, providing consultation on planning, zoning, subdivision regulations, and regulations for water, sewer and street construction.



W.J. "JIM" LANE
Mayor

May 12, 2010

The Honorable Peggy Neely
Councilmember
City of Phoenix
200 W. Washington
Phoenix, AZ 85003

RE: Nomination for MAG Transportation Policy Committee

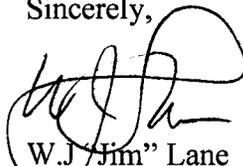
Dear Councilmember Neely:

I am writing to you today to confirm my commitment to continue to serve on the Maricopa Association of Government's Transportation Policy Committee. As effective regional transportation solutions continue to be of the utmost importance to our communities, the City of Scottsdale appreciates the opportunity to contribute the efforts of the Committee.

I would appreciate support for this important position and the opportunity to serve in this regard. Please contact me if you have any questions or require additional information.

Thank you for your consideration.

Sincerely,



W.J. "Jim" Lane
Mayor

cc: MAG Nominating Committee
Dennis Smith, MAG Executive Director





L.E. "Lyn" Truitt, Mayor
City of Surprise
16000 N. Civic Center Plaza
Surprise AZ 85374
623-222-1300/222-1301 fax

June 10, 2010

The Honorable Peggy Neely
Chairperson, MAG Regional Council
302 N. 1st Ave., Ste. 300
Phoenix, AZ 85005

Dear Chairperson Neely,

I am writing to express my interest in retaining the City of Surprise seat on the MAG Transportation Policy Committee. I have received verbal support for my nomination from the Northwest Valley Mayors.

The City of Surprise has demonstrated its commitment as a regional planning partner as evidenced by the financial investment, resource commitment and participation in a wide variety of regional projects. Our planning area borders six of the seven Northwest Valley cities, and Surprise has responded to the responsibility of being so positioned.

Personally, I have been a self-employed businessman for over 30 years. As a national training consultant for a delivery systems company, I have trained entrepreneurs in small business ownership, management and operations. I am currently the Owner/Broker of L.E. Truitt & Associates Real Estate Consulting and a Certified International Property Specialist. I am committed to maintaining the city's growing reputation as a valuable regional planning resource.

I would be happy to answer any questions you or the committee has and appreciate your consideration.

Respectfully submitted,

A handwritten signature in black ink, appearing to read "L.E. Truitt", with a stylized flourish at the end.

L.E. "Lyn" Truitt, Mayor
City of Surprise

cc: Mr. Dennis Smith, Executive Director, MAG



City of Tempe
P.O. Box 5002
31 East Fifth Street
Tempe, AZ 85280
480-350-8225

May 21, 2010

The Honorable Peggy Neely, Chair
Maricopa Association of Governments
c/o Executive Director Dennis Smith
302 North 1st Avenue, Suite 300
Phoenix, Arizona 85003

Hugh Hallman
Mayor

Re: Request for Nomination -- MAG TPC

Shana Ellis
Vice Mayor

Dear Chair Neely:

P. Ben Arredondo
Councilmember

This letter is to reaffirm the nomination of Vice Mayor Shana Ellis to serve as my designee as a member of Transportation Policy Committee (TPC). I am confident that Vice Mayor Ellis will continue to serve as a great representative for Tempe on the TPC.

Mark W. Mitchell
Councilmember

Joel Navarro
Councilmember

I strongly support our regional mission, and believe that I can continue to be an asset in financial accountability, planning and management as part of the MAG team, while serving on the Executive Committee and Regional Council.

Onnie Shekerjian
Councilmember

Corey D. Woods
Councilmember

I have enjoyed working with all of the members of the Regional Council, and look forward to continuing our association. Thank you for considering the appointment of Vice Mayor Shana Ellis as Tempe's designee to the Transportation Policy Committee.

Cordially,

A handwritten signature in black ink, appearing to read 'Hugh Hallman', written in a cursive style.

Hugh

Agenda Item #7

1 IGNACIA S. MORENO
Assistant Attorney General
2 Environment and Natural Resources Division

3 ROCHELLE L. RUSSELL
Trial Attorney
4 United States Department of Justice
Environment and Natural Resources Division
5 Environmental Defense Section
301 Howard Street, Suite 1050
6 San Francisco, CA 94105
Telephone: (415) 744-6566
7 Fax: (415) 744-6476
Email: rochelle.russell@usdoj.gov

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IN THE UNITED STATES DISTRICT COURT
FOR THE DISTRICT OF ARIZONA [PHOENIX DIVISION]

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SANDRA L. BAHR, DIANE E. BROWN,
and DAVID MATUSOW,

CV 09-2511-PHX-MHM

16

Plaintiffs,

**NOTICE OF LODGING OF PROPOSED
CONSENT DECREE**

17

v.

18

LISA JACKSON, in her official capacity as
Administrator of the United States
Environmental Protection Agency, and the
20 UNITED STATES ENVIRONMENTAL
21 PROTECTION AGENCY,

Defendants.

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1 Defendant Lisa Jackson, in her official capacity as Administrator of the United
2 States Environmental Protection Agency, and Defendant United States Environmental
3 Protection Agency (collectively, "EPA"), hereby lodge with the Court a proposed consent
4 decree that contains the terms of a proposed settlement of this action. See Attachment 1,
5 Consent Decree.

6 **The proposed consent decree should not be signed or entered by the Court at**
7 **this time.** Pursuant to section 113(g) of the Clean Air Act, 42 U.S.C. § 7413(g), the EPA
8 Administrator must provide "a reasonable opportunity by notice in the Federal Register to
9 persons who are not named as parties or intervenors to the action or matter to comment in
10 writing" upon the proposed consent decree. Accordingly, EPA will publish in the Federal
11 Register a notice of the proposed consent decree and request public comments. After a
12 reasonable comment period, the EPA Administrator will promptly consider any written
13 comments received and, if none of the comments disclose facts or considerations which
14 indicate that the proposed consent decree is inappropriate, improper, inadequate, or
15 inconsistent with the requirements of the Clean Air Act, Defendants will move for entry
16 of the decree.

17 Respectfully submitted,

18 IGNACIA S. MORENO
19 Assistant Attorney General
Environment and Natural Resources Division

20 Dated: June 23, 2010

21 /s/ Rochelle L. Russell
22 ROCHELLE L. RUSSELL
23 Trial Attorney
24 United States Department of Justice
25 Environment and Natural Resources Division
26 Environmental Defense Section
27 301 Howard Street, Suite 1050
28 San Francisco, CA 94105
Phone: (415) 744-6566
Email: rochelle.russell@usdoj.gov
Attorney for Defendants

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CERTIFICATE OF SERVICE

The undersigned hereby certifies that on June 23, 2010, true and correct copies of the foregoing **NOTICE OF LODGING OF PROPOSED CONSENT DECREE** were served on the following Counsel of Record via the Court's CM/ECF system:

Joy E. Herr-Cardillo
Arizona Center for Law in the Public Interest
2205 E. Speedway Blvd.
Tucson, AZ 85719
520-529-1798
Fax: 520-529-2927
Email: jherrcardillo@aclpi.org

Timothy Michael Hogan
Arizona Center for Law in the Public Interest
2205 E. Speedway Blvd.
Tucson, AZ 85719
520-529-1798
Fax: 520-529-2927
Email: thogan@aclpi.org

/s/ Rochelle L. Russell
ROCHELLE L. RUSSELL

1 IGNACIA S. MORENO
Assistant Attorney General
2 Environment and Natural Resources Division

3 ROCHELLE L. RUSSELL
Trial Attorney
4 United States Department of Justice
Environment and Natural Resources Division
5 Environmental Defense Section
301 Howard Street, Suite 1050
6 San Francisco, CA 94105
Telephone: (415) 744-6566
7 Fax: (415) 744-6476
Email: rochelle.russell@usdoj.gov
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IN THE UNITED STATES DISTRICT COURT
12 FOR THE DISTRICT OF ARIZONA [PHOENIX DIVISION]
13

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SANDRA L. BAHR, DIANE E. BROWN,
and DAVID MATUSOW,

CV 09-2511-PHX-MHM

17

Plaintiffs,

CONSENT DECREE

18

v.

19

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21

LISA JACKSON, in her official capacity as
Administrator of the United States
Environmental Protection Agency, and the
UNITED STATES ENVIRONMENTAL
PROTECTION AGENCY,

22

Defendants.

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1 WHEREAS, on December 2, 2009, Plaintiffs Sandra L. Bahr, Diane E. Brown,
2 and David Matusow filed the complaint in the above-captioned matter against Defendants
3 Lisa Jackson, in her official capacity as Administrator of the United States Environmental
4 Protection Agency, and the United States Environmental Protection Agency (collectively,
5 “EPA”), alleging that EPA has failed to undertake a certain nondiscretionary duty under
6 the Clean Air Act (“CAA”), 42 U.S.C. §§ 7401-7671q, and that such alleged failure is
7 actionable under section 304(a)(2) of the CAA, 42 U.S.C. § 7604(a)(2);

8 WHEREAS, section 110(a)(1) of the CAA, 42 U.S.C. § 7410(a)(1), requires States
9 to adopt and submit to EPA for review state implementation plans (“SIPs”), which
10 establish specific control measures and other requirements that apply to particular sources
11 of air pollution within a State and are designed to attain, maintain, and enforce National
12 Ambient Air Quality Standards established by EPA that specify the maximum permissible
13 concentrations for those pollutants in the ambient air, see 42 U.S.C. §§ 7408, 7409;

14 WHEREAS, section 189(d) of the CAA, 42 U.S.C. § 7513a(d), requires States to
15 adopt and submit to EPA SIP revisions to meet specific additional requirements for
16 serious PM-10 nonattainment areas that have failed to meet the standard by the applicable
17 attainment date;

18 WHEREAS, section 110(k) of the CAA, 42 U.S.C. § 7410(k), sets forth the
19 process by which EPA is to review SIP submissions, including SIP revisions;

20 WHEREAS, Plaintiffs’ complaint alleges that EPA has a nondiscretionary duty to
21 act on SIP submissions and revisions submitted to EPA within the time lines set forth in
22 section 110(k)(2) of the CAA, 42 U.S.C. § 7410(k)(2);

23 WHEREAS, Plaintiffs’ complaint alleges that EPA has failed to take timely action
24 under CAA section 110(k)(2) on the “MAG 2007 Five Percent Plan for PM-10 for the
25 Maricopa County Nonattainment Area,” Maricopa Association of Governments, 2007
26 (the “5% Plan”), a SIP revision submitted to EPA in December 2007 by the State of
27 Arizona pursuant to section 189(d);

28 WHEREAS, Plaintiffs’ complaint seeks an order from this Court directing EPA to

1 either approve or disapprove, in whole or in part, the 5% Plan on a specific timetable;

2 WHEREAS, the parties have agreed to a settlement of this action without
3 admission of any issue of fact or law;

4 WHEREAS, the parties, by entering into this Consent Decree, do not waive or
5 limit any claim or defense, on any grounds, related to any final EPA action;

6 WHEREAS, the parties consider this Consent Decree to be an adequate and
7 equitable resolution of all of the claims in this matter;

8 WHEREAS, it is in the interest of the public, the parties, and judicial economy to
9 resolve this matter without protracted litigation;

10 WHEREAS, the parties agree that this Court has jurisdiction over this matter
11 pursuant to the citizen suit provision in section 304(a)(2) of the CAA and that venue lies
12 in the District of Arizona;

13 WHEREAS, the Court, by entering this Consent Decree, finds that the Consent
14 Decree is fair, reasonable, in the public interest, and consistent with the CAA;

15 NOW THEREFORE, before the taking of testimony, without trial or determination
16 of any issue of fact or law, and upon the consent of the parties, it is hereby ordered,
17 adjudged and decreed that:

18 1. EPA shall sign for publication in the Federal Register:

19 (a) no later than September 3, 2010, a notice of the Agency's proposed
20 action on the 5% Plan pursuant to section 110(k) of the CAA. Once
21 signed, EPA shall deliver the notice to the Office of the Federal
22 Register for publication; and

23 (b) no later than January 28, 2011, a notice of the Agency's final action
24 on the 5% Plan pursuant to section 110(k) of the CAA. Once signed,
25 EPA shall deliver the notice to the Office of the Federal Register for
26 publication.

27 2. When EPA's obligations under Paragraph 1 have been completed, the
28 parties will file a joint request to the Court to dismiss this matter with prejudice.

1 3. The deadlines in Paragraphs 1 and 9 may be extended (a) by written
2 stipulation of Plaintiffs and EPA with notice to the Court, or (b) by the Court upon
3 motion of EPA and upon consideration of any response by Plaintiffs.

4 4. Nothing in this Consent Decree shall be construed to limit or modify the
5 discretion accorded EPA by the CAA and by general principles of administrative law,
6 including the discretion to alter, amend or revise any response and/or final action
7 contemplated by this Consent Decree. EPA's obligation to take the actions set forth in
8 Paragraph 1 by the time specified therein does not constitute a limitation or modification
9 of EPA's discretion within the meaning of this paragraph.

10 5. Nothing in this Consent Decree shall be construed to confer upon the
11 district court jurisdiction to review any decision made in the final action identified in
12 Paragraph 1. Nothing in this Consent Decree shall be construed to confer upon the
13 district court jurisdiction to review any issues that are within the exclusive jurisdiction of
14 the United States Courts of Appeals pursuant to sections 307(b)(1) and 505 of the CAA,
15 42 U.S.C. §§ 7607(b)(1), 7661d.

16 6. This Court shall retain jurisdiction to enforce the terms of this Consent
17 Decree and to consider any requests for costs of litigation, including attorneys' fees.

18 7. In the event of a dispute between the parties concerning the interpretation or
19 implementation of any aspect of this Consent Decree, the disputing party shall provide the
20 other party with a written notice outlining the nature of the dispute and requesting
21 informal negotiations. If the parties cannot reach an agreed-upon resolution within ten
22 (10) business days after receipt of the notice, any party may move the Court to resolve the
23 dispute.

24 8. No motion or other proceeding seeking to enforce this Consent Decree shall
25 be considered properly filed, unless Plaintiffs have followed the procedure set forth in
26 Paragraph 7 and provided EPA with written notice received at least ten (10) business days
27 before the filing of such motion or proceeding.

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1 9. EPA agrees that, pursuant to section 304(d) of the CAA, 42 U.S.C. §
2 7604(d), Plaintiffs are both eligible and entitled to recover their costs of litigation in this
3 action, including reasonable attorneys' fees, incurred prior to entry of this Consent
4 Decree. The deadline for filing a motion for costs of litigation, including reasonable
5 attorneys' fees, is hereby extended until 90 days after the date on which the Court enters
6 this Consent Decree. During this time the parties shall seek to resolve informally any
7 claim for costs of litigation, including reasonable attorneys' fees.

8 10. The obligations imposed upon EPA under this Consent Decree may only be
9 undertaken using appropriated funds. No provisions of this Consent Decree shall be
10 interpreted as or constitute a commitment or requirement that EPA obligate or pay funds
11 in contravention of the Anti-Deficiency Act, 31 U.S.C. § 1341, or any other applicable
12 federal law.

13 11. Plaintiffs and EPA shall not challenge the terms of this Consent Decree or
14 this Court's jurisdiction to enter this Consent Decree.

15 12. The parties agree and acknowledge that before this Consent Decree is
16 entered by the Court, EPA must provide notice of this Consent Decree in the Federal
17 Register and an opportunity for public comment pursuant to section 113(g) of the CAA,
18 42 U.S.C. § 7413(g). After this Consent Decree has undergone notice and comment, the
19 Administrator and/or the Attorney General, as appropriate, shall promptly consider any
20 such written comments in determining whether to withdraw or withhold their consent to
21 the Consent Decree, in accordance with section 113(g) of the CAA. If the Administrator
22 and/or the Attorney General do not elect to withdraw or withhold their consent, EPA shall
23 promptly file a motion that requests the Court to enter this Consent Decree.

24 13. Any notices required or provided for by this Consent Decree shall be made
25 in writing, via facsimile, e-mail or other means, and sent to the following:

26 For Plaintiffs:

27 Joy E. Herr-Cardillo
28 Arizona Center for Law in the Public Interest
 2205 E. Speedway Blvd.

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Tucson, AZ 85719
Phone: (520) 529-1798
Fax: (520) 529-2927
Email: jherrcardillo@aclpi.org

For Defendants:

Rochelle L. Russell
U.S. Department of Justice
Environment and Natural Resources Division
Environmental Defense Section
301 Howard Street, Suite 1050
San Francisco, CA 94105
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Fax: (415) 744-6476
Email: rochelle.russell@usdoj.gov

Geoffrey Wilcox
Office of General Counsel
U.S. Environmental Protection Agency
Ariel Rios Bldg., MC 2344A
1200 Pennsylvania Ave., N.W.
Washington, DC 20460
Phone: (202) 564-5601
Fax: (202) 564-5603
Email: wilcox.geoffrey@epa.gov

Jan Taradash
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U.S. Environmental Protection Agency
75 Hawthorne Street, ORC-2
San Francisco, CA 94105
Phone: (415) 972-3907
Fax: (415) 947-3570
Email: taradash.jan@epa.gov

14. The undersigned representatives of each party certify that they are fully authorized by the party that they represent to bind that party to the terms of this Consent Decree.

IT IS SO ORDERED.

Dated: _____

MARY H. MURGUA
UNITED STATES DISTRICT JUDGE

1 COUNSEL FOR PLAINTIFFS:

2 Dated: June 23, 2010

/s/ Joy E. Herr-Cardillo (with permission)
JOY E. HERR-CARDILLO
TIMOTHY M. HOGAN
Arizona Center for law in the Public Interest
2205 E. Speedway Blvd.
Tucson, AZ 85719
Phone: (520) 529-1798
Email: jherrcardillo@aclpi.org
Email: thogan@aclpi.org
Counsel for Plaintiffs

8 COUNSEL FOR DEFENDANTS:

10 Dated: June 23, 2010

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Assistant Attorney General
Environment and Natural Resources Division
/s/ Rochelle L. Russell
ROCHELLE L. RUSSELL
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Attorney for Defendants

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302 North 1st Avenue, Suite 300 ▲ Phoenix, Arizona 85003
Phone (602) 254-6300 ▲ FAX (602) 254-6490

May 26, 2010

TO: Members of the MAG Regional Council

FROM: Mayor James M. Cavanaugh, Goodyear, Chair
2010 MAG Regional Council Nominating Committee

SUBJECT: MAG REGIONAL COUNCIL NOMINATING COMMITTEE REPORT

It has been my pleasure to serve as the Chair of the 2010 MAG Regional Council Nominating Committee. The Committee was appointed by MAG Regional Council Chair Peggy Neely at the April 28, 2010, Regional Council meeting. Since the appointment of the Nominating Committee, a substitution was made for one of the members. The Nominating Committee, according to the MAG Nomination Process, consists of five members. The other members of the Nominating Committee include Mayor Jackie Meck, Town of Buckeye; Councilmember Dick Esser, Town of Cave Creek; Mayor John Lewis, Town of Gilbert; and Mayor Bob Barrett, City of Peoria.

On May 26, 2010, the Nominating Committee met and made recommendations for the positions of Chair, Vice Chair, Treasurer, and three At-Large Members for the coming year (2010-2011). According to the Nomination Process, the past Chair also serves on the Executive Committee. The election will be held at the June 30, 2010, Regional Council meeting. The slate recommended by the Nominating Committee is noted below:

Chair	Mayor Thomas Schoaf, Litchfield Park
Vice Chair	Mayor Hugh Hallman, Tempe
Treasurer	Mayor Marie Lopez Rogers, Avondale
At-Large Member	Mayor Scott Smith, Mesa
At-Large Member	Mayor W. J. "Jim" Lane, Scottsdale
At-Large Member	Mayor Michael Le Vault, Youngtown
Past Chair	Councilmember Peggy Neely, Phoenix

Again, it was my pleasure to serve as the Chair of the 2010 Nominating Committee. Please contact me at (623) 882-7776 if you have any questions about the Nominating Committee report.